



NEW BEDFORD COMMUNITY PRESERVATION COMMITTEE

COMMUNITY PRESERVATION ACT PROJECT APPLICATION

FY18

Application Form Deadline: **December 11, 2017**

Applicants must submit this application no later than 12:00PM on Monday, December 11, 2017. Please refer to Submission Requirements within the Application Packet Instructions.

Full applications will not be accepted—regardless of project eligibility—until the Project Eligibility Determination Form has been submitted and approved.

Department of Planning, Housing & Community Development
608 Pleasant Street, 2nd Floor
Patrick Sullivan, Director



CITY OF NEW BEDFORD
COMMUNITY PRESERVATION FUND FY 2018

Project Application

Submission Requirements Checklist

Application items should be presented in the following order. Please check each item included in your submission.

GENERAL	
<input checked="" type="checkbox"/>	Application Cover Page(form provided)
<input checked="" type="checkbox"/>	Submission Requirements Checklist (this form)
<input checked="" type="checkbox"/>	Narratives (prompts provided)
<input checked="" type="checkbox"/>	Project Schedule(form provided)
FINANCIAL	
<input checked="" type="checkbox"/>	Budget Form (form provided)
<input checked="" type="checkbox"/>	At least two written quotes for project costs. Detailed cost estimates and/or written quotes. ATTACHMENT M
<input checked="" type="checkbox"/>	Proof of secured funding (commitment letters or bank statements), if applicable. ATTACHMENT N
OWNERSHIP/OPERATION (NON-CITY)	
<input checked="" type="checkbox"/>	If the applicant is not the owner, attach documentation of site control or written consent of owner to undertake the project. ATTACHMENT A
<input checked="" type="checkbox"/>	Certificate of Good Standing(if operating as a corporation) ATTACHMENT B
<input checked="" type="checkbox"/>	501(c)(3) certification (if operating as a non-profit) ATTACHMENT C
<input checked="" type="checkbox"/>	Purchase & Sale agreement or copy of current recorded deed, if applicable. ATTACHMENT A
COMMUNITY SUPPORT	
<input checked="" type="checkbox"/>	Letters of support from residents, community groups, city departments, boards or commissions, etc. ATTACHMENT E
HISTORIC RESOURCES PROJECTS	
<input checked="" type="checkbox"/>	Documentation that the project is listed on the State Register of Historic Places or a written determination from the New Bedford Historical Commission that the resource is significant in the history, archeology, architecture, or culture of New Bedford. ATTACHMENT F
<input checked="" type="checkbox"/>	Photos documenting the condition of the property. ATTACHMENT G
<input checked="" type="checkbox"/>	Report or condition assessment by a qualified professional describing the current condition of the property, if available. ATTACHMENT H
PLANS & REPORTS <i>The following plans and reports, if available, will strength your application, (submit in digital format only). Applicants are encouraged to submit as much detail as possible.</i>	
<input checked="" type="checkbox"/>	Renderings, site plans, engineering plans, design and bidding plans, specifications, and any MAAB variance requests. ATTACHMENT I
<input checked="" type="checkbox"/>	Applicable reports (21E, Historic Structure Report, appraisals, survey plan, feasibility studies, etc.)
VISUAL ATTACHMENT J (CHAPTER 91 LICENSE)	
<input checked="" type="checkbox"/>	Map of the property location (if applicable, show wetlands and wetland buffers, flood plain, water bodies, parks, open spaces, rails, and other features pertinent to the project). Applicants may use the City's interactive mapping website . ATTACHMENT K
<input checked="" type="checkbox"/>	Photos of the project site (not more than four views per site) Digital copies required. ATTACHMENT G
<input type="checkbox"/>	Catalog cuts (i.e. recreation equipment) if applicable. N/A

**DIGITAL
IMAGES**



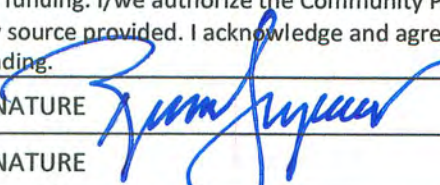
CITY OF NEW BEDFORD
COMMUNITY PRESERVATION FUND FY 2018

Project Application Cover Page

PROJECT INFORMATION			
PROJECT TITLE:	Butler Flats Lighthouse Preservation		
PROJECT LOCATION:	New Bedford Harbor; 41.6038° N, 70.8945° W		
LEGAL PROPERTY OWNER OF RECORD	United States of America (see Attachment A)		
CPA PROGRAM CATEGORY (Indicate all categories that apply)	<input type="checkbox"/> OPEN SPACE	<input checked="" type="checkbox"/> HISTORIC PRESERVATION	
	<input type="checkbox"/> RECREATION	<input type="checkbox"/> HOUSING	
ESTIMATED START DATE	June 1, 2018	ESTIMATED COMPLETION DATE	September 30, 2018
ONE SENTENCE DESCRIPTION OF PROJECT	This project involves repair of the caisson base and deck of Butler Flats, as the first phase of the preservation of this 119-year old lighthouse.		

APPLICANT INFORMATION			
THE APPLICANT IS: (Check only one)	<input type="checkbox"/> CITY BOARD OR DEPT	<input checked="" type="checkbox"/> NON PROFIT 501c3	<input type="checkbox"/> PRIVATE GROUP/INDIV
APPLICANT / ORGANIZATION:	Mass Light, Ltd.		
CO-APPLICANT NAME/ORGANIZATION: (If applicable)			
CONTACT PERSON:	Robert L. Draper		
MAILING ADDRESS:	P.O. Box 31000 Tucson, AZ 85751		
TELEPHONE #:	(520) 429-0911	EMAIL:	robert.draper@comcast.net

BUDGET SUMMARY	
CPA FUNDING REQUEST (must match CPA request in attached Budget Summary)	\$ 325,950
TOTAL BUDGET FOR PROJECT	\$ 449,886

SIGNATURES		
I/we attest that all information provided in this entire submission is true and correct to the best of my/our knowledge and that no information has been excluded, which might reasonably affect funding. I/we authorize the Community Preservation Committee and/or the City of New Bedford to obtain verification from any source provided. I acknowledge and agree that a permanent restriction may be placed on the property as a condition of funding.		
NAME (printed) Robert L. Draper	SIGNATURE 	DATE: 12/11/17
NAME (printed)	SIGNATURE	DATE:

PROJECT NARRATIVES**Project Description****1 GENERAL NARRATIVE (1000 Character Maximum)**

Provide a description of:

- *The project's location, the property involved and its proposed use.*
- *The proposed scope of work.*

Butler Flats Lighthouse, located in New Bedford Harbor, was built in 1898 and is listed on the National Register of Historic Places. Because of its age and exposure to the elements, the lighthouse is in desperate need of repair. It is proposed to be used as an aid to navigation (a lighthouse), a function it served from 1898 to 2014. Intended uses of Butler Flats are addressed further in Attachment S.

This project involves the stabilization and repair of the base of the lighthouse -- a cylindrical cast iron caisson filled with concrete -- and the concrete deck that covers the caisson to form the entry level of the lighthouse. The caisson has cracked in numerous places as a result of the freeze/thaw action of the ocean, and the concrete deck that covers the caisson is also cracked and in need of repair. Restoring these foundational elements is a necessary first step in the larger, comprehensive project of preserving the lighthouse and restoring it to operation.

2 COMMUNITY NEED (1000 Character Maximum)

- *What community need(s) will this project address? How does the project benefit the public and what populations(s) will it serve? If it serves a population currently underserved, please describe.*
- *How does the project preserve and enhance the character New Bedford?*

This project will preserve the Butler Flats lighthouse, one of three historic lighthouses in New Bedford and one of the most significant and historic structures in the area. It was designed by F. Hopkinson Smith, the same engineer who oversaw the construction of the foundation for the Statue of Liberty, and is listed on the National Register. Sitting just a few feet from one of the most active shipping lanes in the region, Butler Flats has served as an important landmark for mariners, and all local residents, for 119 years. Restoring it to good condition and functionality will honor its historic status and the maritime heritage of the area; contribute to and symbolize the resurgence of New Bedford's economy; promote tourism; and support and perpetuate the robust maritime activity of the region. A shining Butler Flats lighthouse will serve as the nautical gateway to New Bedford and be a source of pride for all residents. For more on community need, see Attachment O.

PROJECT NARRATIVES**Project Description****3 GOALS & OBJECTIVES (1000 Character Maximum)**

- Describe the project's goals and objectives. The objectives must be specific, measurable, achievable and realistic.
- How does the project meet the general and category-specific priorities outlined in the Community Preservation Plan or other current relevant planning documents?

The goal of this project is to stabilize and repair the caisson base and concrete deck of the Butler Flats lighthouse, as the first phase of a complete rehabilitation.

This project meets all of the general priorities of the CPP. New Bedford's lighthouses are among the historic resources that are listed specifically in the CPP. Rehabilitation of Butler Flats will benefit the active maritime economy by sustaining an aid to navigation that has operated for 119 years; promote tourism and create a more attractive landscape for tourists and residents; serve as a source of civic pride -- a shining landmark of the area's history and prosperity for those arriving by sea and those observing it from land; and potentially serve educational and research purposes, as it has in the recent past when it served as a SMAST data-gathering station. A more detailed assessment of this project's alignment with the CPP priorities is provided as Attachment Q.

4 MEASURING SUCCESS (1000 Character Maximum)

- How will the success of this project be measured?

The best resource for lighthouse preservation projects is the Historic Lighthouse Preservation Handbook, which was created through a cooperative partnership between the National Park Service, U.S. Coast Guard, Department of Defense Legacy Resource Management Program, and U.S. Lighthouse Society. The 328-page preservation manual, released in August 1997, focuses primarily on the unique restoration and maintenance issues associated with the many different materials and construction techniques used in historic lighthouses.

The success of this project will be measured by monitoring compliance with the National Historic Lighthouse Preservation Act of 2000 (54 U.S.C 35101), an amendment to the National Historic Preservation Act of 1966, and conformance to the Historic Lighthouse Preservation Handbook, and by comparing the condition of the rehabilitated lighthouse to the excellent condition in which it stood at the time of re-lighting during its Centennial celebration in 1998.

PROJECT NARRATIVES

Project Description

5 COMMUNITY SUPPORT (1000 Character Maximum)

- *Explain the level of community support this project has received. If at all possible, please include letters of support from any groups or individuals who have endorsed this project.*

This project has been advanced over the past two years in close coordination with the Office of the Mayor and the New Bedford Harbor Development Commission ("HDC"). The City, through the HDC, operated and maintained the lighthouse for 36 years, from 1978 until 2014, when the Coast Guard announced its intention to dispose of it. The City and HDC remain interested in seeing the lighthouse restored to good condition and operation. The HDC has been providing boat access to the lighthouse and has offered its cooperation and assistance with the preservation project. It is likely that the HDC will play a key role in the rehabilitation and subsequent operation and maintenance of the lighthouse as a U.S. Coast Guard-licensed private aid to navigation. Other supporters of the project include the Waterfront Historic Area League ("WHALE") and longtime Butler Flats champions Pete and Mary Duff and Hazel Murray, widow of longtime Butler Flats champion and volunteer Hugh Murray. See Attachment E.

6 CRITICAL NEED (1000 Character Maximum)

- *Is this project of an urgent nature?*
- *Is there a deadline or factors not controlled by the applicant (i.e. opportunity for immediate acquisition, opportunity to leverage available non-CPA grant or other financial opportunity)?*
- *For historic resource applications, is the property at risk for irreparable loss? If so, please include a condition assessment from a qualified professional.*

The professional engineering firms that inspected Butler Flats in 2014 and 2016 both concluded that the need to stabilize the caisson (base) is urgent, as it continues to crack and degrade as a result of the freeze/thaw action of the ocean and becomes more and more susceptible to shifting or failure with each such cycle. (See Attachments H-1 and H-2.) Fortunately, so far the lighthouse itself shows no significant, irreparable damage, such as shifting or major cracking, but time is of the essence for making repairs to the base and deck.

The project sponsors believe firmly that completing the stabilization and repair of the caisson and deck levels of the lighthouse are not only the most urgent aspects of the preservation project but also the best first step for generating awareness and support for the remaining phases of the work, such as restoring railings, windows and doors, and other decorative and functional elements, and cleaning, painting, and re-lighting the lighthouse.

PROJECT NARRATIVES

Financial

1 FINANCIAL INFORMATION (2000 Character Maximum)

- Describe all successful and unsuccessful attempts to secure funding and/or in-kind contributions, donations, or volunteer labor for the project.
- Will the project require funding over multiple years, and if so, provide annual funding requirements.
- What is the basis for the total CPA request?
- How will the project be affected if it does not receive CPA funds or a reduced amount?

So far, all funding for the Butler Flats preservation effort has been provided by Rincon Etal Investments, Inc. ("Rincon"), the Arizona corporation that placed the high bid in the GSA auction. Costs incurred to date total \$108,936.22, including:

- \$8,000 auction deposit;
- \$72,000 to complete the auction purchase (total bid was \$80,000);
- \$16,336 in fees to engineering firms that completed a condition assessment report and secured a Chapter 91 license;
- \$7,322 in legal and licensing fees;
- \$1,146.95 in IT expense; and
- \$4,131.27 for travel.

This funding request seeks \$325,950, an amount sufficient to complete the first phase of the preservation effort -- restoring the caisson and deck and possibly the access ladders. See Attachment U for details.

The 2016 condition report projects that the total cost of the project will be in the neighborhood of \$1.5MM. See Attachment H-2.

It is anticipated that future phases of the project will be funded through grants from other organizations, private donations, and, if necessary, additional funding from Rincon. Examples of other grant sources include the Maritime Heritage Fund, which has issued six-figure lighthouse restoration grants consistently in recent years, including, for example, a grant of \$105,000 last year to restore the cast iron railings at Block Island SE Lighthouse in Rhode Island, and the U.S. Lighthouse Society, which also has issued restoration grants consistently in recent years, including for example, a grant of \$9,000 to Spring Point Ledge Light in Maine (near-twin of Butler Flats). See Attachment L.

Other anticipated sources of funding include State tax credit funds; donations and fundraising events; and, as in other jurisdictions, vanity license plates.

Significant aspects of the project might be achieved cost-free, through donations of time and materials. See Attachment L-4.

We would also like to keep open the possibility of seeking additional CPA funds for future phases of the project.

PROJECT NARRATIVES**Project Management****1 APPLICANT INFORMATION (1000 Character Maximum)**

- Describe the applicant. Are they public, private non-profit, private for-profit, an individual, a partnership, or another type of entity? What is their history and background?
- Identify and describe the roles of all participants (applicants, architects, contractors, etc.), including the project manager.

Mass Light, Ltd. ("Mass Light"), is a Massachusetts non-profit formed in 2016 to pursue the preservation of Butler Flats.

While the winning GSA auction bid was submitted by Rincon Etal Investments, Inc., which initially applied to the Mass Dept of Environmental Protection ("DEP") for a Chapter 91 license to support the auction closing and transfer of the lighthouse, recently the GSA agreed to permit Rincon to assign its auction rights to Mass Light, and to allow Mass Light to take possession of the lighthouse directly from the GSA. On December 1, 2017, DEP issued the Chapter 91 license to Mass Light. The GSA auction is scheduled to close in December 2017. New Bedford law firm Perry Hicks, LLC has applied for 501 (c)(3) status for Mass Light.

Mass Light directors Robert and David Draper will manage the project and plan to engage the Waterfront Historic Area League for guidance.

Several experienced contractors have submitted proposals, as detailed in Attachment P.

2 APPLICANT BACKGROUND (1000 Character Maximum)

- Describe any past projects of similar type and scale, or experience that demonstrates the applicant's ability to carry out this project.

The Directors of Mass Light, Ltd, are Robert Draper and David Draper.

Robert has extensive experience with large infrastructure projects, as a former partner in the international project finance practice of a global law firm. That experience includes negotiation of complex, multi-party transactions and coordinating and cooperating with financing sources, developers, engineers, agencies, and other interested parties in the development of large projects such as power plants and other energy projects. He also manages businesses that engage in real estate development. He is a graduate of Willams College and Southern Methodist University School of Law.

David has extensive experience in real estate development, with expertise in cell tower siting and development. He holds a business degree from Texas Christian University.

Mass Light plans to engage a highly qualified contractor or consortium, and to work closely with WHALE, and is advised by the New Bedford law firm of Perry Hicks, LLC.

