

newbedford
regional airport

RECONSTRUCT TERMINAL AREA APRONS PHASE I

A map of the New Bedford Regional Airport area. The airport is shown as a white area with a black border, labeled 'NEW BEDFORD REGIONAL AIRPORT'. It is surrounded by green areas representing wetlands: 'ACUSHNET CEDAR SWAMP' to the north, 'APPONAGANSETT SWAMP' to the west, and 'NEW YORK/NEW HAVEN RAIL ROAD' to the south. Blue areas represent water bodies: 'TURNERS POND' to the northwest and 'ACUSHNET' to the northeast. Roads shown include 'OLD FALL RIVER RD' to the west, 'FAUNCE CORNER RD' to the southwest, 'NEW PLAINVILLE RD' to the north, 'AIRPORT RD' to the southeast, 'SEANNAUT AVE' to the south, and 'MT PLEASANT ST' to the east. Highways 140 and 195 are also indicated. A north arrow is in the top right corner.

An aerial photograph of an airport and its surrounding area. A north arrow is located in the top left corner, pointing upwards and labeled "NORTH". A black callout line with a circular end points to a specific building, likely the terminal, located near the center of the runway. The airport features a long runway, taxiways, and various support buildings. The surrounding landscape includes residential areas, wooded land, and a body of water in the upper left.

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G1.1

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2016-09-26 NOI SUBMISSION

2/14/2017 3:11:33 PM P:\ASG Data\Projects\MA - New Bedford\032 Design Only - Reconstruct Terminal Aprons\CADD\ Preliminary Submission\103-032 SHEET_COVER.dwg (MKO)

9/14/2017 3:24:48 PM P:\ASB Data\Projects\MA - New Bedford\032 Design - Recreational Terminal Aprons\CADD\Preliminary Submittal\03-032 SHEET_GENERAL_v1.dwg (MKO)

TERMINAL APRON CONSTRUCTION NOTES:

AIRPORT OPERATIONS COORDINATOR

THE OWNER WILL PROVIDE AN OPERATING COORDINATOR WHO SHALL HAVE THE AUTHORITY TO OPEN AND CLOSE FACILITIES, ISSUE AND CANCEL NOTAMS AND TO COORDINATE WITH AIRPORT USERS.

AIRPORT SECURITY

THE CONTRACTOR SHALL COMPLY WITH ALL AIRPORT SECURITY REQUIREMENTS AS DIRECTED BY THE AIRPORT OPERATIONS COORDINATOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROLLING ACCESS TO THE WORK AREA AND ENSURING THAT AIRPORT SECURITY IS MAINTAINED AT ALL TIMES. THE FAA CAN IMPOSE FINES OF \$10,000 OR MORE FOR SECURITY VIOLATIONS AND INCURSIONS INTO ACTIVE AIRCRAFT OPERATION AREAS (AOA). THE CONTRACTOR SHALL PAY ALL FINES ASSESSED AGAINST THE AIRPORT DUE TO VIOLATIONS CAUSED BY THE CONTRACTOR AND HIS/HER PERSONNEL, SUBCONTRACTORS AND VENDORS.

PARKING PERSONAL VEHICLES SHALL BE IN DESIGNATED LOCATIONS ONLY. THE CONTRACTOR, AS A SUBSIDIARY OBLIGATION, SHALL PROVIDE ADEQUATE AND SAFE TRANSPORTATION FOR HIS/HER EMPLOYEES TO / FROM THE AREA WHERE THE VEHICLES ARE PARKED TO AND FROM THE WORK AREAS, EMPLOYEES AND DRIVERS OF WORK VEHICLES SHALL BE INSTRUCTED AS TO PROPER ACCESS ROADS AND SHALL BE CAUTIONED THAT UNAUTHORIZED USE OF AIRCRAFT PAVEMENTS OR OTHER AREAS OUTSIDE THE DESIGNATED WORK AREAS MAY LEAD TO THEIR ARREST AND SUBSEQUENT PAYMENT OF FINES.

THE CONTRACTOR SHALL PROVIDE INSTRUCTION TO ALL MATERIAL SUPPLIERS REGARDING THE AIRPORT ACCESS PROCEDURES TO BE FOLLOWED BY THEIR DELIVERY DRIVERS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ESCORTS OF CONTRACTOR MANAGEMENT STAFF, VENDORS, SUBCONTRACTORS, VISITORS, DELIVERY DRIVERS, AND OTHERS UNDER THE AUTHORITY OF THE CONTRACTOR WHILE ON THE AIRPORT.

THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER AND THE OWNER WITHIN 10 DAYS AFTER SIGNING OF THE CONTRACT AND PRIOR TO THE START OF WORK A WRITTEN METHOD OF OPERATIONS DETAILING THE PRECAUTIONS HE/SHE PROPOSES FOR THE CONTROL OF VEHICLE TRAFFIC INCLUDING FLAG PERSONS, SIGNS, BARRICADES, ESCORTS AND ANY OTHER MEASURES HE/SHE PROPOSES. AFTER REVIEW AND COMMENT ON HIS/HER OPERATING PROCEDURES BY THE ENGINEER AND THE OWNER, THE CONTRACTOR SHALL FOLLOW THEM EXPLICITLY. THE REVOCATION OF THE CLOSURE OF THE WORK AREA BY OWNER FOR NON-CONFORMANCE SHALL NOT CONSTITUTE A VALID REASON FOR EXTENDING THE CONTRACT TIME OR FOR ANY CLAIM FOR ADDITIONAL COMPENSATION BY THE CONTRACTOR.

ALL SECURITY ARRANGEMENTS SHALL BE SUBJECT TO THE APPROVAL OF THE OWNER.

THE CONTRACTOR'S PERSONNEL AND CONTRACTOR'S VEHICLES SHALL BE RESTRICTED AND SHALL REMAIN WITHIN THE WORK AREAS, HAUL AND ACCESS ROUTES, AND THE STAGING AREAS AS SHOWN ON THE CONTRACT PLANS.

AIR OPERATIONS AREA (AOA)

IN GENERAL, THE WORK ASSOCIATED WITH THIS PROJECT WILL REQUIRE THE CONTRACTOR TO BE NEAR OR WITHIN THE AIR OPERATIONS AREA (AOA). THE AOA IS ANY AREA OF AN AIRPORT USED OR INTENDED TO BE USED FOR LANDING, TAKEOFF, OR SURFACE MANEUVERING OF AIRCRAFT. AN AOA INCLUDES SUCH PAVED OR TURF AREAS THAT ARE USED OR INTENDED TO BE USED FOR THE UNOBSTRUCTED MOVEMENT OF AIRCRAFT IN ADDITION TO ITS ASSOCIATED RUNWAY, TAXIWAY, OR APRON.

FOR THIS PROJECT, THE CONTRACTOR SHALL KEEP HIS/HER PERSONNEL AND EQUIPMENT AT LEAST 200 FEET FROM THE CENTERLINE OF ACTIVE RUNWAYS AND AT LEAST 50 FEET FROM THE EDGE OF ACTIVE TAXIWAYS, TAXILANES, AND APRONS, UNLESS PREVIOUS WRITTEN APPROVAL HAS BEEN OBTAINED FROM THE OWNER AND AFTER ALL REQUIRED PRECAUTIONS ARE TAKEN.

THE CONTRACTOR SHALL FURNISH AVIATION UNICOM RADIO(S), PER SPECIFICATION SECTION M-001. THE CONTRACTOR SHALL MONITOR NEW BEDFORD GROUND FREQUENCY 121.9 MHZ DURING HOURS WHEN THE ATCT IS OPERATED AND NEW BEDFORD CTAF FREQUENCY 118.1 MHZ DURING ALL OTHER HOURS.

OPEN TRENCHES OR EXCAVATIONS

THE CONTRACTOR WILL NOT BE PERMITTED TO LEAVE ANY TRENCHES OR OTHER EXCAVATIONS OPEN AT NIGHT, ON WEEKENDS, OR AT OTHER TIMES WHEN THE CONTRACTOR IS NOT ON THE WORK SITE, UNLESS APPROVALS ARE RECEIVED BY THE AIRPORT ADMINISTRATION AND THE CONTRACTOR PROTECTS THE EXCAVATION WITH STEEL PLATES, BARRICADES, AND LIGHTING, AS APPROVED BY THE ENGINEER. IN ADDITION, NO EXCAVATION EXCEEDING 3 INCHES IN DEPTH SHALL BE LEFT OPEN WITHIN AIRCRAFT OPERATION AREAS, AS DESCRIBED ABOVE, WHILE THE RUNWAY, TAXIWAY, AND APRONS ARE IN USE UNLESS THE EXCAVATIONS ARE COVERED WITH APPROVED STEEL PLATES. STEEL PLATES SHALL BE CAPABLE OF BEARING THE HEAVIEST AIRCRAFT/VEHICLE USING THE AIRPORT OVER THE SPAN IN WHICH THEY ARE TO BE USED. THE CONTRACTOR SHALL KEEP THE LENGTH OF OPEN TRENCHES COVERED WITH STEEL PLATES TO A MINIMUM BUT IN NO CASE SHALL THE LENGTH EXCEED THE DISTANCE BETWEEN TWO ADJACENT MANHOLES, CATCH BASINS, OR STRUCTURES.

ALL EXCAVATIONS SHALL BE BACK FILLED, COMPACTED AND THE PAVEMENT REPAIRED AND PROPERLY CURED PRIOR TO THE AREA BEING REOPENED TO TRAFFIC.

PRIOR TO THE CLOSE OF WORK EACH DAY, THE CONTRACTOR SHALL ENSURE THAT THE WORK AREAS TO BE REOPENED WITHIN AIRCRAFT OPERATION AREAS, AS DESCRIBED ABOVE, ARE GRADED AWAY FROM THE PAVEMENTS AT A MAXIMUM SLOPE OF 5% AND SHALL BE LEFT IN SUCH CONDITION THAT IT WILL DRAIN READILY AND EFFECTIVELY AND WILL NOT POSE A HAZARD TO VEHICLES OR AIRCRAFT. NO PILES OF SOIL SHALL BE LEFT UNSPREAD, NO SHARP CHANGES IN GRADE WILL BE PERMITTED, AND THE SURFACE SHALL BE THOROUGHLY COMPACTED.

DEBRIS AND DUST CONTROL

THE CONTRACTOR SHALL STRICTLY CONTROL DEBRIS AND LITTER AT AIRPORT WORK SITES. MUD, STONES OR OTHER DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS SHALL BE PROMPTLY AND COMPLETELY REMOVED FROM ALL PAVEMENTS TO FACILITATE DAILY AIRCRAFT OPERATIONS AND A CLEAN ENVIRONMENT. DUST CONTROL MEASURES SHALL BE TAKEN AS NECESSARY BY THE CONTRACTOR TO ENSURE THAT NO DUST IS

ALLOWED TO DRIFT INTO ANY AOA, INTO LOCATIONS WHERE AIRCRAFT ARE PARKED AT ANY TIME, OR SURROUNDING RESIDENCES. THE CONTRACTOR SHALL ENSURE THAT ALL PUBLIC ROADS ARE CONTINUOUSLY MAINTAINED FREE OF MUD AND DEBRIS THAT MAY RESULT FROM HIS OPERATIONS. DEBRIS AND DUST CONTROL MEASURES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL REFER TO THE NPDES GENERAL PERMIT AND CONSTRUCTION SWPPP AS PREPARED BY THE ENGINEER FOR ADDITIONAL REQUIREMENTS. THE CONTRACTOR SHALL PROVIDE A MANNED VAC SWEEPER DURING ALL TIMES WHEN ACTIVE AOA PAVEMENTS ARE CROSSED AT NO ADDITIONAL COST TO THE OWNER.

DISPOSAL OF SURPLUS AND UNSUITABLE MATERIALS

ALL UNSUITABLE SURPLUS EXCAVATED MATERIAL, INCLUDING ALL STRUCTURES AND PIPES, ALL BITUMINOUS CONCRETE PAVEMENT, AND ALL PORTLAND CEMENT CONCRETE PAVEMENT SHALL BE LEGALLY DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY UNLESS SPECIFIC INSTRUCTIONS TO THE CONTRARY ARE INCLUDED IN THE PLANS AND SPECIFICATIONS. THE CONTRACTOR SHALL OBTAIN ALL PERMITS AND PAY ALL FEES REQUIRED FOR THE DISPOSAL.

ALL EXCESS MATERIAL OTHER THAN TOPSOIL SHALL BE LEGALLY DISPOSED OF OFF SITE. NO SEPARATE MEASUREMENT FOR PAYMENT SHALL BE MADE FOR DISPOSAL OF MATERIAL BUT RATHER THE DISPOSAL SHALL BE INCIDENTAL TO THE ITEM THAT GENERATES THE MATERIAL TO BE DISPOSED OF.

CONTRACTOR'S STAGING AREAS

THE CONTRACTOR SHALL USE THE AREAS SHOWN ON THE PLANS FOR HIS/HER STAGING AREAS. NO OTHER AREAS ARE APPROVED HEREIN WITHOUT THE EXPLICIT CONSENT OF THE AIRPORT MANAGER AND THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR ANY AND ALL IMPROVEMENT AND RESTORATION OF THE DESIGNATED AREAS SUCH AS GRUBBING, GRADING, AND CONSTRUCTION OF STABILIZED ACCESS ROADS, THAT IS NECESSARY FOR THE UTILIZATION OF THE AREAS. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR ANY TEMPORARY ACCESS PERMITS AND ASSOCIATED FEES FOR ACCESS TO THE ADJACENT ROAD NETWORK. THERE WILL BE NO SEPARATE PAYMENT FOR THIS WORK. ALL WORK NECESSARY TO USE THE STAGING AREAS SHALL BE DEEMED INCIDENTAL TO THE OVERALL PROJECT.

UNDERGROUND UTILITIES AND CABLES

THE APPROXIMATE LOCATIONS OF KNOWN UTILITIES AND UNDERGROUND CABLES ARE SHOWN ON THE CONTRACT DRAWINGS. PRIOR TO COMMENCEMENT OF ANY EXCAVATION, THE CONTRACTOR SHALL COORDINATE ALL WORK ON AND IN THE VICINITY OF THE UNDERGROUND UTILITIES AND CABLES WITH THE FOLLOWING AGENCIES AS APPROPRIATE.

- DIG SAFE (ON TARGET) AND INDICATED UTILITIES
- NEW BEDFORD REGIONAL AIRPORT - AIRPORT ADMINISTRATION
- CITY OF NEW BEDFORD DEPARTMENT OF PUBLIC INFRASTRUCTURE (DPI)
- VERIZON
- COMCAST
- NSTAR GAS COMPANY
- NSTAR ELECTRIC
- SPECTRA GAS COMPANY
- FEDERAL AVIATION ADMINISTRATION (FAA) AIRWAY FACILITIES

THE CONTRACTOR SHALL EMPLOY THE SERVICES OF A UTILITY LOCATING CONTRACTOR TO MARK OUT ALL EXISTING UTILITIES, AS NEEDED. THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE ACTUAL LOCATION OF ALL UNDERGROUND UTILITIES IN THE PROJECT AREA, NOT LIMITED TO THOSE LISTED ABOVE, AND ADJUSTING THEIR WORK METHODS TO AVOID DAMAGE TO SAID UTILITIES.

THE CONTRACTOR SHALL REPAIR, AT HIS/HER OWN EXPENSE, ANY UNDERGROUND OR OVERHEAD CABLES OR UTILITIES DAMAGED BY HIS/HER OPERATIONS AT NO ADDITIONAL COST TO THE OWNER INCLUDING ANY DAMAGE DONE BY DRIVING HIS/HER EQUIPMENT OVER EXISTING UNDERGROUND CABLES OR UTILITIES. THE REPAIR OF FAA CABLES SHALL BE INSPECTED AND APPROVED BY THE FAA.

PERMITS

THE CONTRACTOR SHALL REFER TO, FULLY UNDERSTAND, AND PROVIDE ACKNOWLEDGEMENT OF THE FOLLOWING NEW BEDFORD REGIONAL AIRPORT DOCUMENTS, REPORTS, PLANS AND PERMITS PRIOR TO PROCEEDING WITH THE PROJECT:

- 2015 - STORMWATER POLLUTION PREVENTION PLAN (SWPPP) - EPA NPDES GENERAL PERMIT FOR CONSTRUCTION
- NEW BEDFORD CONSERVATION COMMISSION ORDER OF CONDITIONS

WETLANDS

DISTURBANCE OF WETLANDS IS A POTENTIAL VIOLATION OF FEDERAL, STATE AND LOCAL REGULATIONS. THE CONTRACTOR SHALL TAKE PRECAUTIONS TO COMPLY WITH FEDERAL, STATE, AND LOCAL PROTECTION REGULATIONS INCLUDING BUT NOT LIMITED TO STAKING OUT THE AERIAL EXTENT OF WETLAND RESOURCES LOCATED WITHIN 50 FEET OF THE WORK. WETLAND BOUNDARY DELINEATION, APPROVED BY THE NEW BEDFORD CONSERVATION COMMISSION WILL BE SUPPLIED BY THE ENGINEER OR OWNER. THE CONTRACTOR SHALL PAY ALL FINES ASSESSED AGAINST THE AIRPORT FOR UNAUTHORIZED WETLAND DISTURBANCE AND RELATED EXPENSES DUE TO VIOLATIONS CAUSED BY THE CONTRACTOR AND HIS/HER PERSONNEL, SUBCONTRACTORS AND VENDORS.

SWPPP

IN 2015, THE NEW BEDFORD REGIONAL AIRPORT DEVELOPED A COMPREHENSIVE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) AS A MEANS TO ACHIEVE COMPLIANCE WITH THE EPA'S NPDES GENERAL PERMIT FOR STORMWATER DISCHARGES DURING CONSTRUCTION ACTIVITIES. THE AIRPORT HAS THE AUTHORITY TO INSPECT THE CONTRACTOR'S OPERATION WHILE OPERATING ON THE NEW BEDFORD REGIONAL AIRPORT PROPERTY AND IMPOSE EQUIPMENT FUELING AND MAINTENANCE RESTRICTIONS AND REQUIRE ADDITIONAL EROSION CONTROL BMP'S AND OTHER PROTECTIVE MEASURES TO BE INSTALLED TO REMAIN IN COMPLIANCE WITH THE EPA UNDER THE AIRPORTS SWPPP. THE CONTRACTOR, AS THE SITE "OPERATOR", SHALL BE REQUIRED TO ADHERE TO THE SWPPP FOR CONSTRUCTION AS DEVELOPED BY THE AIRPORT'S ENGINEER AND SHALL BE REQUIRED TO SIGN THE SWPPP AND THE INSPECTION REPORTS PRODUCED BY THE ENGINEER.