PROJECT NARRATIVES**Project Management****3 PROJECT FEASIBILITY (1000 Character Maximum)**

- *List and explain further actions or steps required for completion of the project, such as environmental assessments, zoning or other permits and approvals, agreement on terms of any required conservation, affordability or historic preservation agreements, subordination agreements, and any known or potential barriers or impediments to project implementation.*

Further actions required for completion of the project include:

- 1) Closing the GSA auction to transfer ownership of the lighthouse from the United States of America to Mass Light, Ltd., which is expected to occur this month (December 2017). The funds necessary to close the auction (\$72,000) are in the bank account of Mass Light, Ltd., and the closing documents (quitclaim deed and wiring instructions) have been received from the GSA. We expect to complete the closing next week. See Attachment A.
- 2) Securing this CPA funding.
- 3) Completion of later phases of the project might require filing and obtaining permits from the U.S. Army Corps of Engineers and updates to MassDEP on compliance with the Chapter 91 Waterways License.
- 4) Compliance with the NHPA and NHLPA will be required throughout the project.

4 PROJECT MAINTENANCE (1000 Character Maximum)

- *Please explain the long-term maintenance plan for the completed project.*

Future project phases will include restoration of decorative elements such as posts and railings, windows and doors; repointing and painting of masonry; and interior restoration (rotted wood, cracked lintels, etc.). It is anticipated that an early step will be re-raising and lighting an American flag, for which it is known. See Attachment T. The final step will be relighting the light.

Once the broader project is completed, the lighthouse will require regular maintenance such painting, monitoring of the tensioning system, and upkeep of mechanical systems. It is anticipated that maintenance will be on the order of \$20,000/year and that operation and maintenance of the light itself can be handled at minimal cost, hopefully with assistance of the HDC, as has been the case since 1978. These modest maintenance costs could be covered by fundraising events and donations to "Friends of Butler Flats" and possibly through a vanity license plate program, as has been done in other jurisdictions.

PROJECT NARRATIVES**Historic Resources Rehabilitation Projects Requirements****CPA Compliance (1000 Character Maximum)**

- Describe how the proposed project complies with the [U.S. Secretary of the Interior's Standards for Rehabilitation](#), as required by the CPA legislation under the definition of rehabilitation.
- Describe how the applicant will ensure compliance with these standards as the project is ongoing, including an identification of who will make historic preservation determinations.

The deed that will convey ownership of Butler Flats to Mass Light provides that the lighthouse is conveyed subject to the National Historic Lighthouse Preservation Act of 2000 (54 U.S.C 35101), an amendment to the National Historic Preservation Act of 1966. It also provides that Mass Light "shall maintain and preserve the Property in accordance with the recommended approaches in The Secretary of the Interior's Standards for Treatment of Historic Properties, 1995, Standards for Preservation (Technical Preservation Services for Historic Buildings, National Park Service) in order to preserve and enhance the distinctive materials, features and spaces that make the Property eligible for inclusion in the National Register of Historic Places." See Attachment A-3.

This project will be carried out in accordance with the Historic Lighthouse Preservation Handbook described above. The contractors are well-versed in NHPA compliance.

Mass Light plans to engage WHALE to advise on NHPA compliance.

Accessibility Requirements**CPA Compliance (1000 Character Maximum)**

- Describe how the proposed project complies with the all [ADA/MAAB Regulations](#).

Because of its offshore location, the project has never been, and will likely never be, accessible to the public. For 119 years, the lighthouse has been accessed by either of two vertical cast-iron ladders, one on the north side and one on the south. Boats have been moored at a distance or hoisted on davits; there is no dock. In short, access is difficult. It is anticipated that "access" could be improved by creating "alternative programming," including, for example, the creation of a park-like lighthouse viewing area on the nearby pier that extends from East Rodney French Boulevard and/or the creation of a virtual lighthouse experience somewhere in downtown New Bedford, along the lines of what has been created at the Maine Maritime Museum. See Attachment R. These ideas have been discussed preliminarily with the Office of the Mayor and HDC. Any such projects would be separate from, but complementary to, the rehabilitation of the Butler Flats lighthouse.

COMMUNITY PRESERVATION FUND ■ PROJECT APPLICATION FY18
PROJECT NARRATIVES

Community Housing Projects Requirements

CPA Compliance (1000 Character Maximum)

- *Describe how the proposed project complies with CPA affordability requirements.(100%of AMI for New Bedford)*
- *Describe the number and types of units (e.g.: 1br, 2br).*
- *Provide a complete Development Budget and an Operating Budget (for rental properties).*

not applicable

PROJECT SCHEDULE

Please provide a project timeline below, noting all project milestones. Please note that because the City Council must approve all appropriations, CPA funds, if awarded, may not be available until up to two months following CPC approval.

	ACTIVITY	ESTIMATED DATE
PROJECT START DATE:	inspection of caisson	June 2018
PROJECT MILESTONE:	stabilization and repair of caisson	June and July 2018
50% COMPLETION STAGE:	completion of caisson repair; removal of concrete deck	July and August 2018
PROJECT MILESTONE:	commencement of reconstruction of concrete deck	August and Sept 2018
PROJECT COMPLETION DATE:	completion of deck reconstruction	September 2018

PROJECT BUDGET

Please include a complete itemized budget of all project expenses, including the proposed funding source for each expense, in your submission. Note: CPA funds cannot be used for maintenance.

FUNDING SOURCES		EXPENSES				
		STUDY	SOFT COSTS*	ACQUISITION	CONSTRUCTION**	TOTAL
1	NEW BEDFORD CPA	\$	\$	\$	\$325,950	\$325,950
2	RINCON ETAL INVESTMENTS, INC.	\$	\$28,936	\$80,000	\$	\$108,936
3	RINCON ETAL INVESTMENTS, INC.	\$	\$15,000.00*	\$	\$	\$15,000
4	*additional travel and support	\$	\$	\$	\$	\$
5		\$	\$	\$	\$	\$
TOTAL PROJECT COSTS		\$	\$	\$	\$	\$449,886

* Soft costs include design, professional services, permitting fees, closing costs, legal, etc.

** Construction refers to new construction, rehabilitation, preservation, restoration work, and/or accessibility related expenses.

Project Cost Summary

Please explain the status of each funding source (i.e., submitting application on X date, applied on X date, received award notification on X date, funds on hand, etc.). For sources where funding has been awarded or funds are on hand, please include documentation from the funding source (e.g., commitment letter, bank statement) in application packet.

FUNDING SOURCE	STATUS OF FUNDING
1 RINCON ETAL INVESTMENTS, INC.	\$36,936.22 spent; \$72,000 to be spent on acquisition closing in December 2017
2	\$15,000 budgeted for travel expense and other support during this first phase of preservation
3	
4	
5	

Construction Projects Only

ACTIVITY	CPA FUNDS	OTHER FUNDS	TOTAL
Acquisition Costs			
Land	\$	\$	\$
Existing Structures	\$	\$ 80,000	\$ 80,000
Other acquisition costs	\$	\$	\$
Site Work (not in construction contract)			
Demolition/clearance	\$	\$	\$
Other site costs	\$	\$	\$
Construction/Project Improvement Costs			
New Construction	\$	\$	\$
Rehabilitation	\$ 246,000	\$	\$ 246,000
Performance bond premium	\$	\$	\$
Construction contingency	\$	\$	\$
Other	\$	\$	\$
Architectural and Engineering			
Architect fees	\$	\$	\$
Engineering fees	\$ 36,900	\$ 16,336	\$ 53,236 (engineering and other consulting)
Other A & E fees	\$	\$	\$
Other Owner Costs			
Appraisal fees	\$	\$	\$
Survey	\$	\$	\$
Soil boring/environmental/LBP	\$	\$	\$
Tap fees and impact fees	\$	\$	\$
Permitting fees	\$	\$ 1,130	\$ 1,130
Legal fees	\$	\$ 6,192	\$ 6,192
Other	\$	\$ 15,000	\$ 15,000 (additional travel, legal, etc.)
Miscellaneous Costs			
Developer fees	\$	\$	\$
Project reserves	\$	\$	\$
Relocation costs	\$	\$	\$
Project Administration & Management Costs			
Marketing/management	\$	\$ 1,146.95	\$ 1,146.95
Operating	\$	\$	\$
Taxes	\$	\$	\$
Insurance	\$	\$	\$
Other	\$ 43,050 (conting.)	\$ 4,131.27	\$ 47,181.27
TOTAL	\$ 325,950	\$ 123,936.22	\$ 449,886.22

ATTACHMENTS

ATTACHMENT A – OWNERSHIP OF BUTLER FLATS

ATTACHMENT B – CERTIFICATE OF GOOD STANDING FOR MASS LIGHT, LTD.

ATTACHMENT C – STATUS OF 501(c)(3) CERTIFICATION

ATTACHMENT D – PURCHASE & SALE AGREEMENT OR RECORDED DEED

ATTACHMENT E – COMMUNITY SUPPORT

ATTACHMENT F – EVIDENCE OF LISTING ON STATE REGISTER OF HISTORIC PLACES

ATTACHMENT G – PHOTOS DOCUMENTING THE CONDITION OF THE PROPERTY

ATTACHMENT H – CONDITION ASSESSMENT REPORTS

ATTACHMENT I – SITE PLAN

ATTACHMENT J – CHAPTER 91 WATERWAYS LICENSE

ATTACHMENT K -- MAP OF SITE LOCATION

ATTACHMENT L – SOURCES OF FUNDING FOR LIGHTHOUSE PRESERVATION

ATTACHMENT M – WRITTEN QUOTES FOR PROJECT COSTS

ATTACHMENT N – COMMITMENT OF FUNDING

ATTACHMENT O – COMMUNITY NEED

ATTACHMENT P – LIST OF PROJECT PARTICIPANTS

ATTACHMENT Q – MATRIX OF CONFORMITY TO CPP PRIORITIES

ATTACHMENT R – “ALTERNATIVE PROGRAMMING” EXAMPLE– VIRTUAL LIGHT

ATTACHMENT S – INTENDED USE

ATTACHMENT T – RE-LIGHTING THE AMERICAN FLAG

ATTACHMENT U – DETAILED EXPLANATION OF REQUESTED FUNDING AMOUNT

ATTACHMENT A

OWNERSHIP OF BUTLER FLATS LIGHTHOUSE

Ownership of Butler Flats Lighthouse

Butler Flats Lighthouse is currently owned by the United States of America, as surplus property of the United States Coast Guard.

In 2015, the U.S. General Services Administration (“GSA”) put the lighthouse up for public auction.

The high bidder was Rincon Etal Investments, Inc. (“Rincon”), an Arizona corporation managed by Robert Draper, one of the sponsors of this Butler Flats Lighthouse Preservation project. See Bid Award Letter, Attachment A-1.

The only condition precedent to closing the GSA auction, besides paying the purchase price, is that the high bidder obtain a Chapter 91 Waterways License from the Massachusetts Department of Environmental Protection (“MassDEP”), essentially to permit the lighthouse to rest on State-owned ocean bottom.

A Chapter 91 license application was filed in May 2017, and the license was issued just last week, on December 1, 2017.

In 2016, the project sponsors formed a Massachusetts non-profit corporation, Mass Light, Ltd., as the vehicle for undertaking the preservation of Butler Flats lighthouse. See Attachment A-2. While the Chapter 91 license application was pending, MassDEP agreed to revise the application so that the license could be issued directly to Mass Light, Ltd, and the GSA agreed to allow Rincon to transfer its auction rights to Mass Light, Ltd., so that Mass Light, Ltd., could purchase the lighthouse directly from the GSA.

Rincon’s high bid for Butler Flats was \$80,000, and a deposit of \$8,000 was made in 2015. Rincon has now funded Mass Light, Ltd., with an additional \$72,000 so as to enable Mass Light, Ltd., to complete the auction purchase. The GSA has provided wiring instructions and a draft of the quitclaim deed that will evidence the conveyance of Butler Flats. See Attachment A-3.

So in order to complete the purchase and obtain possession of the lighthouse, Mass Light, Ltd., need only wire the funds, obtain the quitclaim deed, and record it. The GSA anticipates that this process can be completed during the week of December 11th, 2017. See Attachment A-4.

ATTACHMENT A-1

GSA BID AWARD LETTER



GSA Public Buildings Service
Tel: (617) 565-7736
Fax: (617) 565-5720
sara.massarello@gsa.gov

December 2, 2015

Rincon Etal Investments, Inc
c/o Mr. Robert Draper
6100 E. Broadway Blvd
Tucson, AZ 85751
520-571-2210
robert.draper@comcast.net

RE: Notice of Acceptance of Bid
Butler Flats Lighthouse
Offshore of New Bedford
New Bedford, Bristol County, MA 02740
GSA Case Number: 1-X-MA-0871-AC
Sale / Lot / IFB Number: BOSTN-115-011-001

Dear Mr. Draper:

Your bid of EIGHTY THOUSAND DOLLARS (\$80,000.00) for the Butler Flats Lighthouse located offshore of City of New Bedford, Bristol County, Massachusetts 02740 (the "Property"), Sale/Lot/IFB# BOSTN115011001 (IFB), is hereby accepted on behalf of the Government.

Your bid deposit of FIVE THOUSAND DOLLARS (\$5,000.00) will be applied to the purchase price. Within ten (10) business days from the date of this Notice an additional THREE THOUSAND DOLLARS (\$3,000.00) is due. This amount will bring your bid deposit up to EIGHT THOUSAND DOLLARS (\$8,000.00) to equal 10% of the purchase price, per the terms of the IFB. Payment is accepted in the form of a cashier's check, or certified check made payable to *U.S. General Services Administration*, and mailed to:

GSA Real Property Utilization and Disposal Division (1PZ)
Attn: Lawanda Maryland, Program Analyst
Thomas P. O'Neill Federal Building
10 Causeway Street, Room 1010
Boston, MA 02222

Failure to provide the additional \$3,000.000 deposit within ten (10) business days from the date of this Notice, shall result in default of the sales contract and forfeiture of the Registration Deposit.

As stated in the IFB *General Terms of Sale Section Closing: Requirement For Chapter 91 License, Tender Of Payment And Delivery Of Instrument Of Conveyance*, you are required to obtain a Chapter 91 License from Massachusetts Department of Environmental Protection (MassDEP). The Government will not convey title to the Property until you obtain a Chapter 91 License. Within fifteen (15) business days from the date of this Notice, you are required to contact MassDEP to initiate the process to obtain a Chapter 91 License. Failure to timely initiate the process may result in a default and forfeiture of the Registration Deposit and Additional Deposit.

The closing date of the sale (the "Closing Date") will be set by the Government and will take into account the time required to obtain a Chapter 91 License. By the Closing Date, you shall tender to the Government the balance of the Purchase Price in the form of a cashier's check, certified check or electronic wire transfer, as well as proof that a Chapter 91 License has been obtained. We can provide wire transfer instructions if this is your desired method of payment. The amount due to the Government at closing shall be SEVENTY TWO THOUSAND DOLLARS (\$72,000.00).

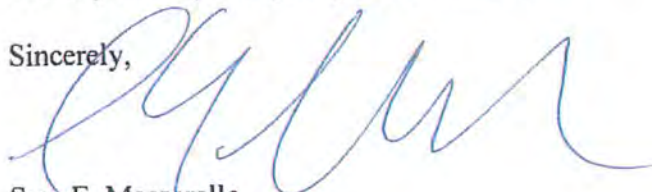
\$80,000.00	Total Purchase Price
- \$5,000.00	Original Bid Deposit
- \$3,000.00	<u>Additional Bid Deposit</u>
\$72,000.00	Due at Closing

Finally once a Closing Date is set, provide the name and address of your selected escrow or title agent, if you are using one, so that we can provide closing instructions. The Government will not be obligated to pay for any escrow, title or other closing costs associated with this sale.

Please contact me at 617-565-7736 or sara.massarello@gsa.gov to coordinate the closing and with any other questions.

Thank you for participating in our online sale.

Sincerely,



Sara E. Massarello
Project Manager
Real Property Utilization and Disposal Division (1PZ)



U.S. General Services Administration

robert.draper@comcast.net

From: LaWanda Maryland - 1PZB <lawanda.maryland@gsa.gov>
Sent: Tuesday, December 8, 2015 6:58 AM
To: Robert Draper
Cc: Sara Massarello - 1PZS
Subject: Re: USA Acceptance of High Bid (\$80,000.00) SALE/LOT: BOSTN115-011-001

Good Morning Mr. Draper,

GSA has received the \$3,000 wire transfer.

Thank You.

On Fri, Dec 4, 2015 at 12:36 PM, Robert Draper <robert.draper@comcast.net> wrote:

Thank you!

Best regards.

Rob

Rob Draper

Rincon Etal Investments, Inc.