INDEX TO DRAWINGS

SHEET NO.	DWG NO.	SHEET TITLE
1	G1.1	COVER SHEET
2	G1.2	TERMINAL APRON NOTES AND INDEX TO DRAWINGS
4	G1.4	GENERAL PLAN
5	C1.1	TERMINAL AREA APRON - PHASE I PLAN
6	C2.1	DRAINAGE DETAILS (1 OF 3)
7	C2.2	DRAINAGE DETAILS (2 OF 3)
8	C2.3	DRAINAGE DETAILS (3 OF 3)

LEGEND

	38	EXISTING MAJOR CONTOUR
	38	PROPOSED MAJOR CONTOUR
		BORDERING VEGETATIVE WETLANDS LIMIT
		OPEN WATER
	25'	25' WETLAND BUFFER ZONE
	100'	100' WETLAND BUFFER ZONE
		CONTRACTOR'S STAGING AREA
		CONTRACTOR HAUL ROAD
	RSA	RUNWAY SAFETY AREA (FAA SAFETY SURFACE)
	OFZ	OBJECT FREE ZONE (FAA SAFETY SURFACE)
	ROFA	RUNWAY OBJECT FREE AREA (FAA SAFETY SURFACE)
	PART77	FAR PART77 34:1 APPROACH SURFACE
	TYPE4	FAA TYPE4 20:1 APPROACH SURFACE
		COMPOST FILTER SOCK
		WETLAND FLAGGING
		DRAINAGE PIPE
		DRAIN MANHOLE / HYDRODYNAMIC SEPARATOR
		CATCH BASIN
		NON-AIRPORT PROPERTY LINE
		WETLAND DELINEATION



NO.	DATE	DESCRIPTION	BY
5	8/3/17	REVISOR WETLAND BUFFER ZONE SHEETS AND 2000'S WETLAND REGULATION AREA ANNOT.	MMO

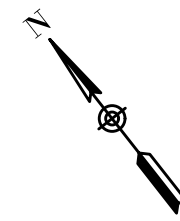
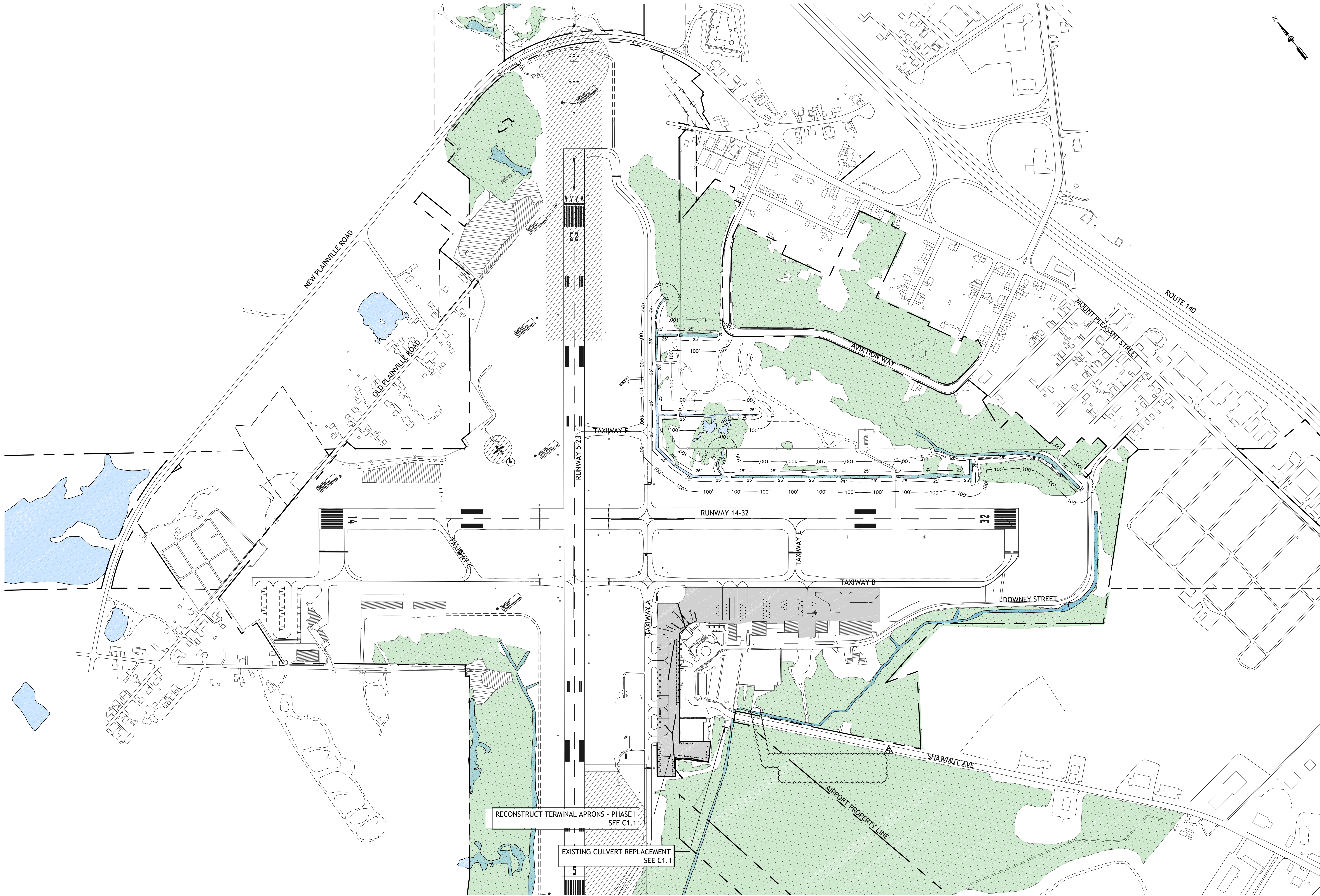
PROJECT	OWNER
RECONSTRUCT TERMINAL APRONS PHASE I	NEW BEDFORD AIRPORT COMMISSION NEW BEDFORD REGIONAL AIRPORT NEW BEDFORD, MASSACHUSETTS

PROJECT NO.	103-032
CADD FILE	SHEET_GENERAL
DESIGNED BY	MMO
DRAWN BY	MMO
CHECKED BY	RAL
DATE	SEPTEMBER 2016
DRAWING SCALE	N.T.S.

SHEET TITLE	GRAPHIC SCALE
TERMINAL APRON NOTES AND INDEX TO DRAWINGS	NOT TO SCALE

DRAWING NO.
G1.2
2 OF 8

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NO.	DATE	DESCRIPTION	BY
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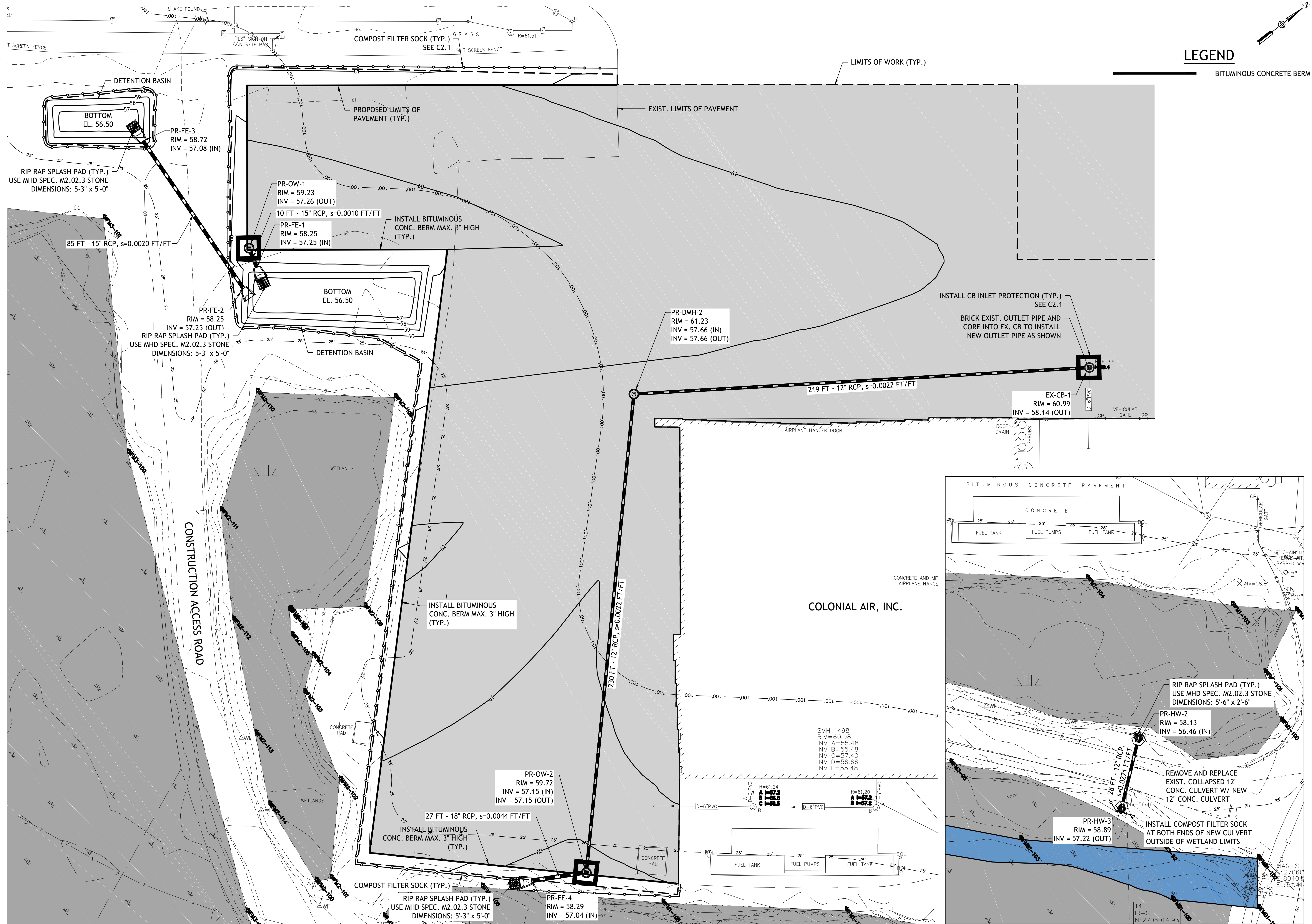
PROJECT	OWNER
RECONSTRUCT TERMINAL APRONS PHASE I	NEW BEDFORD AIRPORT COMMISSION NEW BEDFORD REGIONAL AIRPORT NEW BEDFORD, MASSACHUSETTS


PROJECT NO.	103-032
CADD FILE	SHEET_GENERAL
DESIGNED BY	MKO
DRAWN BY	MKO
CHECKED BY	RAL
DATE	SEPTEMBER 2016
DRAWING SCALE	1" = 300'

SHEET TITLE	GENERAL PLAN
GRAPHIC SCALE	0 150 300 600

DRAWING NO.	G1.4
4 OF 8	

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LEGEND

BITUMINOUS CONCRETE BERM

AIRPORT SOLUTIONS GROUP
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NO.	DATE	DESCRIPTION	BY
1	10/19/16	Added compost filter sock and bit. conc. berm.	MKO
2	12/15/16	Added stone size and rip rap pad dimensions.	MKO
3	1/25/17	Added drainage systems, revised pavement layout and adjusted grading and drainage.	MKO
4	5/18/17		MKO
5	8/31/17		MKO

PROJECT	OWNER
RECONSTRUCT TERMINAL APRONS PHASE I	NEW BEDFORD AIRPORT COMMISSION NEW BEDFORD REGIONAL AIRPORT NEW BEDFORD, MASSACHUSETTS

PROJECT NO.	SHEET GRADING
103-032	MKO
CADD FILE	DESIGNED BY
	MKO
DRAWN BY	CHECKED BY
	RAL
DATE	DRAWING SCALE
SEPTEMBER 2016	1" = 20'

SHEET TITLE

TERMINAL AREA APRON PHASE I PLAN

DRAWING NO.

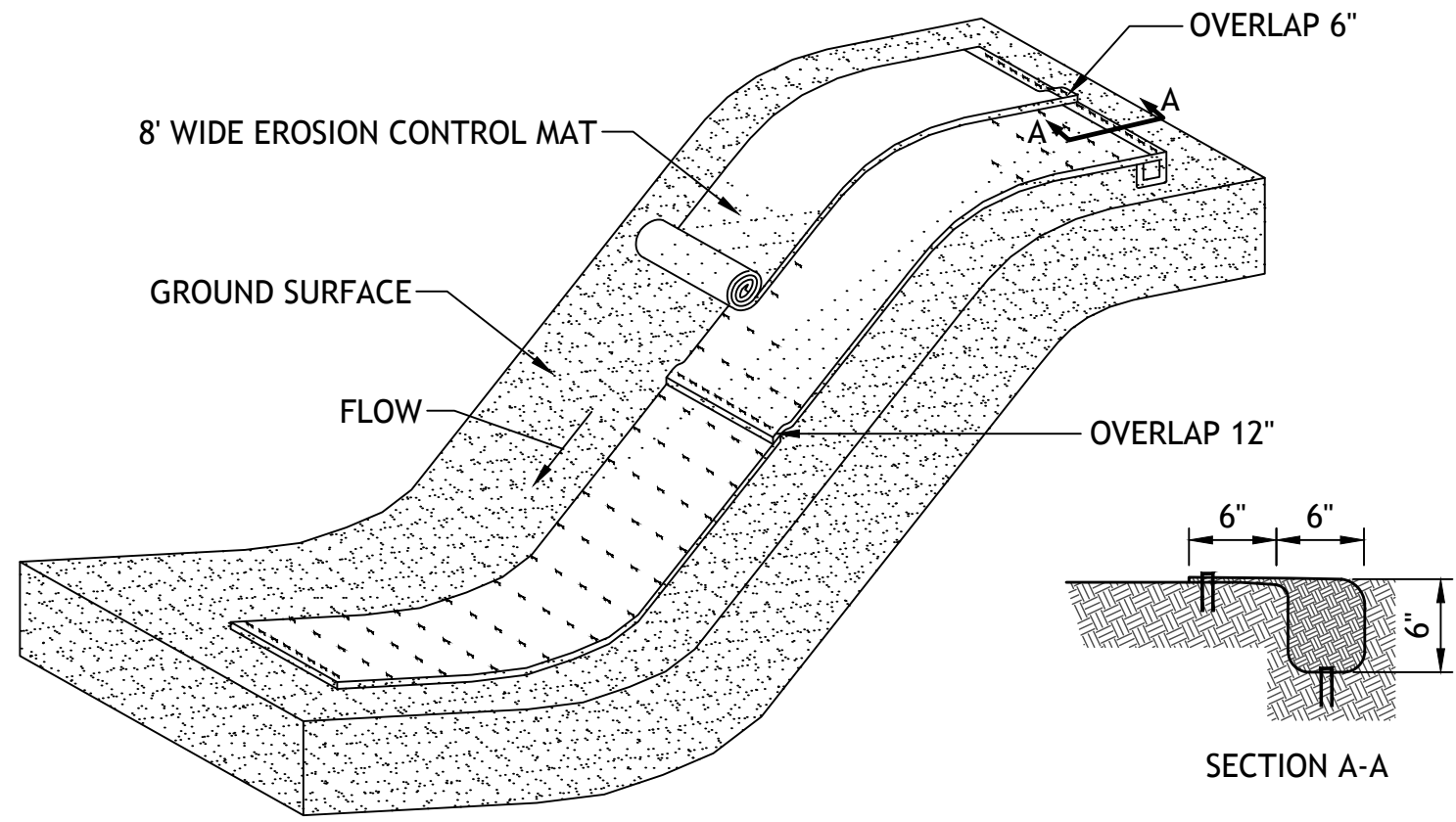
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5 OF 8

GRAPHIC SCALE

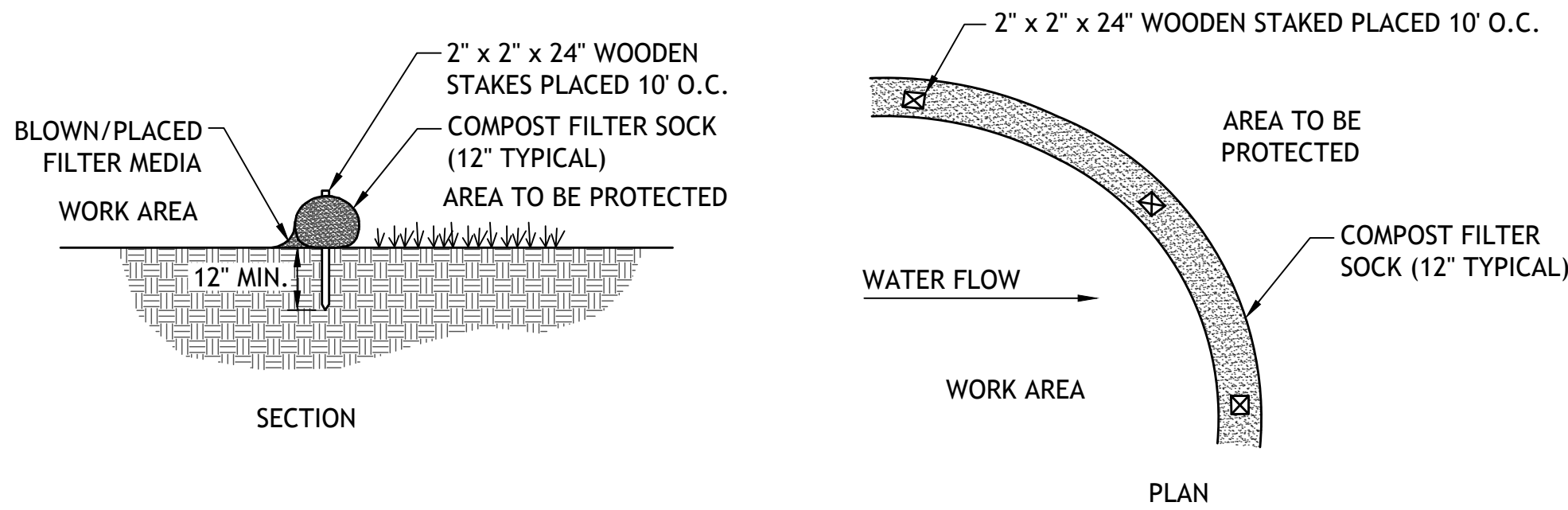
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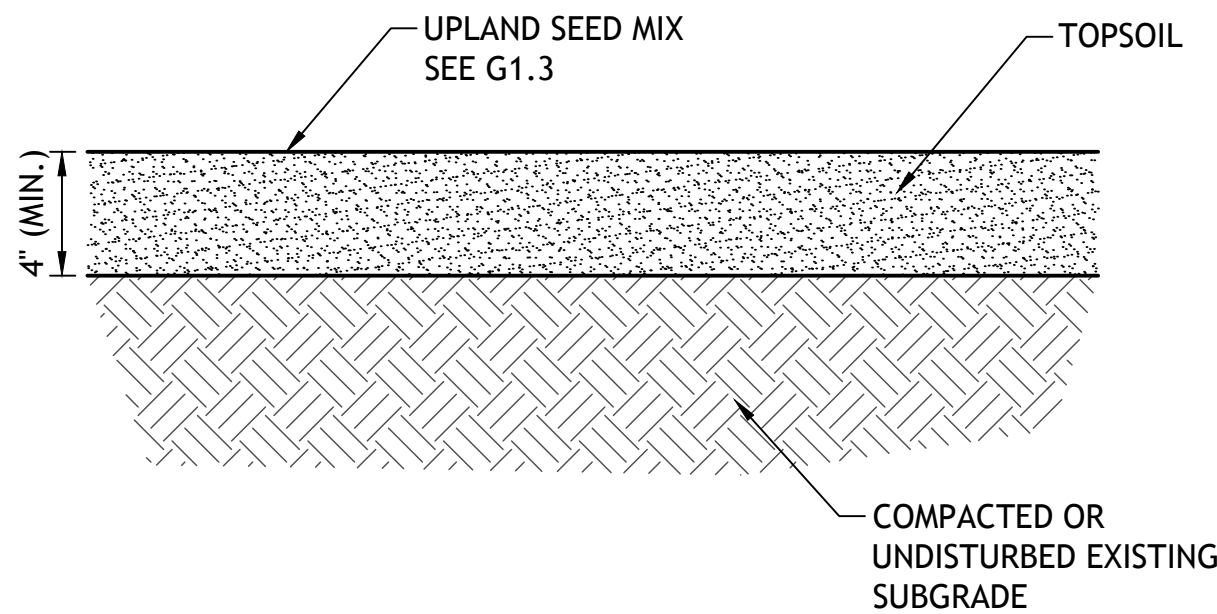


- EROSION CONTROL MATTING NOTES:**
1. PRIOR TO PLACING EROSION CONTROL MATTING, PREPARE THE SOIL BY RAKING AREA FREE OF CLODS AND LARGE STONES.
 2. SEED, MULCH AND FERTILIZER SHALL BE DISTRIBUTED AS SPECIFIED OVER THE PREPARED SOIL PRIOR TO PLACING THE EROSION CONTROL MATTING.
 3. ALL SEAMS SHALL BE OVERLAPPED A MINIMUM OF 12" AND SECURED WITH STAPLES 18" ON CENTER.
 4. TO SECURE MAT TO GROUND, STAPLE RANDOMLY AT 24" INTERVALS THROUGHOUT, OR AT 12" AT ENDS
 5. APPROXIMATELY 200 STAPLES PER ROLL
 6. PLACE EROSION CONTROL MATTING AS DIRECTED BY THE ENGINEER, ON SLOPES GREATER THAN 5:1, AND AS SHOWN ON THESE C4.1 AND W1.1.