P.O. Box 31000

Tucson, AZ 85751

(520) 571-2210

robert.draper@comcast.net

From: LaWanda Maryland - 1PZB [mailto:lawanda.maryland@gsa.gov]
Sent: Friday, December 4, 2015 10:31 AM
To: Robert Draper <robert.draper@comcast.net>
Subject: Re: USA Acceptance of High Bid (\$80,000.00) SALE/LOT: BOSTN115-011-001

ATTACHMENT A-2

**MASS LIGHT, LTD.
ARTICLES OF ORGANIZATION**

SB
Examiner

The Commonwealth of Massachusetts

William Francis Galvin
Secretary of the Commonwealth
One Ashburton Place, Room 1717, Boston, Massachusetts 02108-1512

ARTICLES OF ORGANIZATION (General Laws, Chapter 180)

Name
Approved

ARTICLE I

The exact name of the corporation is:

Mass Light, Ltd.

ARTICLE II

The purpose of the corporation is to engage in the following activities:

Exclusively for educational, scientific and charitable purposes within the meaning of §501(c)(3) of the Internal Revenue Code, as from time to time amended, and Chapter 180, section 4 of the General Laws of the Commonwealth; to restore and preserve for future generations lighthouses deemed excess pursuant to the National Lighthouse Historic Preservation Act of 2000; to secure the function of such lighthouses as approved private aids to navigation, where appropriate; to ensure that such lighthouses remain useful and important to their local communities; to document the history of such lighthouses and their keepers; to raise funds to support the foregoing activities; and to conduct such other activities and programs in furtherance of the foregoing purposes as may be carried out by a corporation organized under Massachusetts General Laws chapter 180 and qualifying under the provisions of section 501(c)(3) of the Internal Revenue Code.

C ☐
P ☐
M ☐
R.A. ☒

7
RC.

Note: If the space provided under any article or item on this form is insufficient, additions shall be set forth on one side only of separate 8 1/2 x 11 sheets of paper with a left margin of at least 1 inch. Additions to more than one article may be made on a single sheet so long as each article requiring each addition is clearly indicated.

ARTICLE III

A corporation may have one or more classes of members. If it does, the designation of such classes, the manner of election or appointments, the duration of membership and the qualification and rights, including voting rights, of the members of each class, may be set forth in the by-laws of the corporation or may be set forth below:

The corporation has no members.

ARTICLE IV

**Other lawful provisions, if any, for the conduct and regulation of the business and affairs of the corporation, for its voluntary dissolution, or for limiting, defining, or regulating the powers of the corporation, or of its directors or members, or of any class of members, are as follows:

See attached Continuation Sheet #4

ARTICLE V

The by-laws of the corporation have been duly adopted and the initial directors, president, treasurer and clerk or other presiding, financial or recording officers, whose names are set out on the following page, have been duly elected.

***If there are no provisions, state "None".*

Note: The preceding four (4) articles are considered to be permanent and may only be changed by filing appropriate Articles of Amendment.

Continuation Sheet #4

Other lawful provisions for the conduct and regulation of the business and affairs of the corporation, for its voluntary dissolution, or for limiting, defining, or regulating the power of the corporation, or of its directors, officers, or members, are as follows:

- (a) In addition to the powers granted to the corporation by General Laws, Chapter 180, the corporation shall have and may exercise in furtherance of its corporate purposes each of the powers specified in Sections 9A and 9B of Massachusetts General Laws Chapter 156B.
- (b) The corporation may be a partner in any enterprise which it would have power to conduct by itself.
- (c) The directors may make, amend or repeal the bylaws in whole or in part.
- (d) No part of the net earnings of the corporation shall inure to the benefit of, or be distributed to its members, directors, officers, private shareholders or individuals, except that the corporation shall be authorized and empowered to pay reasonable compensation for services actually rendered and to make payments and distributions in furtherance of the corporation's purposes set forth in Article II of these Articles of Organization. No substantial part of the activities of the corporation shall consist of the carrying on of propaganda or otherwise attempting to influence legislation (except as otherwise provided in Section 501(h) of the Internal Revenue Code), and the corporation shall not participate or intervene in (including the publishing or distribution of statements) any political campaign on behalf of any candidate for public office. It is intended that the corporation shall be entitled to qualify for exemption from federal income tax under Section 501(c)(3) of the Internal Revenue Code as the same may be amended from time to time (the "Code") and shall not be a private foundation under Section 509(a) of the Code.
- (e) Notwithstanding anything else herein provided, the corporation is organized and shall be operated exclusively for educational, scientific or charitable purposes, as said terms have been and shall be defined pursuant to Sections 170(e) and 501(c)(3) of the Internal Revenue Code. All powers of this corporation shall be exercised only in such manner as will assure the operation of this corporation exclusively for said educational, scientific or charitable purposes, as so defined, it being the intention that this corporation shall be exempt from federal income tax and that contributions to it shall be deductible pursuant to said sections of said Code, and all purposes and powers herein shall be interpreted and exercised consistently with this intention.
- (f) Except as may be otherwise required by law, the corporation may at any time authorize a petition for its dissolution to be filed with the Supreme Judicial Court of the Commonwealth of Massachusetts General Laws by the affirmative vote of a majority of the directors of the corporation then in office; provided, however, that in the event of any liquidation, dissolution, termination or winding up of the corporation (whether voluntary, involuntary or by operation of the law), the property or assets of the corporation remaining after providing for the payment of its debt and obligations shall be conveyed, transferred, distributed and set over outright to one or more educational, charitable or literary institutions or organizations, created and organized for nonprofit purposes similar to those of the corporation, which qualify as exempt

qualify as exempt from income tax under Section 501(c)(3) of the Internal Revenue Code, as a majority of the total number of the directors of the corporation may by vote designate and in such proportions and in such manner as may be determined in such vote; provided, further, that the corporation's property may be applied to charitable or educational purposes in accordance with the doctrine of cy pres in all respects as a court having jurisdiction in the premises may direct.

- (g) Subject to the provisions of paragraphs (d) and (e) of this Article 4, no contract or other transaction of this corporation with any other person, corporation, association, or partnership shall be affected or invalidated by the fact that (i) this corporation is a stockholder in such other corporation, association or partnership or (ii) any one or more of the officers or directors of this corporation is an officer, director or partner of such other corporation, association or partnership, or (iii) any officer or director of this corporation, individually or jointly with others, is a party to or is interested in such contract or transaction. Any director of this corporation may be counted in determining the existence of a quorum at any meeting of the board of directors for the purpose of authorizing or ratifying any such contract or transaction, and may vote thereon, with like force and effect as if he were not so interested or were not an officer, director or partner of such other corporation, association or partnership.
- (h) No officer or director shall be personally liable to the corporation for monetary damages for any breach of fiduciary duty by such officer or director as an officer or director notwithstanding any provision of law imposing such liability, except that, to the extent provided by applicable law, this provision shall not eliminate or limit the liability of an officer or director (i) for breach of the officer's or director's duty of loyalty to the corporation (ii) for acts or omissions not in good faith or which involve intentional misconduct or a knowing violation of law or (iii) for any transaction from which the officer or director derived an improper personal benefit. No amendment or repeal of this provision shall deprive an officer or director of the benefit hereof with respect to any act or omission occurring prior to such amendment or repeal.
- (i) The corporation shall, to the extent legally permissible, indemnify each person who may serve or who has served at any time as a director, president, vice president, treasurer, assistant treasurer, clerk, assistant clerk or other officer of the corporation or who at the request of the corporation may serve or at any time has served as a fiduciary or trustee of an employee benefit plan of the corporation (collectively, "Indemnified Officers" or individually, "Indemnified Officer"), against all expenses and liabilities, including, without limitation, counsel fees, judgments, fines, excise taxes, penalties and settlement payments, reasonably incurred by or imposed upon such Indemnified Officer in connection with any threatened, pending or completed action, suit or proceeding whether civil, criminal, administrative or investigative (a "proceeding") in which an Indemnified Officer may become involved by reason of serving or having served in such capacity (other than a proceeding voluntarily initiated by such Indemnified Officer unless the proceeding was authorized by a majority of the full board of directors); provided that no indemnification shall be provided for any such Indemnified Officer with respect to any matter as to which the Indemnified Officer shall have been finally adjudicated in any proceeding not to have acted in good faith in the reasonable belief that such Indemnified Officer's action was in the best interests of the corporation or, to the extent that such matter relates to service with respect to an employee benefit plan, in the

best interests of the participants or beneficiaries of such employee benefit plan; and further provided that any compromise or settlement payment shall be approved by the corporation in the same manner as provided below for the authorization of indemnification.

Such indemnification may, to the extent authorized by the board of directors of the corporation, include payment by the corporation of expenses incurred in defending a civil or criminal action or proceeding in advance of the final disposition of such action or proceeding, upon receipt of an undertaking by the person indemnified to repay such payment if such Indemnified Officer shall be not entitled to indemnification under this paragraph, which undertaking may be accepted without regard to the financial ability of such Indemnified Officer to make repayment.

The payment of any indemnification or advance shall be conclusively deemed authorized by the corporation under this paragraph, and each director and officer of the corporation approving such payment shall be wholly protected, if:

- (1.) the payment has been approved or ratified (1) by a majority vote of a quorum of either (a) the members who are not at that time parties to the proceeding or (b) the directors who are not at that time parties to the proceeding or (2) by a majority vote of a committee of two or more directors who are not at that time parties to the proceeding and are selected for this purpose by the full board (in which selection directors who are parties may participate); or
- (2.) the action is taken in reliance upon the opinion of independent legal counsel (who may be counsel to the corporation) appointed for the purpose by vote of the directors in the manner specified in clauses (1) or (2) of subparagraph (1.) or, if that manner is not possible, appointed by a majority of the full board of directors then in office; or
- (3.) the directors have otherwise acted in accordance with the standard of conduct applied to directors under Chapter 180 of the Massachusetts General Laws, as amended; or
- (4.) a court having jurisdiction shall have approved the payment.

The indemnification provided hereunder shall inure to the benefit of the heirs, executors and administrators of any Indemnified Officer entitled to indemnification hereunder.

The right of indemnification under this paragraph shall be in addition to and not exclusive of all other rights to which any person may be entitled. Nothing contained in this paragraph shall affect any rights to indemnification to which corporation employees, agents, directors, officers and other persons may be entitled by contract or otherwise under law.

No amendment or repeal of the provisions of this paragraph which adversely affects the right of an Indemnified Officer under this paragraph shall apply with respect to such Indemnified Officer's acts or omissions that occurred at any time prior to such amendment or repeal, unless such amendment or repeal was voted for by or was made with the written consent of such Indemnified Officer.

ARTICLE VI

The effective date of organization of the corporation shall be the date approved and filed by the Secretary of the Commonwealth. If a later effective date is desired, specify such date which shall not be more than *thirty days* after the date of filing.

ARTICLE VII

The information contained in Article VII is not a permanent part of the Articles of Organization.

a. The street address (post office boxes are not acceptable) of the principal office of the corporation *in Massachusetts* is:

c/o Perry Hicks, LLC, 388 County Street, New Bedford, MA 02740

b. The name, residential address and post office address of each director and officer of the corporation is as follows:

	NAME	RESIDENTIAL ADDRESS	POST OFFICE ADDRESS
President:	Robert Draper	2801 N Indian Ruins Tucson AZ 85715	P.O. Box 31000, Tucson, AZ 85751
Treasurer:	Veronica K. Bartz	9875 S Cam dela Artina Vail AZ 85641	P.O. Box 31000, Tucson, AZ 85751
Clerk:	Robert Draper	2801 N Indian Ruins Tucson AZ 85715	P.O. Box 31000, Tucson, AZ 85751
Directors: (or officers having the powers of directors)	Robert Draper	2801 N Indian Ruins Tucson AZ 85715	P.O. Box 31000, Tucson, AZ 85751
	David Draper	2617 MockingbirdCt Ft WorthTX 76109	P.O. Box 31000, Tucson, AZ 85751

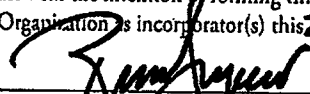
c. The fiscal year of the corporation shall end on the last day of the month of: **December**

d. The name and business address of the resident agent, if any, of the corporation is:

Daniel C. Perry, Esq., 388 County Street, New Bedford, MA 02740

I/We, the below signed incorporator(s), do hereby certify under the pains and penalties of perjury that I/we have not been convicted of any crimes relating to alcohol or gaming within the past ten years. I/We do hereby further certify that to the best of my/our knowledge the above-named officers have not been similarly convicted. If so convicted, explain.

IN WITNESS WHEREOF AND UNDER THE PAINS AND PENALTIES OF PERJURY, I/we, whose signature(s) appear below as incorporator(s) and whose name(s) and business or residential address(es) are clearly typed or printed beneath each signature, do hereby associate with the intention of forming this corporation under the provisions of General Laws, Chapter 180 and do hereby sign these Articles of Organization as incorporator(s) this 22nd day of September, 2016.



Robert L. Draper

2801 N. Indian Ruins Rd., Tucson AZ 85715

P.O. Box 31000, Tucson AZ 85751

Note: If an existing corporation is acting as incorporator, type in the exact name of the corporation, the state or other jurisdiction where it was incorporated, the name of the person signing on behalf of said corporation and the title he/she holds or other authority by which such action is taken.

RECEIVED

OCT 07 2016

SECRETARY OF THE COMMONWEALTH
CORPORATIONS DIVISION

THE COMMONWEALTH OF MASSACHUSETTS

ARTICLES OF ORGANIZATION

(General Laws, Chapter 180)

40293

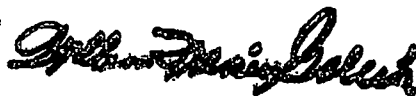
I hereby certify that, upon examination of these Articles of Organization, duly submitted to me, it appears that the provisions of the General Laws relative to the organization of corporations have been complied with, and I hereby approve said articles; and the filing fee in the amount of \$ 15 having been paid, said articles are deemed to have been filed with me this 7 day of October 20 16.

Effective date: _____

SECRETARY OF THE
COMMONWEALTH

2016 OCT -7 PM 12:34

CORPORATIONS DIVISION



WILLIAM FRANCIS GALVIN

Secretary of the Commonwealth

1275848

TO BE FILLED IN BY CORPORATION

Contact information:

Daniel C. Perry, Esq.

Perry Hicks, LLC

388 County Street, 2nd Fl., New Bedford, MA 02740

Telephone: (508) 996-8291

Email: danperry@perryhicks.net

A copy this filing will be available on-line at www.state.ma.us/sec/cor once the document is filed.

ATTACHMENT A-3

DRAFT QUITCLAIM DEED AND WIRING INSTRUCTIONS

QUITCLAIM DEED

KNOW ALL MEN BY THESE PRESENTS, that the UNITED STATES OF AMERICA, acting by and through the ADMINISTRATOR OF GENERAL SERVICES (the "Grantor"), under and pursuant to the powers and authority contained in the provisions of the National Historic Lighthouse Preservation Act of 2000 (54 U.S.C 35101), an amendment to the National Historic Preservation Act of 1966 (the "NHLPA"), having an address of General Services Administration, New England Region, Thomas P. O'Neill, Jr. Federal Building, 10 Causeway Street, Boston, Massachusetts 02222, for and in consideration of EIGHTY THOUSAND DOLLARS (\$80,000.00) hereby GRANT, GIVE, REMISE, AND RELEASE, without warranties or representations of any kind or nature, express or implied, Mass Light, Ltd, having an address of 388 County Street, New Bedford, MA 02740, (the "Grantee"), all such right, title and interest as Grantor has in and to that certain Government-owned real property, commonly known as the Butler Flats Light ("the Property") (GSA Control No. 1-X-MA-0871) which is a lighthouse located in the waters of New Bedford Channel at the entrance to New Bedford Harbor, at the mouth of the Acushnet River, Bristol County, Massachusetts. The location coordinates of the Property are 41° 36' 3" N 70° 53' 42" W. The Property includes the lighthouse structure, and an underwater cable, more fully described as follows:

Butler Flats Light, constructed in 1898, consists of a 53-foot high brick and cast iron sparkplug-style tower resting on a caisson foundation. There are four (4) levels including two (2) watch decks, and a basement containing approximately 1091 usable square feet. Each floor is approximately 37-feet in diameter with 15-foot head clearance. The Light includes a lantern optic.

Together with a License #2264 issued to US Coast Guard, its successors and assigns, by Commonwealth of Massachusetts Department of Public Works authorizing the US Coast Guard to lay and maintain a submarine power cable and telephone cable in, under and across a portion of Acushnet River from mainland to Butler Flats Light Station, dated October 16, 1940, and recorded at the Registry of Deeds, Bristol County, Southern District, New Bedford, April 3, 1941 at Book 837, Page 220-221.

The Property is conveyed subject to the covenants, conditions and restrictions hereinafter contained as set forth in the NHLPA. No submerged lands are conveyed under this Deed. The Grantee is responsible for obtaining and maintaining a Chapter 91 License from the Commonwealth of Massachusetts (the Commonwealth) to allow for legal occupancy of the bottomlands held in trust by the Commonwealth. The Chapter 91 License between the Grantee and the Commonwealth is attached hereto and incorporated herein as Exhibit "A".

The Property is conveyed subject to any and all existing reservations, easements, restrictions, covenants, and rights, of record to the extent in force and applicable,

including, but not limited to, any easements, described herein; any state of facts that would be disclosed by a physical examination of the Property; any state of facts that an accurate and adequate survey would disclose; and any and all other matters of record.

CONDITION OF THE PROPERTY. The Grantee, in accepting this Deed, acknowledges and attests that it has inspected, is aware of, and accepts the condition and state of repair of the Property. It is understood that the Property is conveyed "AS IS" and "WHERE IS" without any representation, warranty, or guarantee of any kind or nature, express or implied, including without limitation, any representation, warranty or guarantees as to quantity, quality, title, character, condition, size, or kind, or that the same is in condition or fit to be used for any particular purpose. The Grantee, in accepting this Deed, acknowledges that the Grantor has made no representation or warranty concerning the condition or state of repair of the Property that has not been fully set forth in this Deed.

NOTICE OF LEAD BASED PAINT FOR REAL PROPERTY CONSTRUCTED PRIOR TO 1978. The Property contains no improvements defined by 42 U.S.C. 4852d (Title X) as target housing. However, in the event that any improvement on the Property are converted to residential use, the Grantee covenants and agrees that in its use and occupancy of such Property it will comply with 24 CFR 35 and 40 CFR 745 and all applicable Federal, State and local laws relating to lead-based paint; and that United States assumes no liability for damages for Property damage, personal injury illness, disability, or death, to Grantee, its successors or assigns, or to any other person, including members of the general public, arising from or incident to the purchase, transportation, removal, handling, use disposition, or other activity causing or leading to contact of any kind whatsoever with lead-based paint on the Property described in this deed, whether Grantee, and its successors or assigns, have properly warned or failed properly to warn the individual(s) injured. Grantee further agrees to indemnify, defend and hold harmless the United States of America from any and all loss, judgment, claims, demands, expenses or damages, of whatever nature which might arise or be made against the United States of America, due to, or relating to the presence of lead-based paint hazard on the Property, any related abatement activities, or the disposal of any material from the abatement process.

The Grantee further covenants and agrees that it will comply with all Federal, state, local, and any other applicable law regarding the lead-based paint hazards with respect to the Property.

NOTICE OF THE PRESENCE OF ASBESTOS. The Grantee, by acceptance of this Deed, acknowledges that it has been informed that this property may contain asbestos-containing materials. Grantee, by acceptance of this Deed, acknowledges that it accepts the transfer and Deed subject to the terms and conditions herein:

1. The Grantee is warned that the Property may contain asbestos-containing materials. Asbestos is a hazardous material. Unprotected or unregulated exposures to asbestos in product manufacturing, shipyard, and building construction workplaces have been associated with asbestos-related diseases. Both the U.S. Occupational Safety and Health Administration (the "OSHA") and the Environmental Protection Agency (the "EPA")

regulate asbestos because of the potential hazards associated with exposure to airborne asbestos fibers. Both OSHA and EPA have determined that such exposure increases the risk of asbestos-related diseases, which include certain cancers and which can result in disability or death.

2. Grantee is deemed to have relied solely on its own judgment in assessing the overall condition of all or any portion of the Property including, without limitation, any asbestos hazards or concerns.
3. No warranties, either expressed or implied, are given with regard to the condition of the Property including, without limitation, whether the Property does or does not contain asbestos or is or is not safe for a particular purpose. The failure of any Grantee to have inspected, or to be fully informed as to the condition of all or any portion of the Property offered will not constitute grounds for any claim or demand against the United States.
4. The description of the Property set forth above and any other information provided to the Grantee with respect to the Property is based on the best information available to the General Services Administration Real Property Utilization and Disposal Office and is believed to be correct, but any error or omission, including, but not limited to, the omission of any information available to the agency having custody over the Property and/or any other Federal agency, shall not constitute grounds or reason for nonperformance of the contract of sale, or any claim by the Grantee against the Grantor including, without limitation, any claim for allowance, refund, or deduction from the purchase price.
5. The Grantor assumes no liability for damages for personal injury, illness, disability, or death, to the Grantee, or to the Grantee's successors, assigns, employees, invitees, or any other person subject to Grantee's control or direction, or to any other person, including members of the general public, arising from or incident to the purchase, transportation, removal, handling, use, disposition, or other activity causing or leading to contact of any kind whatsoever with asbestos at the Property that is the subject of this sale, whether the Grantee, its successors or assigns has or have properly warned or failed properly to warn the individual(s) injured.
6. The Grantee further agrees by acceptance of the Deed to the Property that it will comply with all Federal, State, and local laws, ordinances, orders, and regulations relating to asbestos.

HISTORIC PRESERVATION COVENANTS. The Property is listed on the National Register of Historic Places reference NR # 87001528. The Grantee, in accepting this Deed, acknowledges and accepts the following conditions and covenants:

1. Grantee shall maintain and preserve the Property in accordance with the recommended approaches in *The Secretary of the Interior's Standards for Treatment of Historic Properties, 1995, Standards for Preservation (Technical Preservation Services for Historic Buildings, National Park Service)* in order to preserve and enhance the distinctive materials, features and spaces that make the Property eligible for inclusion in the National Register of Historic Places.
2. When rehabilitation is the appropriate treatment, Grantee shall rehabilitate the Property in accordance with the recommended approaches in *The Secretary of the Interior's Standards for Treatment of Historic Properties, 1995, Standards for Rehabilitation (Technical Preservation Services for Historic Buildings, National Park Service)*. Rehabilitation is appropriate when repair and replacement of deteriorated features is necessary or when alteration or additions to the Property are planned.
3. Distinctive materials, features, finishes, construction techniques and examples of craftsmanship that characterize the Property shall be preserved.
4. Plans of proposed rehabilitation, construction, alteration or replacement of distinctive materials, features finished or spaces which would affect the appearance or structural integrity of the Property shall be reviewed and approved by the National Park Service/Massachusetts State Historic Preservation Officer ("SHPO") for consistency with *The Secretary of the Interior's Standards for Treatment of Historic Properties*.
5. The SHPO shall be permitted at all times to inspect the Property in order to ascertain if the above conditions are being observed.
6. The covenants, conditions and restrictions contained herein shall be inserted by the Grantee verbatim or by express reference in any instrument by which it divests itself of interest in the Property or by which it grants any interest in the Property.
7. The failure of the Secretary, the Administrator or the SHPO to exercise any right or remedy granted under this instrument shall not have the effect of waiving or limiting the exercise of any other right or remedy or the use of such rights or remedy at any other time.
8. The Grantee agrees that the SHPO may at its discretion without prior notice to the Grantee convey and assign all or part of its rights and responsibilities contained herein to a third party.
9. The covenants, conditions and restrictions set forth in this Historic Preservation Covenant shall constitute a binding servitude upon the Property and shall be deemed to run with the land on which the Property is situated. All the covenants, conditions, restrictions and obligations

described are binding upon the Grantee and its heirs, successors and assigns.

ASSIGNMENT OF LICENSE. Further, the Grantor assigns to the Grantee License #2264 to lay and maintain a submarine power cable and telephone cable in, under and across a portion of Acushnet River from mainland to Butler Flats Light Station, issued by Commonwealth of Massachusetts Department of Public Works, dated October 16, 1940 and recorded at the Registry of Deeds, Bristol County, Southern District, New Bedford, April 3, 1941 at Book 837, Page 220-221. License #2264 is attached hereto and incorporated herein as Exhibit "B".

All of the covenants, conditions, restrictions, and obligations described in this Deed, run with the Property and are binding upon the Grantee, its successors and assigns. Grantee's acceptance of this Deed is an acknowledgement that it is bound by all such covenants, conditions, restrictions, and obligations.

IN WITNESS WHEREOF, the UNITED STATES OF AMERICA, acting by and through the Administrator of General Services, has caused these presents to be duly executed for and in its name and behalf by Glenn C. Rotondo, Regional Commissioner, Public Buildings Service, New England Region, General Services Administration, who has this ____ day of _____ 2017, hereunto his hands and seals.

UNITED STATES OF AMERICA
by and through the Administrator of
General Services

Witnesses:

By:

GLENN C. ROTONDO
Regional Commissioner

ACKNOWLEDGEMENT

Commonwealth of Massachusetts)
County of Suffolk) ss.

In Boston, in said County and State, on this ____ day of _____ 2016 before me personally appeared Glenn C. Rotondo, Regional Commissioner, Public Buildings Service, New England Region, General Services Administration, duly empowered and authorized, proved to me through satisfactory evidence of identification, which was a U.S. General Services Administration ID Card, to be the person whose name is signed on the preceding instrument and by him duly executed, to be his free act and deed in his capacity as Regional Commissioner, Public Buildings Service, General Services Administration, New England Region.

Carol H. Chirico, Notary Public
My commission expires 10/14/2016

Format for Funds Transfer Messages To Treasury

Type/Subtype code (entered by sender)

Amount	Sender Financial Institution (9 digit routing number and short name)

Sender Institution Reference Information

Receiver Financial Institution Information
021030004 TREAS NYC

* GSA's routing number with U.S. Treasury

Beneficiary
47000016 General Services Administration

* GSA's account number with U.S. Treasury

Originator to Beneficiary Information
Use this line for the case number and property name - Case #: MA0871 Property Name: Butler Flats Lighthouse, New Bedford, MA
DDA 540012 (this number remains the same, always)
Attn: Melody Bingham (816) 926-2467 for deposit into 199X account
Additional line if needed

DDA # 540012 - Real Property

If you or your bank have any questions concerning wire transfer procedures, please contact:

Melody Bingham (816) 926-2467 or (816) 926-6092 at GSA's financial and Payroll Services Division

Federal Reserve Bank New York
33 Liberty Street
New York, NY 10045

Required Information for Funds Transfer Messages to Treasury

Tag Name	Tag	Required Information
Type/Subtype Code	(1510)	Type and Subtype codes entered by the sender: 10 00 (enter as shown)
Amount	(2000)	The dollar amount of transfer. The amount will include the dollar sign and the appropriate punctuation including cents digits. EXAMPLE: \$9,999,999.99
Sender FI	(3100)	Identifies the sending Financial Institution's nine digit routing number and short name. Sending Bank will provide this information. EXAMPLE: 999999999 BANK XYZ
Sender Reference	(3320)	The sender institution's reference information may be inserted by the sending bank to identify the transaction. EXAMPLE: 97040300066030303
Receiver FI	(3400)	Identifies the receiving institution. The nine digit identifier 021030004 is the routing number of the Treasury. The short name for Treasury is TREAS NYC. This item is a constant and is required for all funds transfer messages sent to Treasury. 021030004 TREAS NYC (Routing Number)
Beneficiary	(4200)	Provided by Agency. The three, four, or eight digit numeric symbol (Agency Location Code) used to identify Government departments and agencies. The Agency Location Code is critical and must be specified in the funds transfer message in order for the funds to be correctly classified to the respective agency. The agency name may also be provided and entered in this field. 47000016 (ALC Number/Account Number) General Services Administration
Originator to Beneficiary Info	(6000)	Used to identify information conveyed from originator to the beneficiary; e.g., details of payment, including invoice numbers, sales number, lot number, or other identifying information and amounts. Please reference DDA 540012, Your name, for property number, property name

Sample of Funds Transfer Format

Type/Subtype Code	(1510)	10 00
Amount	(2000)	\$9,999,999.99
Sender FI	(3100)	999999999 BANK XYZ
Sender Reference	(3320)	97040300066030303
Receiver FI	(3400)	021030004 TREAS NYC
Beneficiary	(4200)	470000016 General Services Administration
Originator to Beneficiary Info	(6000)	Sale number 3456x DDA 540012 XXXXXXXXXXXXXXXXXXXXXXXXXXXXXXXXXXXX XXXXXXXXXXXXXXXXXXXXXXXXXXXXXXXXXXXX

ATTACHMENT A-4

**CORRESPONDENCE FROM GSA
REGARDING AUCTION CLOSING**

robert.draper@comcast.net

From: Sara Massarello - 1PZS <sara.massarello@gsa.gov>
Sent: Friday, December 8, 2017 12:16 PM
To: Robert Draper
Subject: Re: Butler Flats -- Chapter 91 Waterways License

Rob,

WOW. I do not believe it! Congratulations!

It normally takes a few business days for the wire transfer to be reflected in our account. I will ask my colleague to be on the lookout for the \$72,000 deposit.

I will also ask my Assistant Regional Counsel and notary to schedule time with the Regional Commissioner next week to execute the deed. Once GSA confirms the deposit and executes the deed, I will send it by UPS to you for recording.

I request that you email me a copy of the recorded deed.

Thank you!
Sara

Sara E. Massarello
U.S. General Services Administration
Real Property Utilization and Disposal Division (1PZ)
Thomas P. O'Neill Federal Building, 11th Floor
10 Causeway Street
Boston, MA 02222
Phone: [617-565-7736](tel:617-565-7736)
Cell: [617-894-5184](tel:617-894-5184)
Fax: [617-565-5720](tel:617-565-5720)
sara.massarello@gsa.gov

On Thu, Dec 7, 2017 at 4:31 PM, <robert.draper@comcast.net> wrote:

Sara,

You're probably not going to believe this but..... the Chapter 91 license for Butler Flats was issued on Monday and recorded on Tuesday!

This afternoon I am going to open a bank account for Mass Light, Ltd., so that we can close the auction through that entity (which is the license holder).

If I wire funds tomorrow (Friday), what do you anticipate will be the timetable for getting the deed issued and any other closing items completed?

With best regards,

Rob

Rob Draper

Rincon Etal Investments, Inc./Mass Light, Ltd.

P.O. Box 31000

Tucson, AZ 85751

(520) 571-2210

robert.draper@comcast.net

ATTACHMENT A-5

**GSA PERMISSION TO SEEK
CPA FUNDING PRIOR TO
CLOSING OF GSA AUCTION PURCHASE**

robert.draper@comcast.net

From: Sara Massarello - 1PZS <sara.massarello@gsa.gov>
Sent: Thursday, October 26, 2017 7:11 AM
To: Robert Draper
Subject: Fwd: Butler Flats update

Rob,

Below is the email by which I conveyed the GSA's permission for Butler Flats to seek Community Preservation Act funding.

Thank you
Sara

Sara E. Massarello
U.S. General Services Administration
Real Property Utilization and Disposal Division (1PZ)
Thomas P. O'Neill Federal Building, 11th Floor
10 Causeway Street
Boston, MA 02222
Phone: 617-565-7736
Cell: 617-894-5184
Fax: 617-565-5720
sara.massarello@gsa.gov

----- Forwarded message -----

From: Sara Massarello - 1PZS <sara.massarello@gsa.gov>
Date: Fri, Oct 6, 2017 at 3:59 PM
Subject: Re: Butler Flats update
To: Robert Draper <robert.draper@comcast.net>

Hi Rob,

GSA Legal confirmed my earlier email.

You do have a License to access the site, which may be sufficient to support checking off "Site Control."

By email, GSA provides further written consent for Rincon Etal Investments, Inc. to submit an application.

Thank you
Sara

Sara E. Massarello
U.S. General Services Administration
Real Property Utilization and Disposal Division (1PZ)
Thomas P. O'Neill Federal Building, 11th Floor
10 Causeway Street
Boston, MA 02222
Phone: [617-565-7736](tel:617-565-7736)

ATTACHMENT B

**CERTIFICATE OF GOOD STANDING
FOR MASS LIGHT, LTD.**



William Francis Galvin
Secretary of the
Commonwealth

The Commonwealth of Massachusetts
Secretary of the Commonwealth
State House, Boston, Massachusetts 02133

Date: December 04, 2017

To Whom It May Concern :

I hereby certify that according to the records of this office,
MASS LIGHT, LTD

is a domestic corporation organized on **October 07, 2016**

I further certify that there are no proceedings presently pending under the Massachusetts General Laws Chapter 180 section 26 A, for revocation of the charter of said corporation; that the State Secretary has not received notice of dissolution of the corporation pursuant to Massachusetts General Laws, Chapter 180, Section 11, 11A, or 11B; that said corporation has filed all annual reports, and paid all fees with respect to such reports, and so far as appears of record said corporation has legal existence and is in good standing with this office.