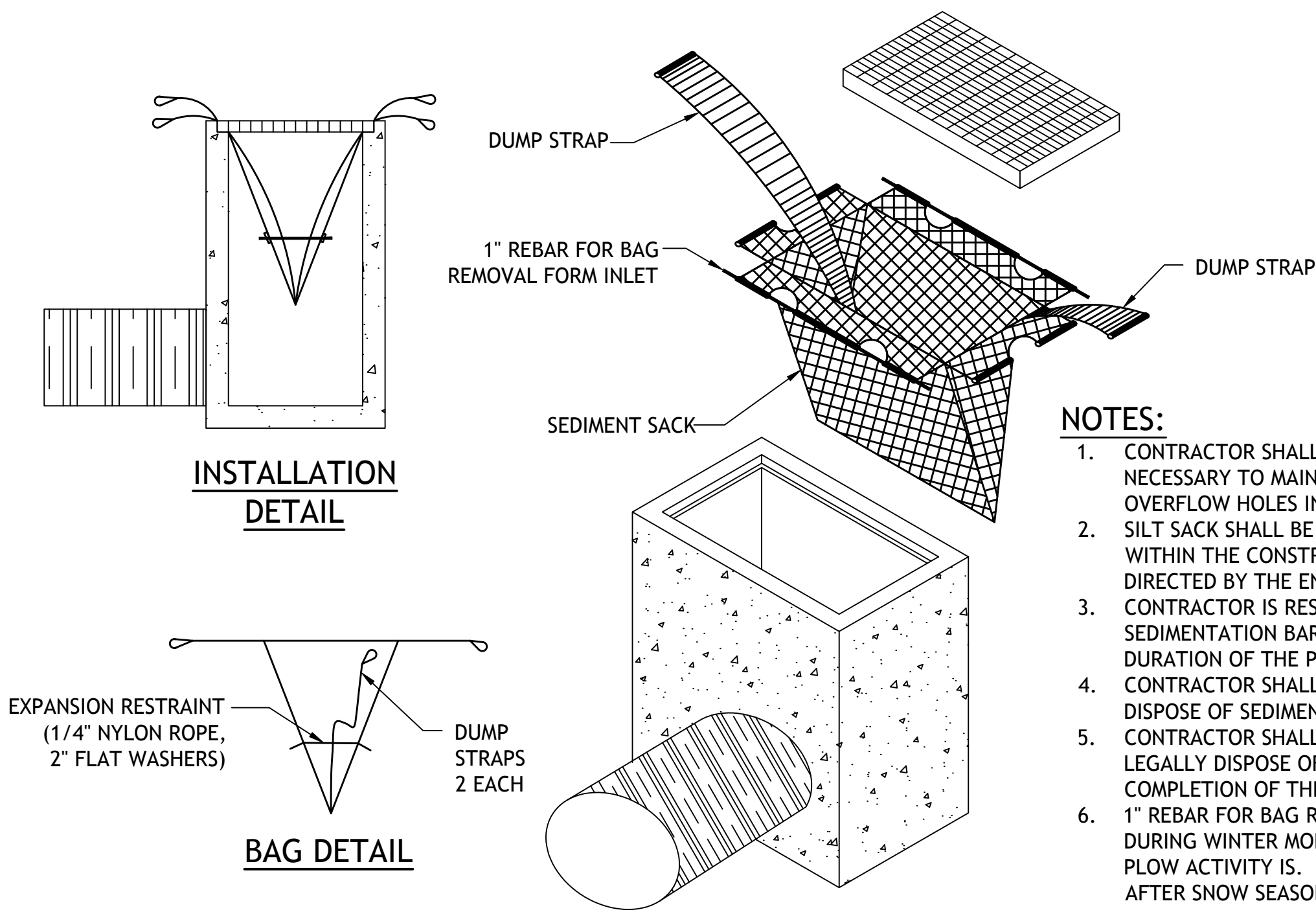
EROSION CONTROL MATTING DETAIL
SCALE: N.T.S.



COMPOST FILTER SOCK DETAIL
SCALE: N.T.S.

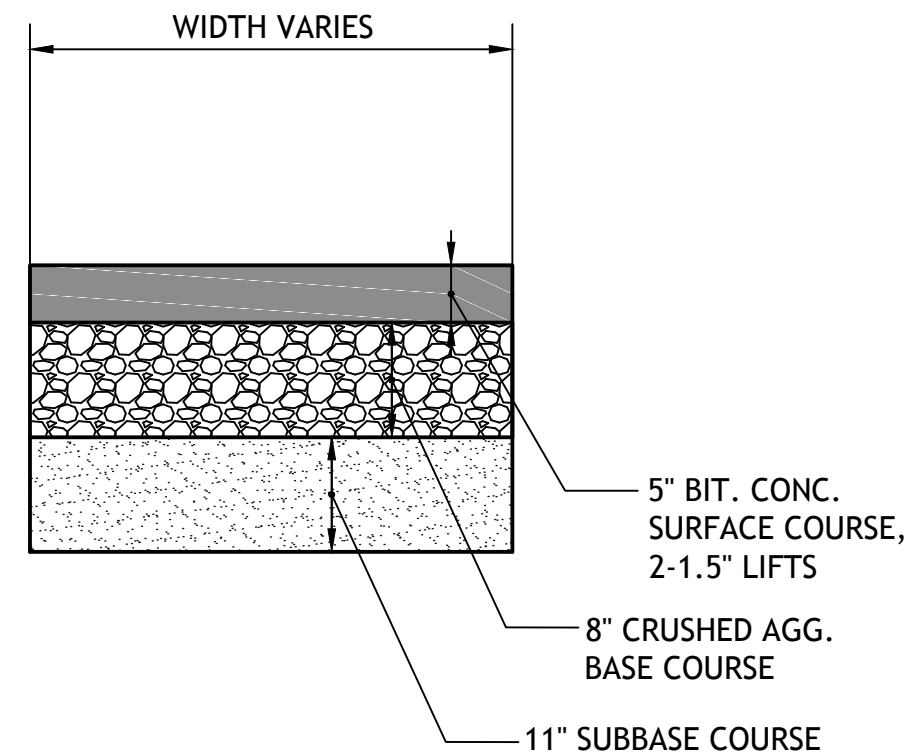


TOPSOIL AND SEED DETAIL
SCALE: N.T.S.



- NOTES:**
1. CONTRACTOR SHALL REMOVE SEDIMENT AS NECESSARY TO MAINTAIN LEVEL BELOW OVERFLOW HOLES IN SILT SACK.
 2. SILT SACK SHALL BE USED ON ALL CATCH BASINS WITHIN THE CONSTRUCTION ZONE OR AS DIRECTED BY THE ENGINEER.
 3. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING SEDIMENTATION BARRIERS THROUGHOUT THE DURATION OF THE PROJECT.
 4. CONTRACTOR SHALL REMOVE AND LEGALLY DISPOSE OF SEDIMENT AS REQUIRED.
 5. CONTRACTOR SHALL REMOVE SILT SACKS AND LEGALLY DISPOSE OF THEM OFF-SITE, UPON COMPLETION OF THE PROJECT AND AS REQUIRED.
 6. 1" REBAR FOR BAG REMOVAL SHALL BE REMOVED DURING WINTER MONTHS AT LOCATIONS WHERE PLOW ACTIVITY IS. BARS SHALL BE REINSTALLED AFTER SNOW SEASON IS COMPLETE.

CATCH BASIN INLET PROTECTION DETAIL
SCALE: N.T.S.



APRON TYPICAL SECTION
SCALE: N.T.S.



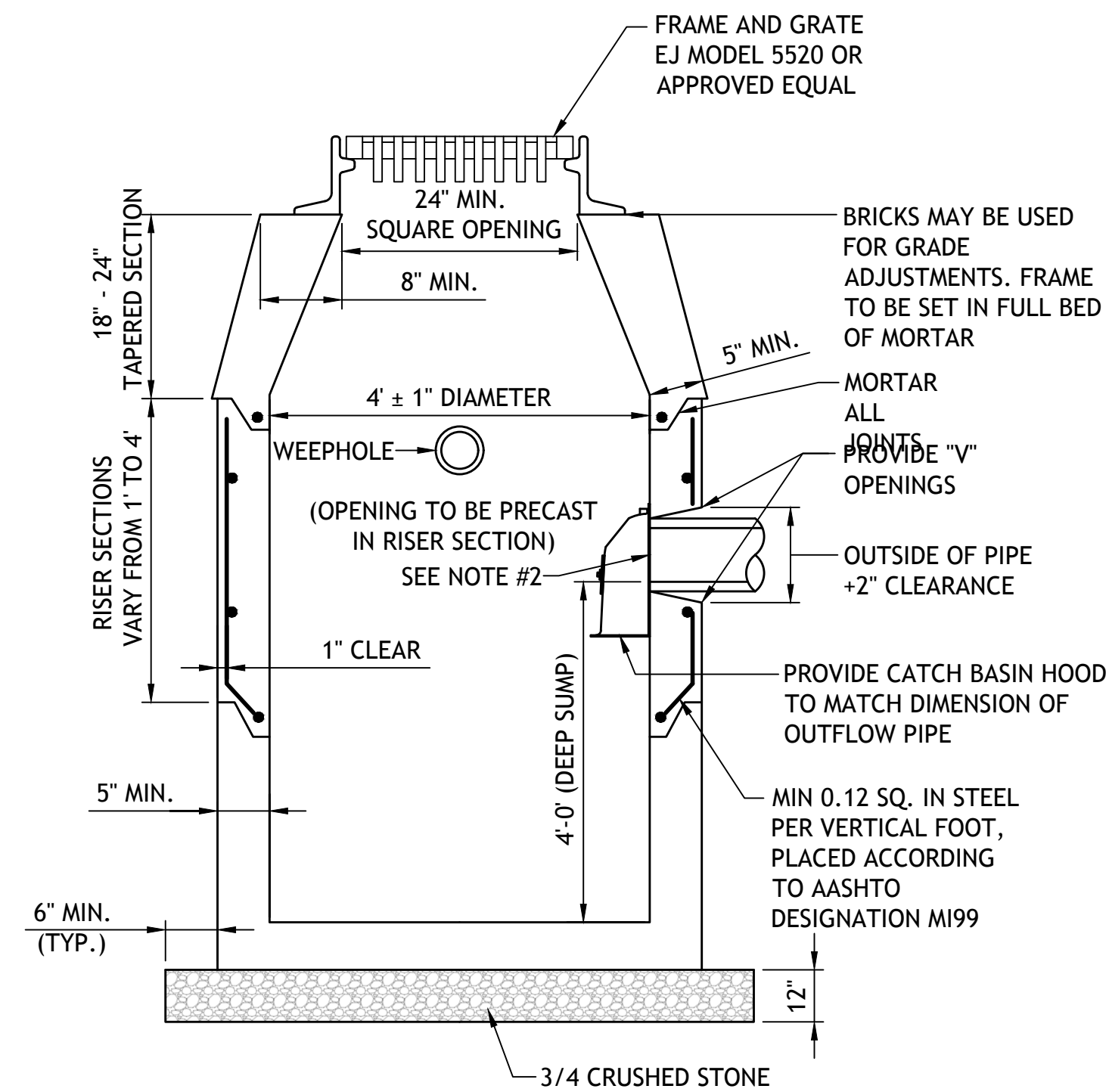
NO.	DATE	DESCRIPTION	BY
1	12/2016	ADDED PAVEMENT SECTION (MKO)	