In testimony of which,
I have hereunto affixed the
Great Seal of the Commonwealth
on the date first above written.

William Francis Galvin

Secretary of the Commonwealth

Certificate Number: 17120027410

Verify this Certificate at: <http://corp.sec.state.ma.us/CorpWeb/Certificates/Verify.aspx>

Processed by:

ATTACHMENT C

STATUS OF 501(c)(3) CERTIFICATION

Status of 501(c)(3) Certification for Mass Light, Ltd.

The New Bedford law firm of Perry Hicks, LLC, is processing a 501(c)(3) application for Mass Light, Ltd. The project sponsors recognize that securing 501(c)(3) status is essential to eligibility for CPA funding and for progressing the Butler Flats lighthouse restoration project. We are confident that we will secure 501(c)(3) status in time to commence the work described in this application and recognize that the project will not be eligible to access funding if we do not.

Mass Light, Ltd., has also filed to do business as “Friends of Butler Flats,” in anticipation of creating a public and social media presence in hopes of raising awareness, participation, and financial support for the historic preservation project and future operation and maintenance of Butler Flats. See attached filing.

PERRY HICKS, LLC
ATTORNEYS AT LAW
388 COUNTY STREET
NEW BEDFORD, MASSACHUSETTS 02740-4992

DANIEL C. PERRY

NEIL B. SMOLA

TINA M. FORD, PARALEGAL

PATTI L. EATON, OFFICE MANAGER

TELEPHONE
(508) 996-8291

TELECOPIER
(508) 997-2637

LEONARD E. PERRY
(1965-1999)

EDWARD D. HICKS
(1965-2002)

INFO@PERRYHICKS.NET
WWW.PERRYHICKS.NET

February 6, 2017

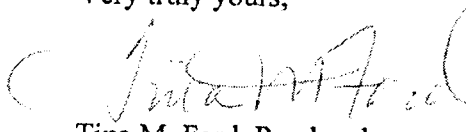
Rob Draper
Rincon Etal Investments, Inc.
P.O. Box 31000
Tucson, AZ 85751

Re: *Friends of Butler Flats*

Dear Mr. Draper:

Enclosed for your records please find an *attested* copy of the approved Business Certificate for the Friends of Butler Flats.

Very truly yours,



Tina M. Ford, Paralegal

Enclosure

cc: Daniel C. Perry, Esq.



BUSINESS CERTIFICATE

City of New Bedford

☒ New Filing

☐ Renewal

Filing Fee: \$40.00

This Certificate, file # 128070 Expires 1/20/2021

In conformity with the provisions of Chapter one hundred and ten, Section five of the General Laws, as amended, the undersigned hereby declares that a business under the title of:

Business Name: Friends of Butler Flats

Corporate Name: Mass Light, Ltd., a Massachusetts not for profit corporation

is conducted at 388 County Street

Zip 02740

Business Phone: 508-996-8291

by the following named person(s)

Owner Name: Robert Draper as President

Owner Tel: 520-571-2210

Owner Home Address: 2801 N. Indians Ruins, Tuscon AZ 85715

Owner Name 2: _____

Owner 2 Home Address: _____

Signed

Robert Draper

(SIGNATURE)

(SIGNATURE)

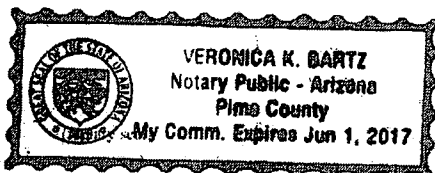
~~Commonwealth of Massachusetts~~ State of Arizona

County: PIMA

Date 12 / 9 / 16

Personally appeared before me, the named individual(s) Robert Draper

On the above date and made oath that the foregoing statement is true.



Signature:

Veronica K. Bartz
Notary Public / Designated Clerk

Commission Expires:

6 / 1 / 17

Important Notice: This certificate expires four years from the date of issue. If you cease conducting business before that time, the law requires that you withdraw this certificate with the Office of the City Clerk. Any change of location, residence or owner must be filed in the office of the City Clerk. A business operating in violation of this State Law "shall be punished by a fine of not more than \$300 for each month during which such violation occurs."

THIS CERTIFICATE DOES NOT GRANT PERMISSION TO CONDUCT BUSINESS IN VIOLATION OF ZONING LAWS.

CITY CLERKS OFFICE
NEW BEDFORD, MA

2017 JAN 20 A 9:28

CITY CLERK

BUSINESS CERTIFICATE

Name FRIENDS OF BUTLER FLATS

388 COUNTY STREET

508-996-8291

Owner ROBERT DRAPER, PRESIDENT

2801 N. INDIANS RUINS

TUCSON, AZ 85715

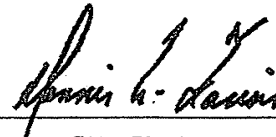
520-571-2210

Received and Recorded in the New Bedford

City Clerk's office on 1/20/2017

This Certificate, file # 128070, Expires 1/20/2021

Attest:

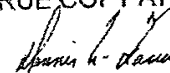


City Clerk

JAN 20 2017

Date Issued

A TRUE COPY ATTEST



CITY CLERK-NEW BEDFORD, MA

(This Side for Office use only)

ATTACHMENT D

**PURCHASE AND SALE AGREEMENT
OR
COPY OF CURRENT RECORDED DEED
[PLEASE REFER TO ATTACHMENT A]**

ATTACHMENT E

COMMUNITY SUPPORT

Community Support

This project has been progressed over the past two years in close coordination with the Office of the Mayor and the New Bedford Harbor Development Commission ("HDC"). The City, through the HDC, operated and maintained the lighthouse for 36 years, from 1978 until 2014, when the Coast Guard announced its intention to dispose of it. The City and HDC remain interested in seeing the lighthouse restored to good condition and operation. The HDC has been providing boat access to the lighthouse and has offered its cooperation and assistance with the preservation project. It is likely that the HDC will play a key role in the rehabilitation and subsequent operation and maintenance of the lighthouse as a U.S. Coast Guard-licensed private aid to navigation.

The project sponsors also intend to engage with the Waterfront Historic Area League and to coordinate with the New Bedford Historical Commission, the New Bedford Economic Development Council, the New Bedford Whaling Museum, and other important historic preservation resources.

The attached social media post is included because we believe it provides an excellent example of the broad positive sentiment for the Butler Flats lighthouse and its significance to mariners and other residents of the region. The post speaks for itself.

Other supporters of the project include longtime Butler Flats champions Pete and Mary Duff and Hazel Murray, widow of longtime Butler Flats champion and volunteer Hugh Murray. Hugh and Pete were instrumental in organizing the celebration of the Centennial of Butler Flats and its repair and relighting in 1998 (and over many years before and after) and are a source of inspiration for this preservation project.



December 10, 2017

To: CPA Committee for New Bedford
From: The waterfront Historic Area League (WHALE)

Dear CPA Committee and City Council:

Butler Flats Lighthouse, located in New Bedford Harbor, was built in 1898 and is listed on the National Register of Historic Places. Because of its age and exposure to the elements, the lighthouse is in desperate need of repair. It is proposed to be used as an aid to navigation (a lighthouse), a function it served from 1898 to 2014.

The most essential aspects of this preservation project are the stabilization and repair of the base of the lighthouse -- a cylindrical cast iron caisson filled with concrete -- and the concrete deck that covers the caisson to form the entry level of the lighthouse. Restoring these foundational elements of the lighthouse is a necessary first step in the larger, comprehensive project of preserving the Butler Flats lighthouse and restoring it to operation.

WHALE fully supports this project and will provide historical expertise as required by the Butler Flats project team. We hope that it receives CPA funding.

Sincerely,

Teri Bernert
Executive Director, WHALE



Colby Costeira

September 29, 2015 · Instagram · 🌐

Butler flats lighthouse. Not just a navigational aid, but a landmark that provides safety, comfort, and joy to all seafarers who are returning from sea. Also one of the last landmarks all men who left to sea and never returned saw on their way out from the port of New Bedford. I'll never forget the feeling I get when passing by this lighthouse on my way home, ask any fisherman or merchant mariner that sail to and from New Bedford, they'll explain the best they can. [#butlerflats](#) [#newbedford](#) [#commercialfishing](#) [#merchantmariner](#) [#slaveofthesea](#)



👍 29

6 Comments

➦ Share



**LONGTIME CHAMPIONS AND VOLUNTEERS
HUGH MURRAY AND PETE DUFF AT BUTLER FLATS**



HUGH MURRAY

195 Maryland Street
New Bedford, Massachusetts 02745-2555
EMail - Murray724@AOL.com



41° 36.2' N
70° 53.7' W

Friday, April 7, 2017

Dear Rob:

In trying to clean out Hughie's desk, I came across the enclosed.
I hope you will be able to use some of this material, because a lot of it
is original. Please let me know if it's useful to you, as there might be lots
more to come.

Respectfully,

Hazel Murray



HUGH MURRAY

195 Maryland Street
New Bedford, Massachusetts 02745-2555
EMail - Murray724@AOL.com



41° 36.2' N
70° 53.7' W

COPY

November 7, 2005

James Dollins, Vice President
AFC Cable Company
New Bedford Industrial Park
New Bedford, MA 02745

Re: Butler Flats Lighthouse

Dear Mr. Dollins:

Butler Flats Lighthouse, originally constructed between 1896-1898, was officially added to the National Register of Historic Places on June 15, 1987. It is a caisson-style lighthouse in the middle of the New Bedford Harbor and can be seen from many points on the waterfront - a proud symbol of the rich maritime history of the New Bedford area.

When the Coast Guard deemed it necessary to deactivate this historic structure, New Bedford residents rallied to save it and it has been preserved and privately maintained as a private aid to navigation by a group of volunteers. I presently serve as Volunteer Crew Chief.

We want this Lighthouse to continue to welcome New Bedford Seafarers back to their homeport, and in connection with the maintenance of this Lighthouse, I am in need of 100' of #6/3 covered MC cable.

Can you assist us in this worthwhile project of maintenance and preservation?

Very truly yours,

Hugh J. Murray
Volunteer Crew Chief

ATTACHMENT F


**EVIDENCE OF LISTING ON
STATE REGISTER OF HISTORIC PLACES**

Massachusetts Cultural Resource Information System

MACRIS

[MHC Home](#) | [MACRIS Home](#)

For more information about this page and how to use it, [click here](#).

Inventory No: NBE.908 
Historic Name: Butler Flats Light Station
Common Name:
Address: Acushnet River
New Bedford Channel
City/Town: New Bedford
Village/Neighborhood:
Local No: 8
Year Constructed: 1898
Architect(s): Smith, F. Hopkinton
Architectural Style(s):
Use(s): Lighthouse
Significance: Engineering; Maritime History; Military; Transportation
Area(s): [NBE AD: Lighthouses of Massachusetts](#)
Designation(s): Nat'l Register TRA (06/15/1987); Nat'l Register Individual Property (06/15/1987)
Building Material(s):



[New Search](#)

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Massachusetts Cultural Resource Information System

MACRIS

[MHC Home](#) | [MACRIS Home](#)

For more information about this page and how to use it, [click here](#).

Inventory No: NBE.AD
Historic Name: Lighthouses of Massachusetts
Common Name:
Address:

City/Town: New Bedford
Village/Neighborhood:
Local No:
Year Constructed:
Architect(s):
Architectural Style(s):
Use(s): Lighthouse Related
Significance: Architecture; Engineering; Maritime History; Military; Transportation
Area(s):
Designation(s): Nat'l Register TRA (06/15/1987)
Building Material(s):

Digital Photo
Not Yet
Available

[New Search](#)

[Previous](#)

[MHC Home](#) | [MACRIS Home](#)

ATTACHMENT G

**PHOTOS DOCUMENTING THE
CONDITION OF THE PROPERTY**











ATTACHMENT H

CONDITION ASSESSMENT REPORTS

ATTACHMENT H-1

PARE CORPORATION VISUAL INSPECTION FINDINGS REPORT FEBRUARY 2014

PREPARED FOR:
NEW BEDFORD HARBOR DEVELOPMENT COMMISSION

**VISUAL INSPECTION FINDINGS REPORT
PORT OF NEW BEDFORD
BUTLER FLATS LIGHT STATION**



February 2014

Table of Contents

**VISUAL INSPECTION FINDINGS REPORT
BUTLER FLATS LIGHT STATION
NEW BEDFORD, MASSACHUSETTS**

- A. INTRODUCTION
- B. GENERAL
- C. INSPECTION FINDINGS AND RECOMMENDATIONS
- D. OPINION OF PROBABLE COST

APPENDICES

- A. PHOTOGRAPHS
- B. 1989 INSPECTION REPORT

This report has been prepared by:



Karl W. Hammond, P.E.



**INSPECTION FINDINGS REPORT
BUTLER FLATS LIGHT STATION**

**NEW BEDFORD HARBOR
MASSACHUSETTS**

A. INTRODUCTION

This report presents the findings of a visual condition inspection and assessment of the Butler Flats Light Station, located in the mouth of the Acushnet River off of East Rodney French Boulevard in New Bedford, Massachusetts. Pare Corporation (PARE) completed the inspection of the Light Station at the request of and under contract to the New Bedford Harbor Development Commission. The inspection was completed on August 12, 2013.

The purpose of the inspection and the Inspection Findings Report is to provide the New Bedford Harbor Development Commission and the City of New Bedford with an understanding of the present condition of the Light Station, including deficiencies and recommended repairs, and an opinion of probable cost to restore the structure to its original condition.

B. GENERAL

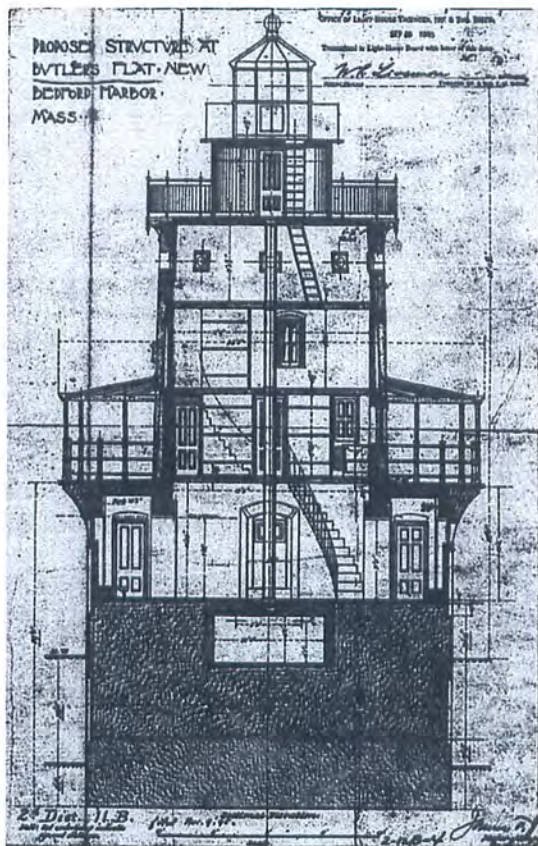
The inspection of the Butler Flats Light Station was performed by Pare Corporation (PARE) professional engineering staff, experienced in the condition inspection and assessment of waterfront structures. The facility was inspected on August 12, 2013, from a boat and from accessible areas of the structure itself. Photographs in Appendix A document conditions at the time of the inspection. An underwater investigation was not carried out as part of this inspection.

Review of existing information indicates that the Butler Flats Light Station was originally constructed in 1898, utilizing a 35 foot diameter cast iron caisson structure filled with concrete and stone as a foundation. The caisson bears on hardpan approximately 17 feet below Mean Low Water, achieved with removal of approximately 5 feet of dredged material. The Light Station is a “sparkplug style” brick and cast iron structure, and was last painted in 1998.

The superstructure is comprised of seven interior levels including a basement, main deck and kitchen/engine room, parlor, sleeping quarters, watchroom and deck, and lantern room. The walls of the structure are comprised of brick masonry, with the deck and railings of cast iron construction.

The facility was previously inspected in 1989 by Dyer/Brown and Associates, Inc. Architects. A copy of this inspection report is provided in the appendices, and provides a good historical overview of the facility.





C. INSPECTION FINDINGS AND RECOMMENDATIONS FOR RESTORATION

The following conditions and deficiencies were observed during the inspection of the Light Station structure. It is understood that the Light Station has been neglected, likely since 1998, and the harsh marine environment has resulted in the significant corrosion of the cast iron, in general. The brick masonry walls were noted to be in generally good condition, with few exceptions as noted. Photographs in Appendix A document the observed conditions.

Exterior components:

- The cast iron caisson shell (at the base of the structure) was observed to be significantly corroded and deteriorated. Breaks at the welds were observed near the high water line, and evidence of previous patching was observed. As the cast iron shell serves to protect the concrete foundation from ice and water damage, it is recommended that repairs be carried out to maintain and restore this function. It has been assumed that the concrete foundation is intact and performing as intended, as no differential settlement or cracking of the structure was noted. Repairs to the cast iron caisson shell should consist of inspection, blast cleaning, welding, patching, and the application of an epoxy paint. An underwater inspection will need to be provided as part of the inspection of the cast iron, and repairs completed as required.
- The north access ladder was observed to be in poor condition, and at the time of inspection, was

only accessible during periods of high tide. The welded connections were observed to be corroded, and the ladder was noted to be nearing the end of its useful life. Since the time of inspection, the ladder has further deteriorated such that it is presently considered un-usable and unsafe. It is recommended that the ladder be removed and replaced with a new ladder. Similarly, the south access ladder requires replacement.

- Along the main deck, the cornice (along the edge of the deck), the handrail and roof support posts are cast iron. Some items are more severely corroded than others, however the extent of corrosion is such that significant repairs are required. In particular, the southern area of the main deck, at the access location, the cornice is completely missing for about 20 feet, and severely corroded for an additional length. One roof support post is broken off at the base, the concrete deck was observed to be broken. Numerous sections of the handrail are missing. Major repair and replacement is required in this area, as the replacement of the cornice will require the removal of the roof support posts and railing. Additionally, repairs to corroded handrail components are required. If the Light Station is to be considered for public access, it is recommended that the railing be replaced in its entirety.
- The main deck roof structure was observed to be in generally fair condition, with no major deficiencies noted. Corrosion was observed on the underside of the roofing and at the roof supports. It is recommended that these components be blast cleaned and epoxy painted.
- The main body of the Light Station is brick masonry, which was observed to be in generally good condition. It is recommended that the masonry walls be cleaned and painted.
- The watch gallery deck is cast iron diamond plate, with a cornice and handrail supported by cast iron brackets. The cast iron deck plate was observed to be in generally good condition. The support brackets were observed to be corroded and broken, and the handrailing was observed to have significant corrosion. It is recommended that the watch gallery deck be replaced in its entirety.
- The lantern gallery deck and railing are cast iron. The diamond plate decking was observed to be slightly corroded. Similarly the handrail was observed to be corroded, with section loss observed at the base of the vertical posts and along the handrail itself. It is recommended that the lantern gallery deck be cleaned, painted, with the handrail and posts repaired as necessary.
- The lantern housing was observed to be in generally fair condition. Some separation of the roof and flashing members were observed. Water staining and corrosion was observed on the inside the structure. It is recommended that the lantern housing be cleaned, sealed and epoxy painted.

Interior Components:

- The brick masonry walls were observed to be in generally good condition. Cleaning and painting of the walls will be required.
- Windows were observed to be in generally good condition, with cleaning and painting required.
- Doors were observed to be in fair condition. The watchroom door was seized and could not be opened. The doors exhibited some corrosion and require cleaning and painting. Wooden thresholds were observed to be deteriorated.



- The interior plywood floors and ceilings were observed to have peeling paint, and appeared to have moisture issues. Mold was thought to be present, especially in the kitchen area. Removal and replacement of the wood and plywood floors and wall partitions is recommended.

Utilities:

- Electrical service is provided to the Light Station from shore. The electrical system was observed to be energized, however the condition of the electrical system was not considered as part of this inspection.
- The Light Station is provided with a cistern for the holding of fresh water. The condition of the cistern was not included as part of this inspection.
- The existing sanitary sewer system is suspected to discharge directly into the harbor. A chemical toilet was observed at the facility.

D. OPINION OF PROBABLE COST

If the Butler Flats Light Station is to be restored to its original condition, substantial repair and replacement of existing deteriorated components is required. As the site is a remote location, accessible by water only, the rehabilitation effort is made even more difficult. Light Station Restoration Contractors specialize in work of this nature, and it is recommended that their opinion and expertise be sought prior to the preparation of a detailed Light Station restoration scope of work and associated budget.

Based on our understanding of marine construction and industry unit prices, PARE has compiled the following “order of magnitude” opinion of probable cost for the scope of work provided herein. As many factors come into play with construction of this nature, the costs provided are intended to be a guideline only.

1. Mobilization and Demobilization	\$40,000
2. Crane Barge at \$4,000/day for 30 days	\$120,000
3. Repair Cast Iron Caisson Shell, Sandblast, Paint	\$100,000
4. Replace Ladders	\$20,000
5. Main Deck Gallery Cornice, Railing, Posts	\$120,000
6. Masonry Cleaning and Painting	\$60,000
7. Watch Gallery Handrail Replacement	\$50,000
8. Lantern Gallery Handrail Replacement	\$30,000
9. Lantern Housing Cleaning and Painting	\$20,000
10. Cleaning and Painting Interior	\$100,000
11. Replacement of Wood Ceilings, Floors and Walls	\$50,000
12. Cleaning and Painting of Stairs, Handrails	\$25,000
Subtotal Construction	\$735,000
25% Contingency	\$184,000
15% Engineering and Oversight	<u>\$110,000</u>
Total Light Station Restoration	\$1,029,000

APPENDIX A – PHOTOGRAPHS





Photo No. 1: View of corroded and deteriorated caisson shell, north ladder and main gallery handrail.



Photo No. 2: View of south ladder and deteriorated handrail posts.



Photo No. 3: View of south face of lighthouse.



Photo No. 4: View of west side of brick masonry superstructure and watch gallery.



Photo No. 5: View of lantern housing and upper galleries.



Photo No. 6: View of deteriorated north ladder.



Photo No. 7: View of deteriorated cornice and handrail on main gallery.

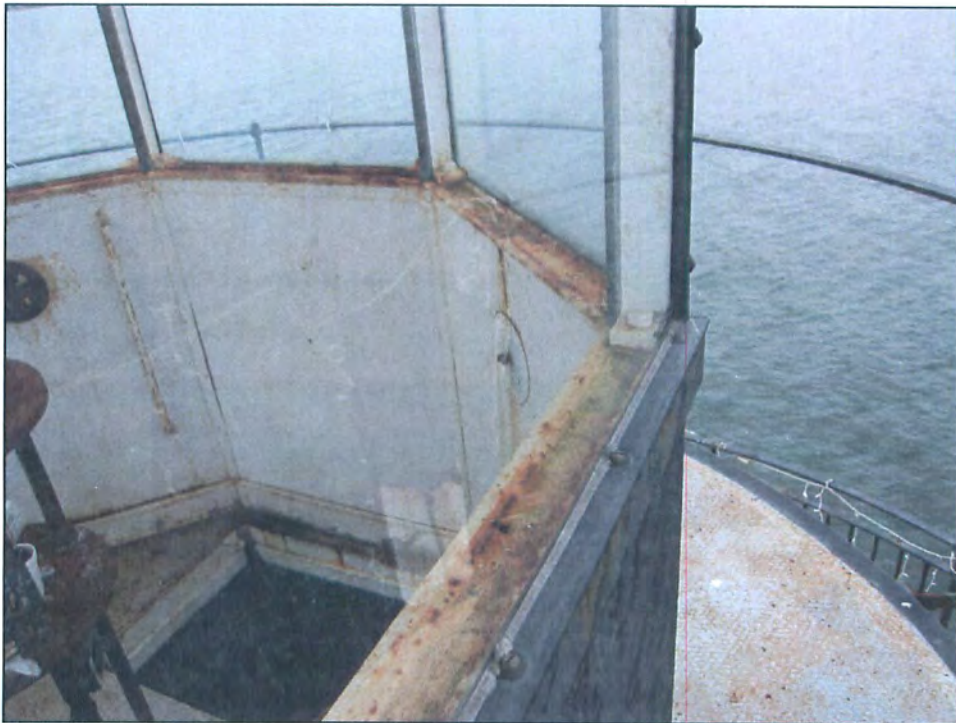


Photo No. 8: View of lantern room.



Photo No. 9: Interior masonry wall and stairs.



Photo No. 10: View of ceiling.



Photo No. 11: Interior walls.



Photo No. 12: Typical basement walls.

APPENDIX B – 1989 INSPECTION REPORT



ATTACHMENT H-2

**BECKER STRUCTURAL ENGINEERS
CONDITION ASSESSMENT REPORT
DECEMBER 2016**



December 30, 2016

Rob Draper
Rincon Etal Investments, Inc.
P.O. Box 31000
Tucson, AZ 85751
(520) 571-2210
robert.draper@comcast.net

WO3910
Butler Flats Lighthouse
New Bedford, Massachusetts

Dear Robert,

I am providing this summary report regarding our observations and findings associated with our site visit to the above noted lighthouse on September 28, 2016. Our findings reflect our site observations and are supported by background information provided by you prior to our visit which included:

1. Visual Inspection Findings Report dated February 2014 prepared by Pare Corporation
2. GSA Office of Real Property Utilization and Disposal Invitation to Bid, IFB Sale/Lot #: BOSTN-115-011-001
3. Historic Lighthouse Improvement Project Restoration Plan dated August 1989 prepared by Dyer Brown & Associates, Architects
4. Butler Flats Lighthouse original design drawings dated 1888
5. Conimicut Light House new boat mooring and ramp dated 6-29-1994

Background

From the GSA invitation to bid, the following background information is provided as an excerpt:

Butler Flats Light ("the Property") was constructed in 1898, and consists of a 53-foot high brick and cast iron sparkplug-style tower resting on a caisson foundation. There are four (4) levels including two (2) watch decks, a basement and three (3) floors of living space that are approximately 37-feet in diameter with 15-foot head clearance. There is approximately 1091 usable sq. ft. It was designed by Francis Hopkinson Smith and is listed on the National Register of Historic Places, reference #87001528.

The Property is located in New Bedford Channel at the mouth of the Acushnet River, offshore from the City of New Bedford, County of Bristol, Massachusetts. It is accessible by boat only.

GSA will not provide access to the interior of the Property. The exterior ladder has been severely compromised by rust.

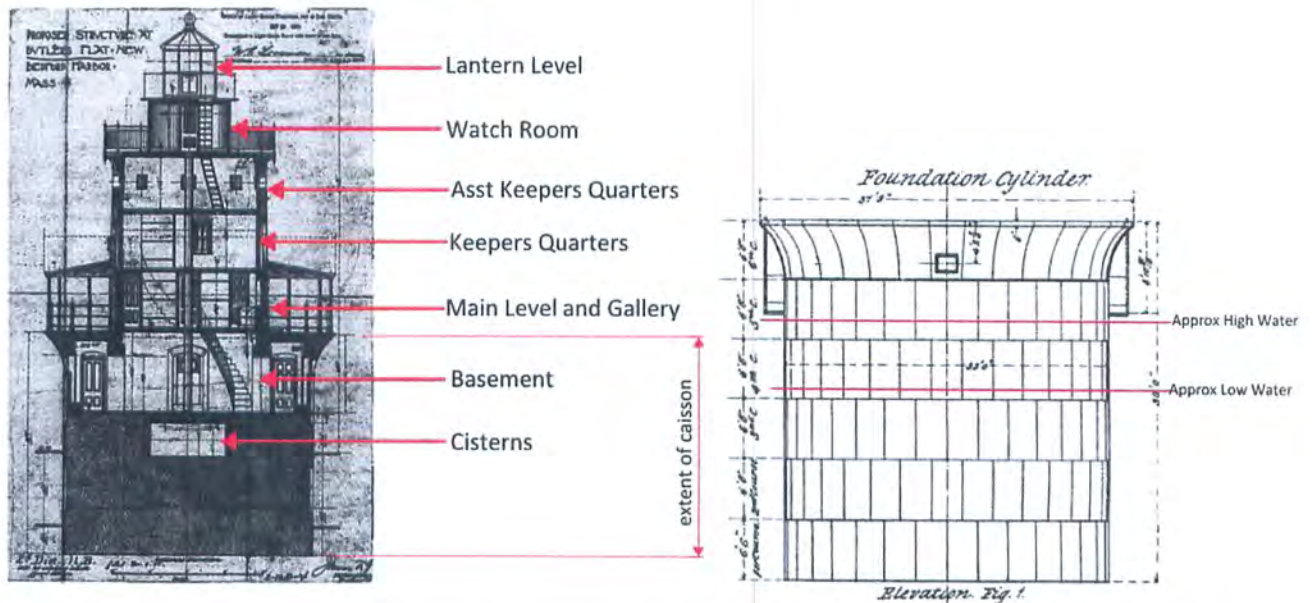
The Property is located at Latitude: 41.603894 Longitude: -70.894505.

There is no sound signal.

The Property is not a Federal Aid to Navigation (ATON), nor a Private Aid To Navigation. The US Coast Guard decommissioned the ATON in 1978.

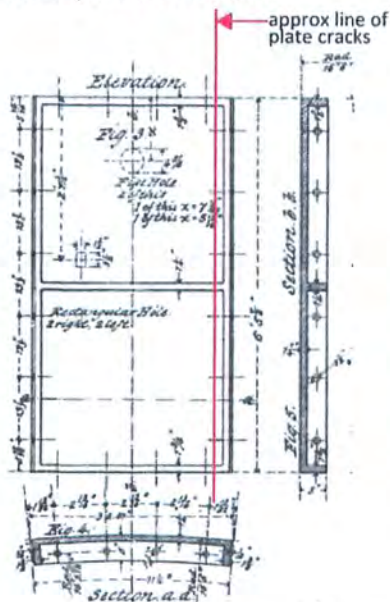
Observations

Our observations were limited to items and conditions which were visible and accessible. No selective demolition, probing or testing of materials was performed. Our observations are supplemented by photographs provided at the rear of this report. Please refer to the annotated cross-section below to define the regions of the lighthouse noted in our observations.



Caisson

The cast-iron caisson is constructed of a series of flanged plates which are 1.25 inches thick in the field of the plate and a flange which is 1.375 inches thick. The plates are fastened together from the interior of the caisson using bolts. The plates are stacked six high to form the caisson cylinder as shown. Tidal action occurs between the second and third plate down from the gallery deck level, with MLW at approximately 20 feet below the top of the caisson.



There is significant cracking of plates, especially within the tidal zone where a consistent defect is noted to include vertical cracks in the plates directly adjacent to and running parallel with the return flanges of the plates. We have observed this same crack pattern on the Spring Point Light House in Portland, ME and the Lubec Lighthouse in Lubec, ME. It is our opinion that stress concentrations occur at the transition of the face plate to the connection flanges caused by thermal expansion pressure as the plates heat in the summer and then freeze-thaw expansion caused by water caught within the annular space between the plate and concrete surface in the winter. It is unclear the condition is exacerbated by a metallurgical flaw in the castings. The initial construction of the caisson



was held somewhat watertight by linseed soaked jute placed between the plate joints. As the jute deteriorated, water was introduced to the plate joints and worked its way into any cavities that existed between the plates and concrete. The freeze thaw action of the trapped water puts highly expansive pressure on the plate joints and the thinner face plate seems to yield first. As more water is introduced into the cavity, the process intensifies and speeds up. It should be noted that historic records of the Spring Point Lighthouse in Portland, show repairs to plates being undertaken as early as 1929.

The concrete core of the caisson is the primary structural support element of the light house with the cast iron caisson providing protection of the concrete from vessel strikes, ice, and erosion due to wave action. The issue is that the caisson has been breached which allows salt water to attack the concrete through freeze thaw action. The larger the gaps become, the more water enters the annular space and the more aggressive the cycle becomes. We believe the solution will include the following:

1. Have a diver conduct a thorough underwater survey of the plates, noting damage and gaps.
2. Open those areas of the caisson plate that have been damaged and remove loose concrete debris to the greatest extent possible.
3. Repair plate damage with new belt plates, making the caisson as water tight as possible.
4. Pump out seawater in annular space.
5. Pump in a grout mix to fill annular space between caisson plates and the existing concrete core.
6. Blast clean the plates
7. Paint the caisson with a marine epoxy paint.
8. Replace access ladders with in-kind ladders or boat mooring and ramp system similar to that proposed for Conimicut Light by the USCG.