PROJECT	OWNER
RECONSTRUCT TERMINAL APRONS PHASE I	NEW BEDFORD AIRPORT COMMISSION NEW BEDFORD REGIONAL AIRPORT NEW BEDFORD, MASSACHUSETTS

PROJECT NO.	103-032
CADD FILE	SHEET_DETAILS
DESIGNED BY	MKO
DRAWN BY	MKO
CHECKED BY	RAL
DATE	SEPTEMBER 2016
DRAWING SCALE	N.T.S.

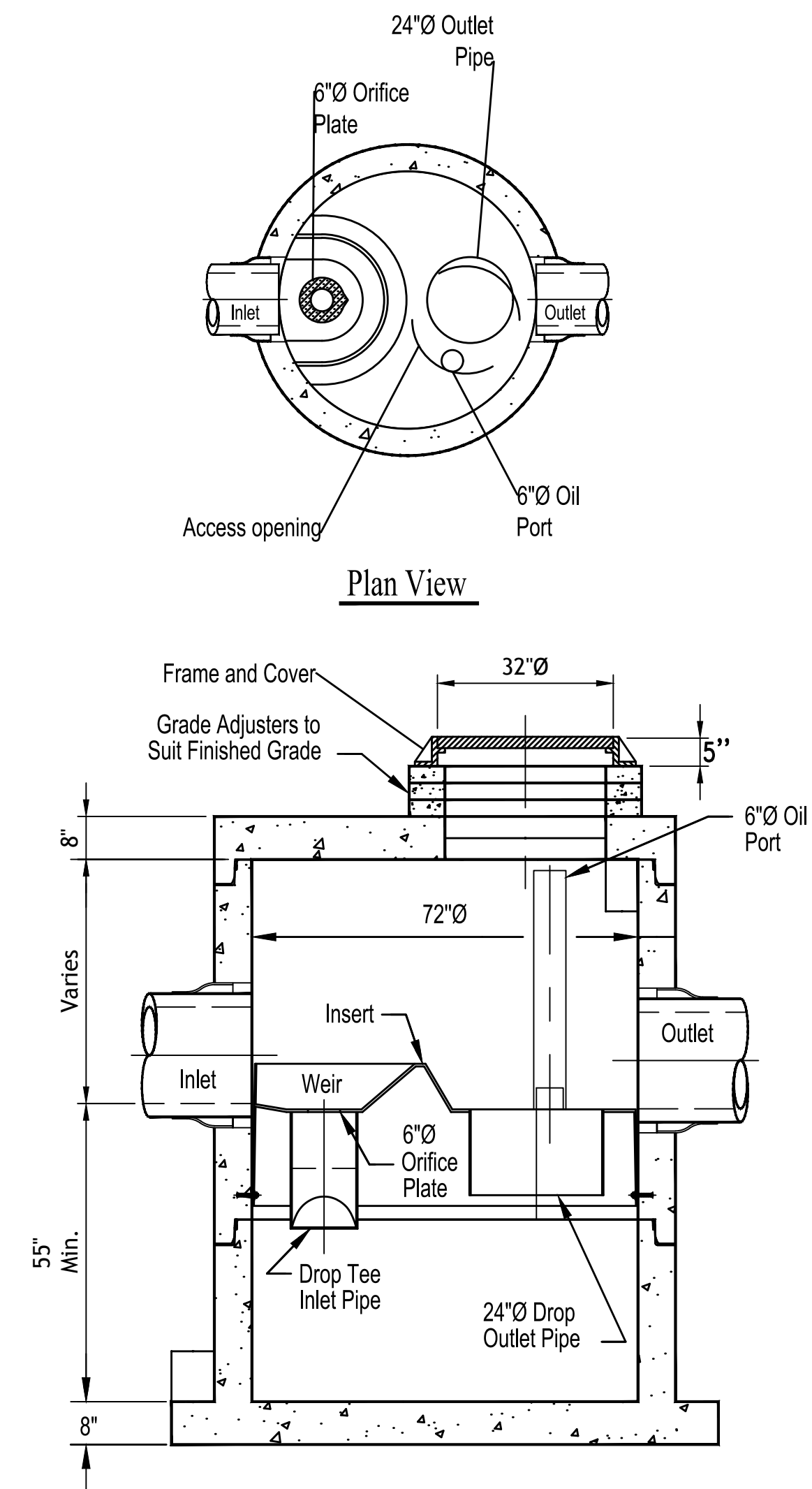
SHEET TITLE	GRAPHIC SCALE
DRAINAGE DETAILS (1 OF 3)	NOT TO SCALE

DRAWING NO.
C2.1
6 OF 8



DEEP SUMP CONCRETE CATCH BASIN DETAIL

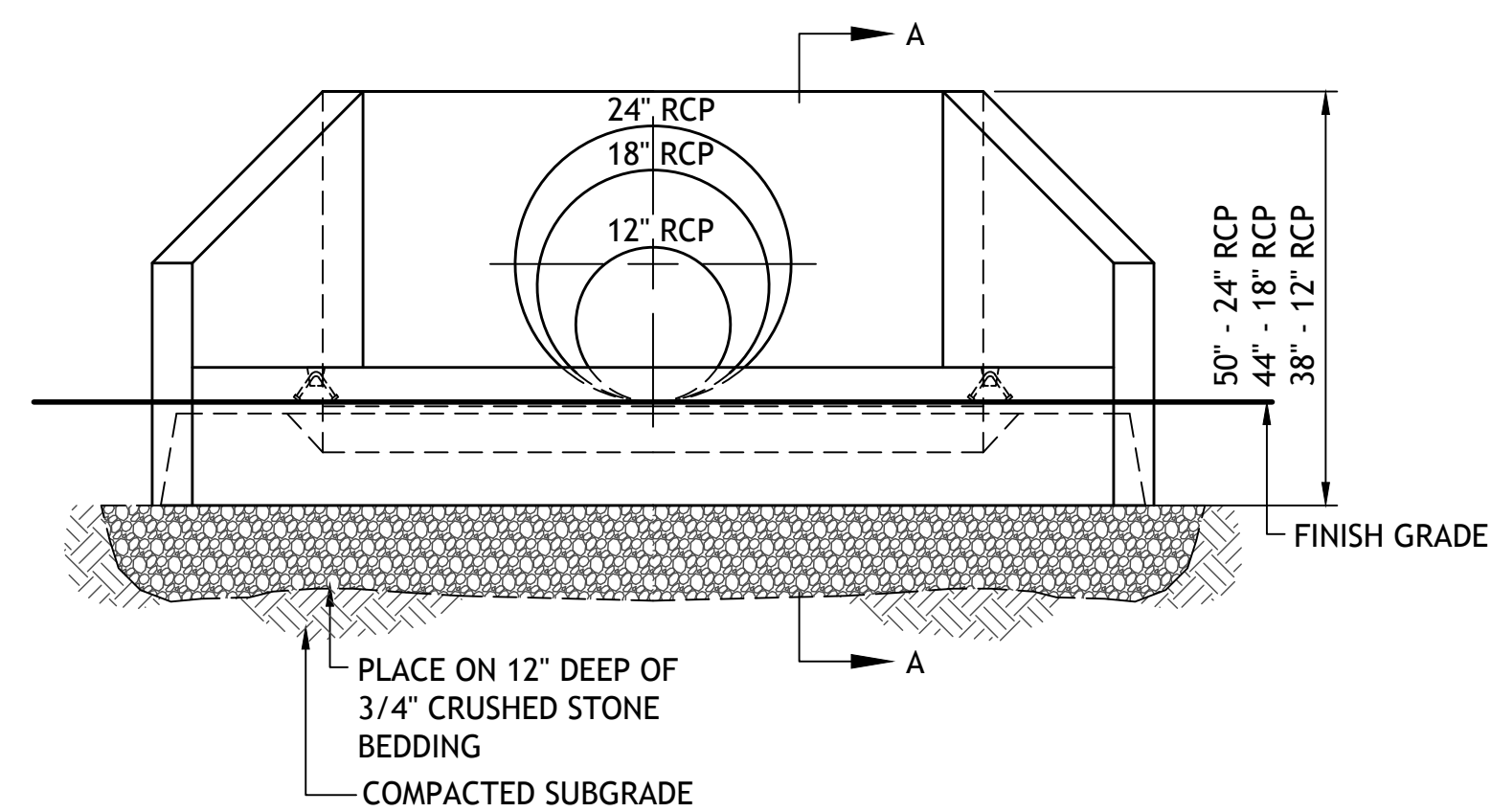
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Section Thru Chamber