Basement

1. In general, masonry in the basement appears to be in good condition with little moisture/freeze-thaw damage to the bricks. Existing loose paint should be removed. Cracks should be repaired by raking and repointing or injecting with an epoxy filler.
2. Arched masonry lintels over openings (4'-0" max) into areas for coal, wood, oil, and provision storage are cracked in several locations and these will need to be rebuilt.
3. Steel beams supporting brick flat arches over these storage areas (supports gallery deck concrete) are corroded and must be blast cleaned and painted. Embedded ends should be checked for deterioration which may be hidden from view.
4. Steel beams and deck plates forming main floor level are corroded and should be blast cleaned and painted.



5. Stair to basement is in good structural condition but must be blast cleaned and painted.
6. Cisterns were not observed. The 1989 report by Dyer Brown & Associates indicated that they were clean and sound.

Main Floor Level and Gallery

1. Concrete at the main floor exterior gallery is in very poor condition with large, open cracks, spalls and generally large gaps at the caisson interface. Water entering at the caisson interface is adding to the deterioration of the concrete core and freeze-thaw action on the caisson plates.
2. The concrete at the Gallery Deck should be removed and replaced and then coated with a membrane.
3. Embedded iron circular and square hatches to the basement and hatches at the ladders are in very poor condition. They are inoperable, unsecured and open, allowing water to enter the basement. These need to be re-fabricated and cast into the new gallery slab.
4. Railings are deteriorated and sections are missing. These should be repaired and/or replaced.
5. Exterior brick masonry is in good condition except for masonry in the area of the gallery roof beam embedment. The localized masonry is cracked and spalled, possibly from water infiltration and associated freeze thaw.
6. Gallery roof beams and plates appear to be in good structural condition but need to be blast cleaned and painted.
7. Interior masonry is in good condition but paint is peeling. It should be stripped and repainted.

Keeper's Quarters

1. Interior masonry is in good condition but paint is peeling. It should be stripped and repainted.
2. Windows have been blocked closed on the exterior with glass block. Window frames and sashes have been removed on the interior. The masonry cavity is exposed where the window frame was positioned. The glass block is leaking and there is green mildew present on the interior masonry of the south and west openings. The masonry around the windows should be rebuilt to create a more watertight opening. Glass block should be replaced or windows should be rebuilt and replaced.
3. The granite sill is cracked on the south opening. The crack should be epoxy injected.
4. The iron ceiling deck plates that form the floor of the assistant keeper's quarters are in good condition but should be cleaned and painted.



Assistant Keeper's Quarters

1. The brick walls are in good condition at this level and are approximately 18 inches thick.
2. The porthole windows are in good condition and are operable.
3. The exterior cast iron floor deck plates at the watch level are leaking which has entered the assistant keeper's quarters and has damaged the wood ceiling and the plywood replacement floor. These wood elements need to be removed and replaced.

Watch Level

1. Watch Room interior floor plates are in good condition. Exterior floor plates at watch level gallery are structurally sound but corroding and the joints are leaking. The plates should be power tool or blast cleaned. The joints between exterior plates should be sealed and then the plates should be painted with a marine epoxy.
2. The interior wood bead board walls and ceiling are intact and in good condition. The walls and ceiling finishes should be scraped and painted.
3. Exposed steel should be power tool or blast cleaned and repainted.
4. Door on east side opening to watch level gallery has a broken hinge on the bottom of the right leaf. This need to be fabricated and replaced. Other hinges should be inspected and cleaned to provide operable door with good seal.
5. The railings and balusters are in poor condition with advanced corrosion. We suggest the railing elements be replaced in kind or with a fiberglass or other non-ferrous product.

Lantern Level

1. The cast iron wall panels are in good structural condition. They should be power tool cleaned and painted.
2. The glazing of the lantern level appears to be intact and in good condition.
3. Window frame bolts are missing on the exterior in several locations and should be replaced. The holes will need to be drilled to remove the old bolt body and then tapped to clean the threads and receive a new bolt.
4. Deck plates are in good structural condition. The plates should be power tool or blast cleaned. The joints between exterior plates should be sealed and then the plates should be painted with a marine epoxy.
5. The railings and balusters are in poor condition with advanced corrosion. We suggest the railing elements be replaced in kind or with a fiberglass or other non-ferrous product.
6. The roof vent ball may be the source of leaks into the lantern level. The vent ball baffles should be inspected for deterioration and repaired if required.
7. Roof of lantern room should be cleaned and repainted.



Exclusions

We did not evaluate the electrical service although there was power provided by a cable to the shore.

We did not evaluate the heating system.

We did not evaluate the existing sanitary sewer system. Originally sewage was discharged directly into the harbor. Most recently a chemical toilet was used at the facility as noted in the Pare report from 2014.

Opinion of Cost

In consideration of cost, the 2014 Pare report provided an itemized list of repair items and an "order of magnitude" opinion of potential costs. I have used their original list and expanded the items based on our observations and have updated costs based on our more recent observations and adjustments for inflation and a strengthening economy.

1. Mobilization/Demob	\$40,000.00
2. Crane Barge at \$4500/day for 45 days	\$202,500.00
3. Repair Cast Iron Caisson Shell, blast, paint	\$200,000.00
4. Replace Ladders, add platform	\$80,000.00
5. Main Deck gallery Concrete-remove/replace+castings	\$50,000.00
6. Main Deck Gallery cornice, railings and posts	\$120,000.00
7. Masonry Repairs, cleaning and paint	\$75,000.00
8. Watch Gallery deck cleaning/paint	\$10,000.00
9. Watch Gallery handrail replacement	\$60,000.00
10. Lantern gallery handrail replacement	\$30,000.00
11. Lantern Gallery deck cleaning/paint	\$10,000.00
12. Lantern Housing clean/paint	\$20,000.00
13. Replace wood ceilings and floors	\$50,000.00
14. Clean wood ceilings and floors	\$30,000.00
15. Interior stairs-clean/paint	\$30,000.00
16. Clean and paint interior	\$100,000.00

Subtotal Construction	\$1,107,500.00
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25% Contingency	\$276,875.00
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15% Engineering	\$166,125.00
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Total Estimated Restoration Costs	\$1,550,500.00
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Closing

We trust this information will be helpful as you determine your future needs regarding the possible purchase and restoration of the Butler Flats Light. Please understand that our observations and findings are based on an approximately four hour visit to the lighthouse and there may be items that were not observed during our visit. We have accounted for this in the contingency of our opinion of cost. Using the information contained in this report, a specialty contractor experienced in this type of work should be engaged to provide a contractor's estimate of costs based on work items, access, time of year and OSHA and environmental requirements. Please contact me if you have any questions or need additional information.

Sincerely,
Becker Structural Engineers, Inc.

Paul B. Becker, PE
President



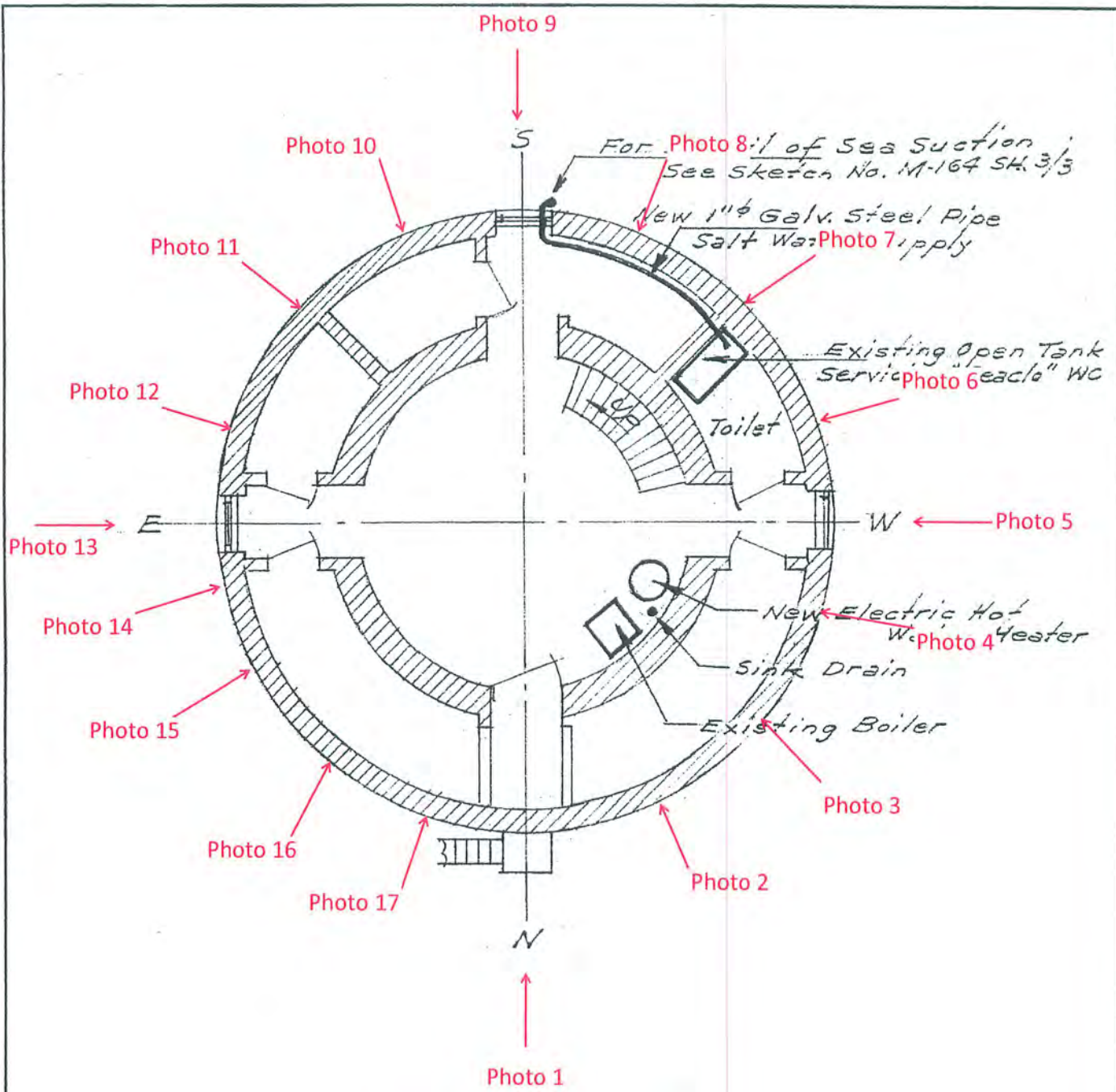


PHOTO LOCATIONS

BUTLER FLATS L/S
SANITARY FACILITIES

DRAWN BY: J. W. M. DATE MAY 3, 1960
CHECKED BY: *aat*
APPROVED BY:

IC.G.D. SKETCH NO.
M-164 SHEET 1/3

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 1-view from due north



Photo 2-view 22 degrees west of north



Photo 3-view 45 degrees west of north



Photo 4-view 70 degrees west of north

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 5 -view due west



Photo 6- view 22 degrees south of west



Photo 7 -view 45 degrees south of west



Photo 8- view 70 degrees south of west

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 9 -view from south



Photo 10 -view 22 degrees east of south



Photo 11 - view 45 degrees east of south



Photo 12 -view 70 degrees east of south

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 13 view from due east



Photo 14 -view 9 degrees north of east



Photo 15 -view 18 degrees north of east



Photo 16 – view 27 degrees north of east

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 17 -view 36 degrees north of east



Photo 18 – prior plate repair



Photo 19 – prior plate repair and vertical crack adjacent to plate joint



Photo 20 – vertical crack adjacent to plate joint

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 21 – gaps at prior plate repairs



Photo 22 – gaps at prior plate repairs



Photo 23 – gaps at prior plate repairs



Photo 24 – gaps at prior plate repairs with vertical crack adjacent to plate joint



Photo 25 – vertical and horizontal joints



Photo 26 – masonry missing paint and stained from bracket rust

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 27 – missing plate at cornice



Photo 28 – missing hatch and ladder and ladder supports



Photo 29 –deteriorated hatch in gallery deck



Photo 30 –cracked concrete at gallery deck

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 31 – cracked concrete at gallery deck and deteriorated hatch



Photo 32 – cracked concrete at gallery deck



Photo 33 – cracked concrete at gallery deck



Photo 34 – gap in concrete at caisson interface

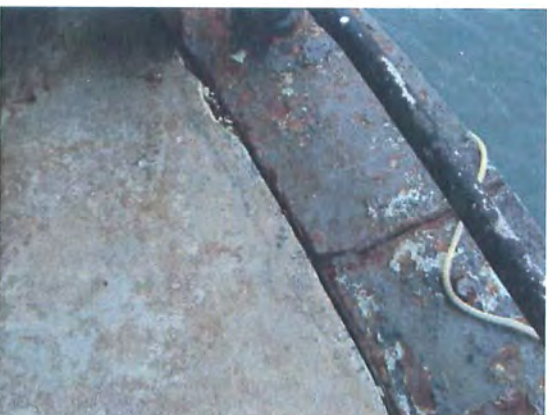


Photo 35 – gap in concrete at caisson interface



Photo 36 – corroded railing at gallery level

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 37 –corroded hatch and cracked concrete



Photo 38 –missing railing



Photo 39 –corroded beam at gallery floor plate as viewed from basement



Photo 40 –grass growing in gallery concrete crack



Photo 41 –caisson cornice plate crack and concrete crack



Photo 42 –masonry damage at gallery roof beam embedment

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 43 – masonry damage at gallery roof beam embedment



Photo 44 –basement stair



Photo 45 –corrosion at underside of main level deck plates



Photo 46 –corrosion at gallery hatch and floor beams



Photo 47 – corrosion at gallery hatch and floor beams



Photo 48 – corrosion at gallery hatch and floor beams + cracked masonry lintel

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 49 –masonry wall crack



Photo 50 – crack at brick masonry arch bearing point



Photo 51 –corroded beams at main level gallery floor support



Photo 52 – corroded beams at main level gallery floor support



Photo 53 –main level support column and deck plates



Photo 54 – corroded beams at main level gallery floor support

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 55 – Main level deck plate



Photo 56 –cracked masonry lintel at main level



Photo 57 – cracked masonry lintel at main level, failing arch bricks



Photo 58 –peeling paint on masonry



Photo 59 –corrosion at wall and deck plates in asst keeper's qtrs



Photo 60 – corrosion deck plates in asst keeper's qtrs

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 61 –moisture infiltration causing mildew



Photo 62 –cracked granite lintel and separation of brick wythes



Photo 63 –gaps in brick wythes exposed by loss of window frames and trim



Photo 64 – gaps in brick wythes exposed by loss of window frames and trim



Photo 65 –corroded stair support framing into masonry arch.



Photo 66 –wood threshold at watch deck needs replacement

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 67 –rust on watch deck gallery plates



Photo 68 – rust on watch deck gallery plates and on stanchion



Photo 69 –corroded fog horn brackets



Photo 70 –rust on watch level wall plates

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 71 –corroded watch level railing elements



Photo 72 – corroded watch level railing elements



Photo 73 –corrosion on watch level wall plates



Photo 74 –corrosion on watch level plates

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 75 –corrosion on watch level brackets



Photo 76 –corrosion on watch level railing



Photo 77 –lantern room wall coorsion at window sill



Photo 78 –rust staining from leaking vent ball



Photo 79 –lantern level railing corrosion



Photo 80 –missing fasteners at lantern level window frames

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 81 – missing fasteners at lantern level window frames



Photo 82 –corrosion on lantern level railings



Photo 83 – corrosion on lantern level railings



Photo 84 – corrosion on lantern level railings



Photo 85 –corrosion at lantern level roof eave



Photo 86 –corrosion on chimney brackets

Butler Flats Lighthouse, New Bedford, Massachusetts



Photo 87 –chimney opening



Photo 88 –vent ball with corrosion and failed paint on lantern level roof



Photo 89 – vent ball with corrosion and failed paint on lantern level roof

End of Photos and End of Report

ATTACHMENT I

SITE PLAN

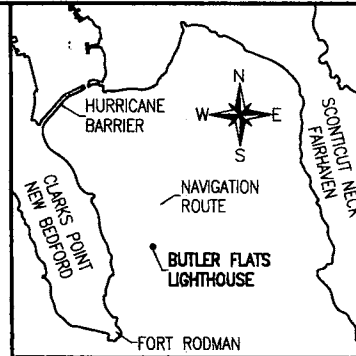
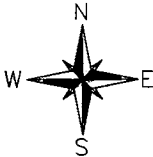
"I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED
IN CONFORMANCE WITH THE RULES AND REGULATIONS
OF THE REGISTERS OF DEEDS OF THE COMMONWEALTH
OF MASSACHUSETTS."