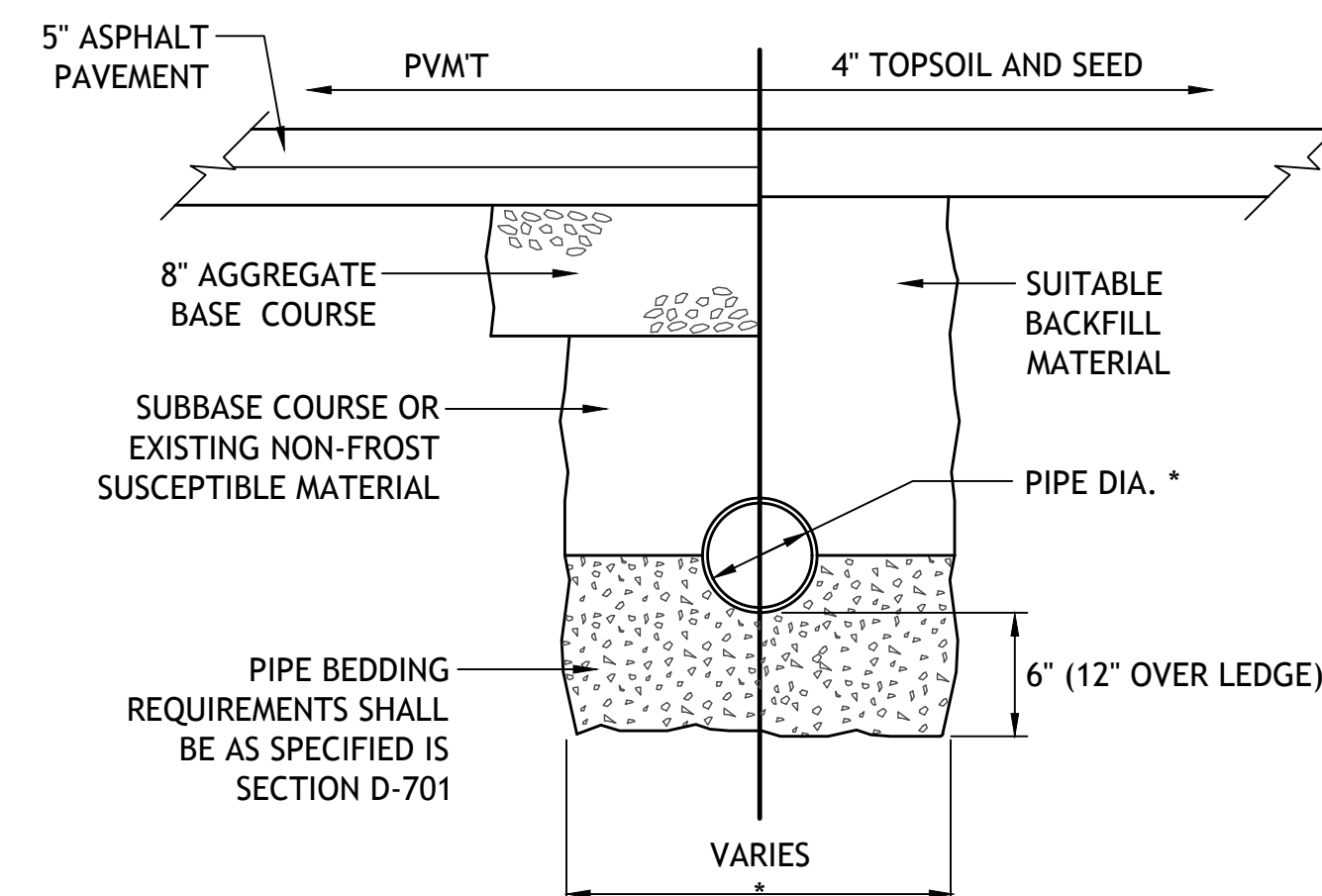
HYDRODYNAMIC SEPARATOR DETAIL

SCALE: N.T.S.



CONCRETE HEADWALL DETAIL

SCALE: N.T.S.

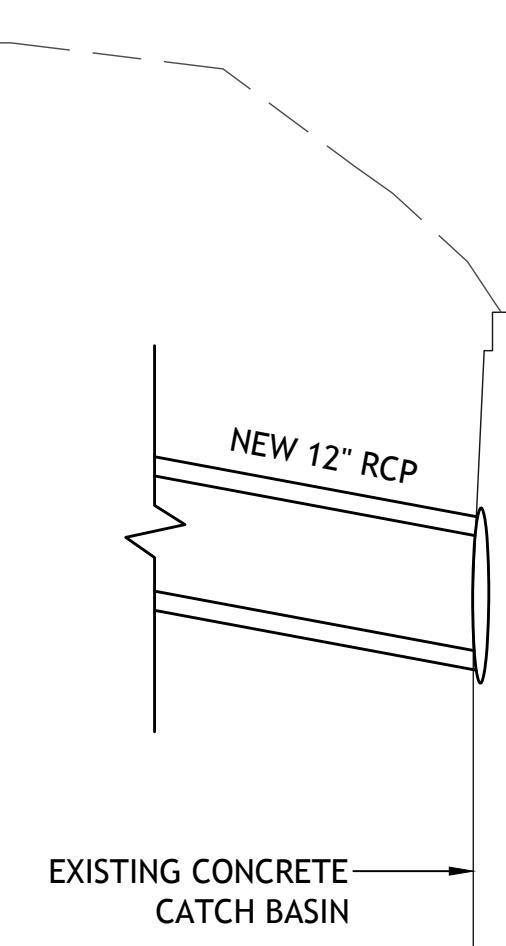


* PIPE DIA.	TRENCH WIDTH
UP TO 12"	36"
12" TO 24"	I.D. PLUS 24"
GREATER THAN 24"	2 x I.D.

NOTE:
CONTRACTOR SHALL SHORE
TRENCH SIDES WHEN REQUIRED.

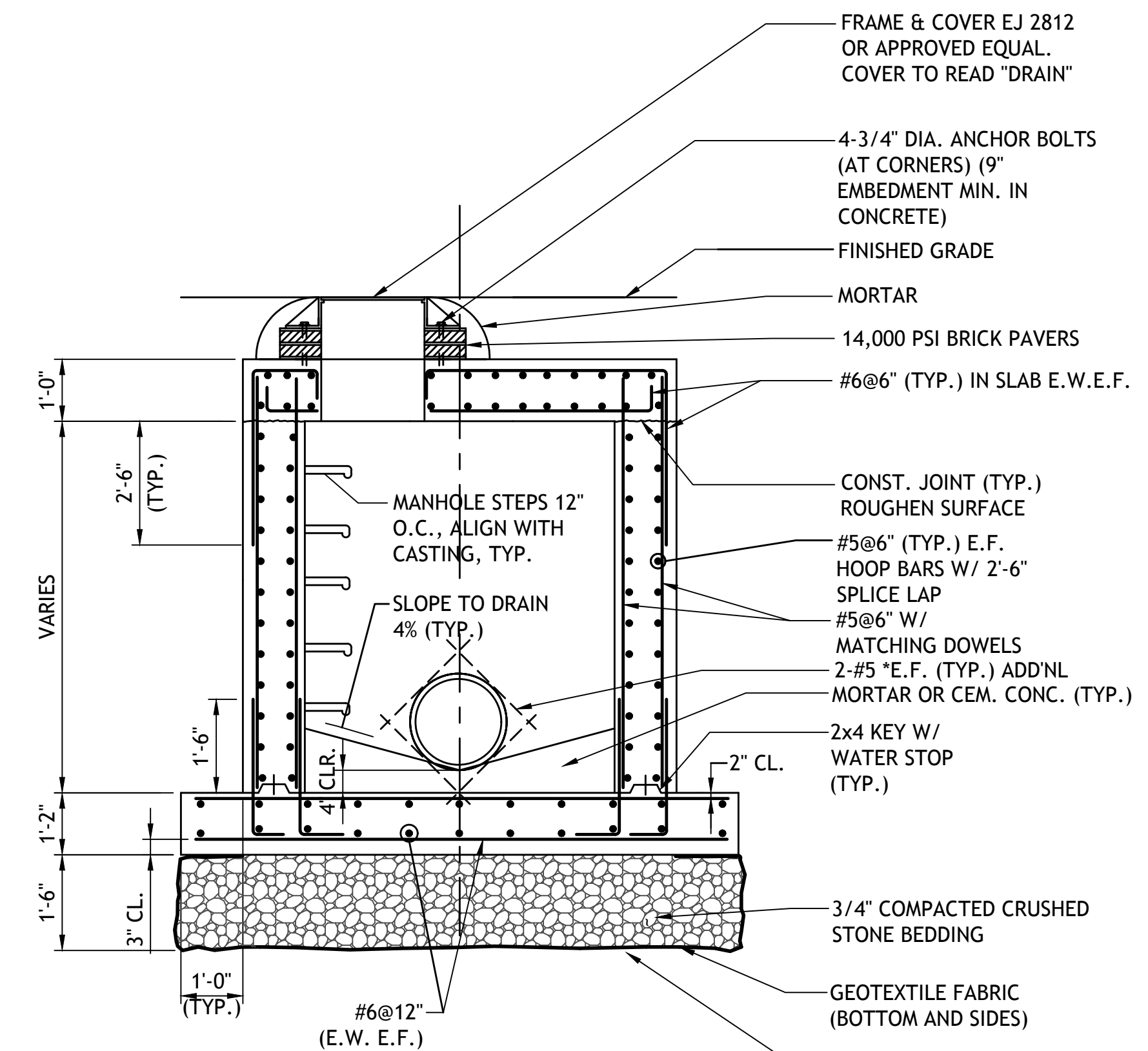
TYPICAL DRAINAGE PIPE TRENCH DETAIL

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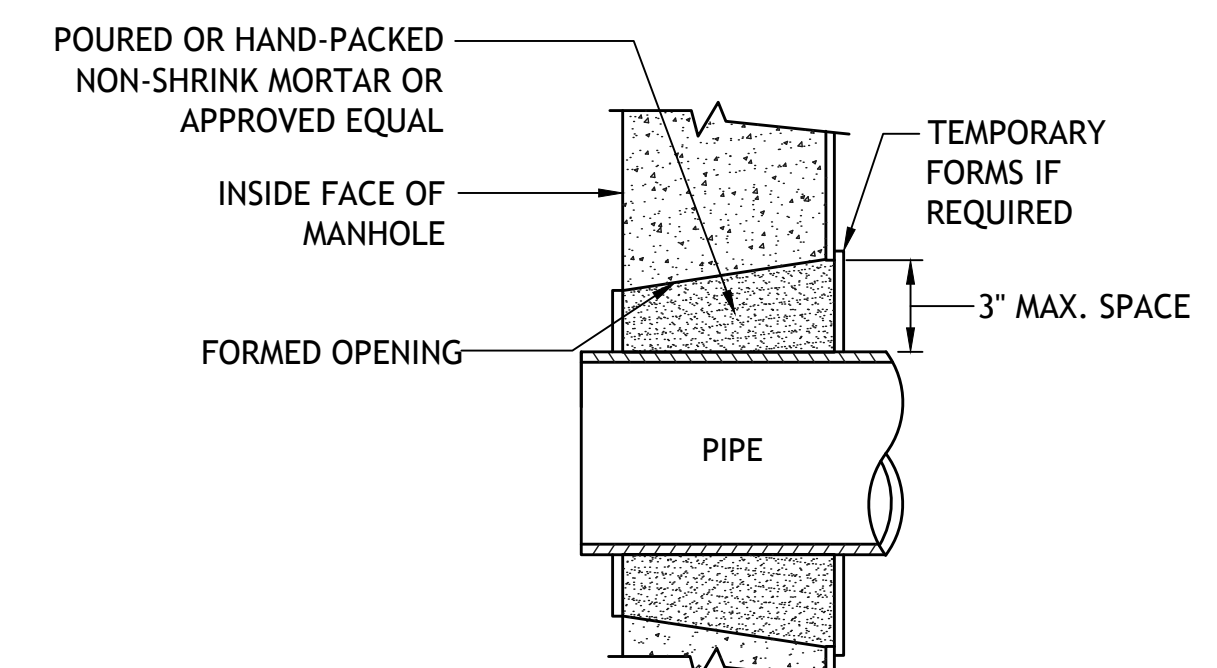
CORE EXISTING CATCH BASIN DETAIL

SCALE: N.T.S.



DRAIN MANHOLE DETAIL

SCALE: N.T.S.



MORTAR PIPE CONNECTION AT STRUCTURES

SCALE: N.T.S.

[illegible]


PROJECT	RECONSTRUCT TERMINAL APRONS PHASE I
OWNER	NEW BEDFORD AIRPORT COMMISSION NEW BEDFORD REGIONAL AIRPORT NEW BEDFORD, MASSACHUSETTS

PROJECT NO.	103-032
CADD FILE	SHEET_DETAILS
DESIGNED BY	MKO
DRAWN BY	MKO
CHECKED BY	RAL
DATE	SEPTEMBER 2016
DRAWING SCALE	N.T.S.

SHEET TITLE

DRAINAGE DETAILS

(2 OF 3)



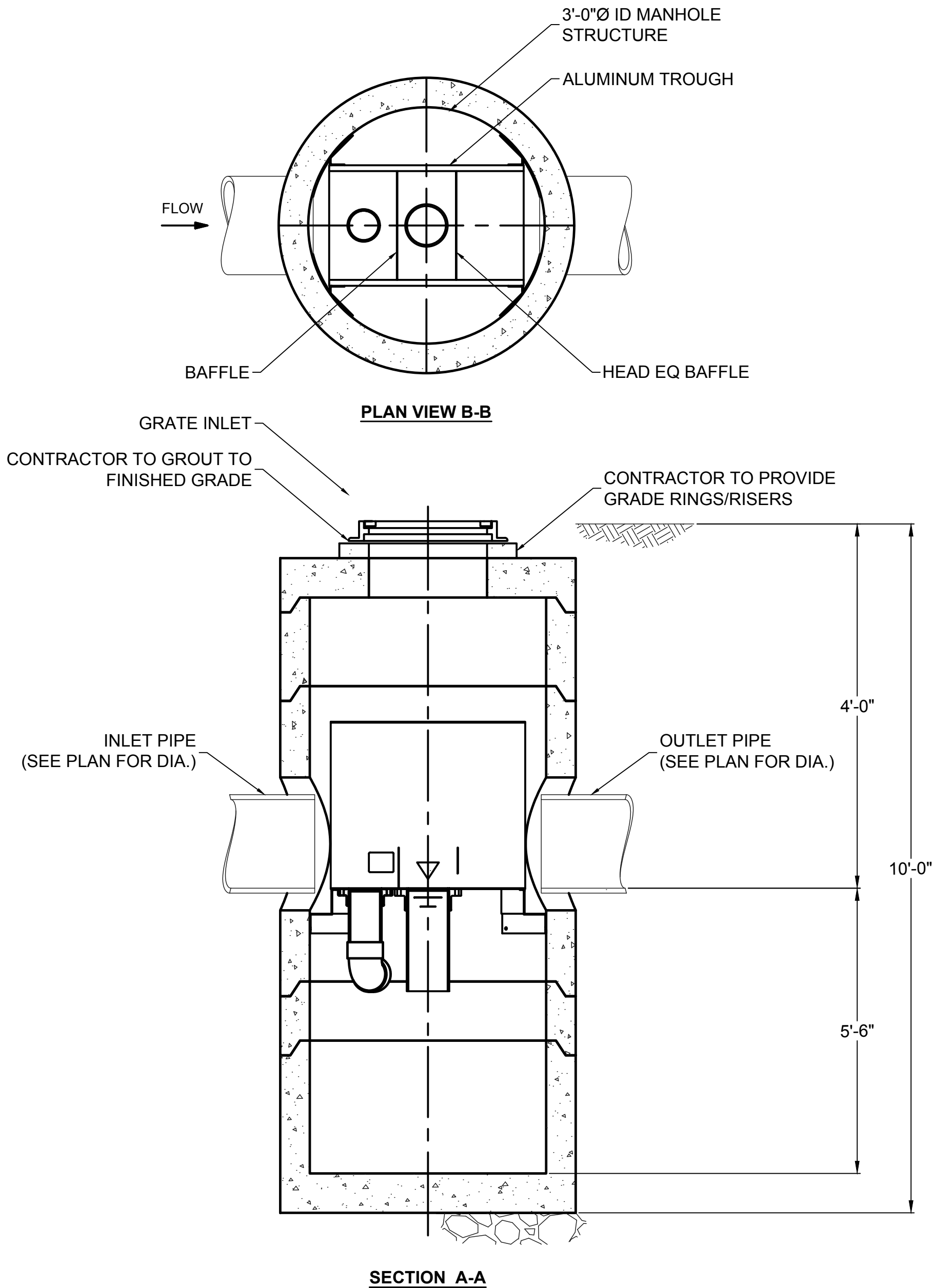
GRAPHIC SCALE
NOT TO SCALE

DRAWING NO.

C2.2

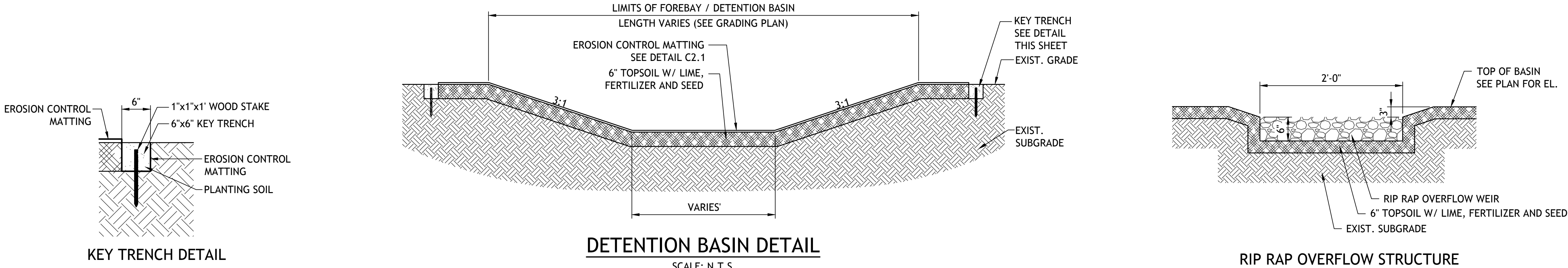
7 OF 8

9/14/2017 3:11:19 PM E:\ASG Data\Projects\MA - New Bedford\032 Design Only - Reconstruct Terminal Aprons\CADD\Preliminary Submission\02-032 SHEET DETAILS.dwg (MKO)



PROPRIETARY SEPARATOR DETAIL
SCALE: N.T.S.

- GENERAL NOTES**
- VORTSENTRY HS WATER QUALITY STRUCTURE SHALL BE IN ACCORDANCE WITH ALL DESIGN DATA AND INFORMATION CONTAINED IN THIS DRAWING.
 - STRUCTURE SHALL MEET AASHTO H20 LOAD RATING.
- INSTALLATION NOTES**
- ANY SUB-BASE, BACKFILL DEPTH, AND/OR ANTI-FLOTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
 - CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE MANHOLE STRUCTURE (LIFTING CLUTCHES PROVIDED).
 - CONTRACTOR TO INSTALL JOINT SEALANT BETWEEN ALL STRUCTURE SECTIONS AND ASSEMBLE STRUCTURE.
 - CONTRACTOR TO PROVIDE, INSTALL, AND GROUT PIPES. MATCH PIPE INVERTS WITH ELEVATIONS SHOWN.
 - CONTRACTOR TO TAKE APPROPRIATE MEASURES TO ASSURE UNIT IS WATER TIGHT, HOLDING WATER TO FLOWLINE INVERT MINIMUM. IT IS SUGGESTED THAT ALL JOINTS BELOW PIPE INVERTS ARE GROUTED.



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NO.	DATE	DESCRIPTION	BY
5	8/3/17	SHEET ADDED	MKO

PROJECT	OWNER
RECONSTRUCT TERMINAL APRONS PHASE I	NEW BEDFORD AIRPORT COMMISSION NEW BEDFORD REGIONAL AIRPORT NEW BEDFORD, MASSACHUSETTS

PROJECT NO.	PROJECT FILE	DESIGNED BY	DRAWN BY	CHECKED BY	DATE	DRAWING SCALE
103-032	SHEET DETAILS	MKO	MKO	RAL	SEPTEMBER 2016	N.T.S.

SHEET TITLE

**DRAINAGE DETAILS
(3 OF 3)**

GRAPHIC SCALE
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DRAWING NO.

C2.3

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