Timothy M. Gauvin
TIMOTHY M. GAUVIN

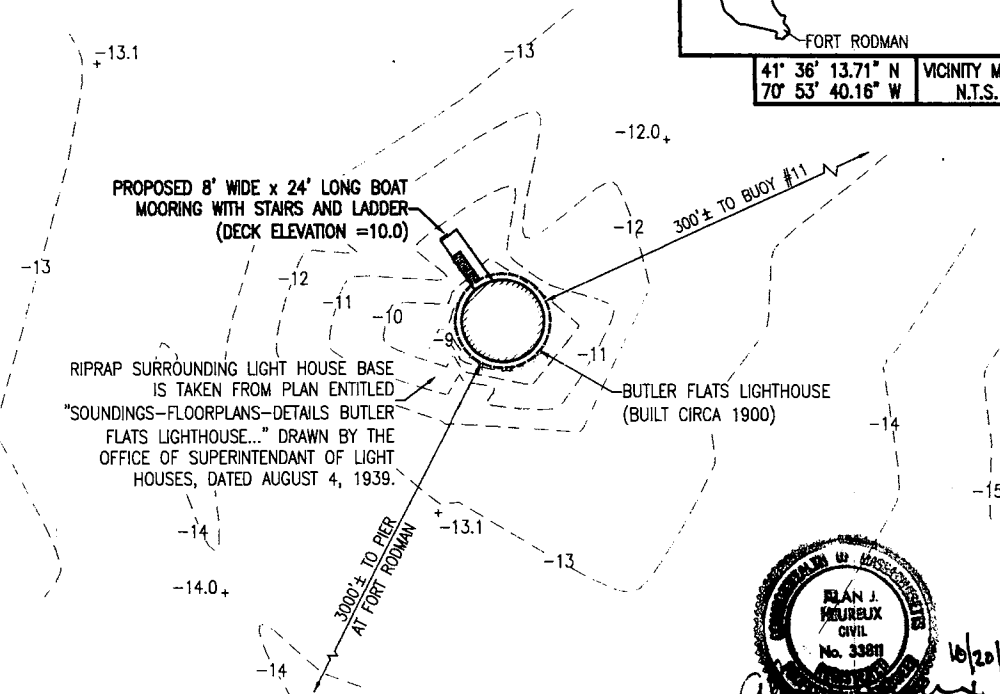
10/20/17
DATE

— LEGEND —

---10--- EXISTING CONTOUR
-13.1
+ SPOT ELEVATION ON
OCEAN BOTTOM



41° 36' 13.71" N VICINITY MAP
70° 53' 40.16" W N.T.S.



BENCHMARK:

SURVEY DISC #7 LOCATED IN THE NORTHWEST CORNER OF THE
SECOND CONCRETE STEP OF THE BATTERY WALCOTT NO. 2
ELEVATION = 9.03 (MLW DATUM)
(TIDAL B.M. DISK I.D. LW0072, 1931)

50 0 50
SCALE 1" = 50' (FEET)

PLAN ACCOMPANYING THE PETITION OF MASS LIGHT
LTD., TO LICENSE, REPAIR AND MAINTAIN AN EXISTING
LIGHTHOUSE AND PROPOSED BOAT MOORING AT BUTLER
FLATS, NEW BEDFORD, MA IN BUZZARDS BAY.

DWG#: 3263-01-WWLP.dwg APRIL 7, 2017
SHEET 1 OF 3 REVISED THROUGH: OCTOBER 20, 2017

Boucher & Associates, Inc.
CIVIL ENGINEERS LAND SURVEYORS

648 AMERICAN LEGION HIGHWAY
WESTPORT, MASSACHUSETTS 02790
DATUM: MEAN LOW WATER ELEVATION = 0.0

LICENSE PLAN NO. 14502

Approved by Department of Environmental Protection
of Massachusetts

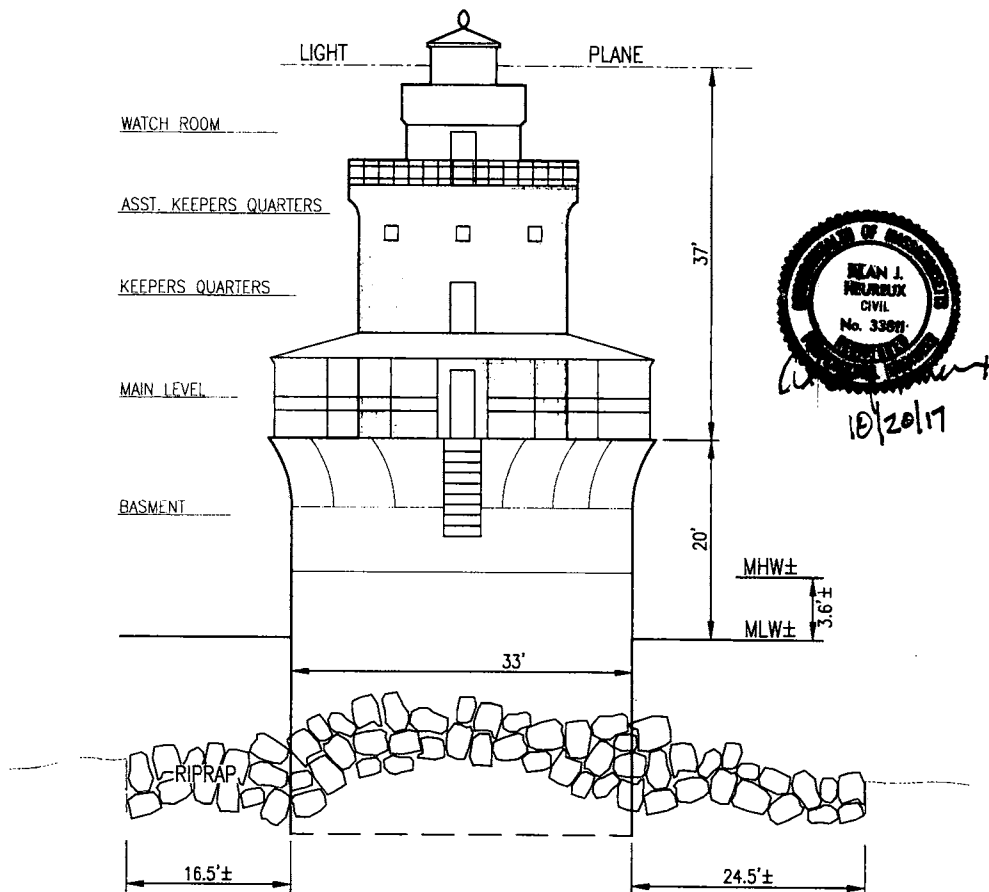
David E. Hill

David E. Hill

"I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED
IN CONFORMANCE WITH THE RULES AND REGULATIONS
OF THE REGISTERS OF DEEDS OF THE COMMONWEALTH
OF MASSACHUSETTS."

Timothy M. Gauvin
TIMOTHY M. GAUVIN

10/20/17
DATE



SOUTH ELEVATION
(NOT TO SCALE)

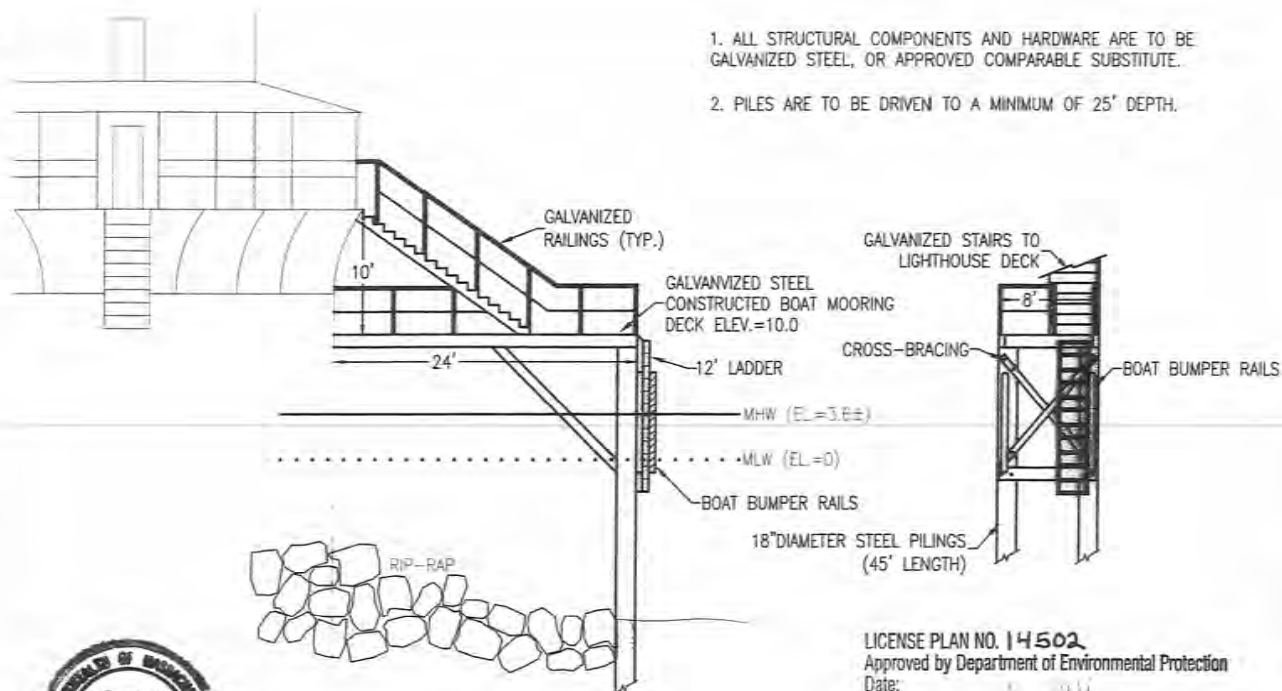
LICENSE PLAN NO. 14502
Approved by Department of Environmental Protection
Date: 10/17

PLAN ACCOMPANYING THE PETITION OF MASS LIGHT
LTD., TO LICENSE, REPAIR AND MAINTAIN AN EXISTING
LIGHTHOUSE AND PROPOSED BOAT MOORING AT BUTLER
FLATS, NEW BEDFORD, MA IN BUZZARDS BAY.

Boucher & Associates, Inc.
CIVIL ENGINEERS / LAND SURVEYORS

648 AMERICAN LEGION HIGHWAY
WESTPORT, MASSACHUSETTS 02790
DATUM: MEAN LOW WATER ELEVATION = 0.0

DWG#: 3263-01-WWLP.dwg APRIL 7, 2017
SHEET 2 OF 3 REVISED THROUGH: OCTOBER 20, 2017



1. ALL STRUCTURAL COMPONENTS AND HARDWARE ARE TO BE GALVANIZED STEEL, OR APPROVED COMPARABLE SUBSTITUTE.

2. PILES ARE TO BE DRIVEN TO A MINIMUM OF 25' DEPTH.

PROFILE VIEW PROPOSED BOAT MOORING

DATUM: MEAN LOW WATER ELEVATION = 0.0

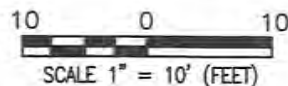
LICENSE PLAN NO. 14502
Approved by Department of Environmental Protection
Date: 10/20/17

DISPLACEMENT VOLUME:

LIGHTHOUSE = 114 CUBIC YARDS

PILES = 0.5 CUBIC YARDS

TOTAL VOLUME = 114.5 CUBIC YARDS



APRIL 7, 2017
REVISED THROUGH: OCTOBER 20, 2017

DWG#: 3263-01-WWLP.dwg



Boucher & Associates, Inc.
CIVIL ENGINEERS / LAND SURVEYORS

648 AMERICAN LEGION HIGHWAY
WESTPORT, MASSACHUSETTS 02790

PLAN ACCOMPANYING THE PETITION OF MASS LIGHT LTD., TO LICENSE, REPAIR AND MAINTAIN AN EXISTING LIGHTHOUSE AND PROPOSED BOAT MOORING AT BUTLER FLATS, NEW BEDFORD, MA IN BUZZARDS BAY.

"I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED IN CONFORMANCE WITH THE RULES AND REGULATIONS OF THE REGISTERS OF DEEDS OF THE COMMONWEALTH OF MASSACHUSETTS."

Timothy M. Gavin
TIMOTHY M. GAVIN

10/20/17
DATE

ATTACHMENT J

CHAPTER 91 WATERWAYS LICENSE

The Commonwealth of Massachusetts



No. 14502

Whereas, Mass Light, Ltd.

of -- Tucson -- in the State of -- Arizona --, has applied to the Department of Environmental Protection for license to -- repair and maintain a lighthouse, and construct and maintain a boat mooring --

and has submitted plans of the same; and whereas due notice of said application, ~~and of the time and place fixed for a hearing thereon~~, has been given, as required by law, to the -- Mayor and City Council -- of the -- City of New Bedford. --

NOW, said Department, having heard all parties desiring to be heard, and having fully considered said application, hereby, subject to the approval of the Governor, authorizes and licenses the said

-- Mass Light, Ltd. --, subject to the provisions of the ninety-first chapter of the General Laws, and of all laws which are or may be in force applicable thereto, to -- repair and maintain a lighthouse, and construct and maintain a boat mooring --

in and over the waters of -- Buzzards Bay -- at -- Butler Flats -- in the -- City of New Bedford -- and in accordance with the locations shown and details indicated on the accompanying DEP License Plan No. 14502 (3 sheets).

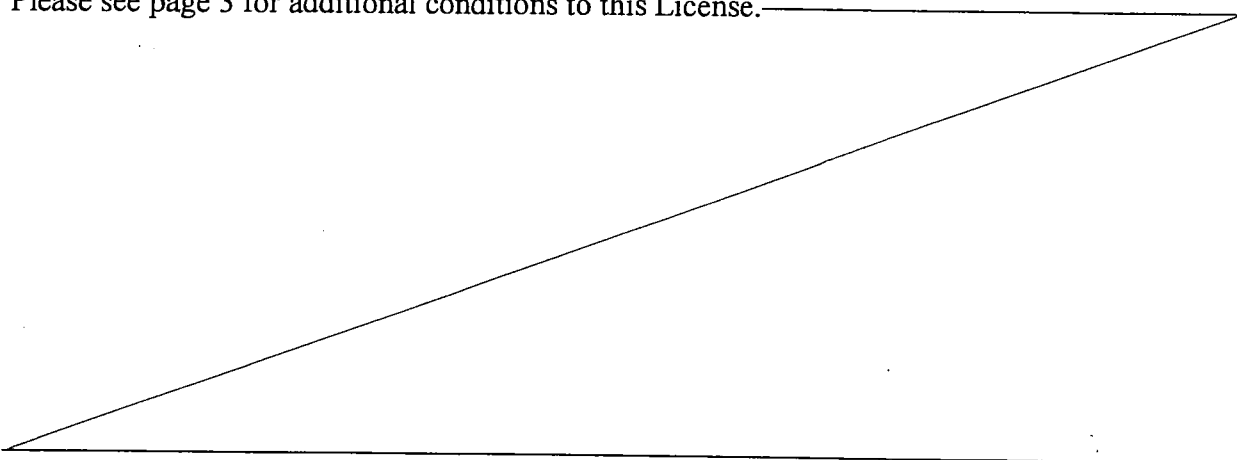
The structures hereby authorized shall be limited to the following use(s): noncommercial docking and boating access to navigable waters, and a water dependent, private aid to navigation.

This license will expire thirty (30) years from the date of license issuance. By written request of the licensee for an amendment, the Department may grant a renewal for the term of years not to exceed that authorized in the original license.

Special Waterways Conditions:

1. The refurbished lighthouse is to be used as a water-dependent, private aid to navigation. The structure shall not be used for private or commercial residential purposes without first seeking a change in use approval from the Department.
2. No dredging (including, but not limited to the effects of prop wash) is permitted herein.
3. All work authorized herein shall be completed within five (5) years of the date of license issuance. Said construction period may be extended by the Department for one or more one year periods without public notice, provided that the Applicant submits to the Department, thirty (30) days prior to the expiration of said construction period, a written request to extend the period and provides an adequate justification for said extension.
4. Within sixty (60) days of completion of the licensed project, the Licensee shall request, in writing, that the Department issue a Certificate of Compliance in accordance with 310 CMR 9.19. The request shall be accompanied by a certification by a registered professional engineer licensed in the Commonwealth that the project was completed in accordance with the License.

Please see page 3 for additional conditions to this License.



Duplicate of said plan, number 14502 is on file in the office of said Department, and original of said plan accompanies this License, and is to be referred to as a part hereof.

STANDARD WATERWAYS LICENSE CONDITIONS

1. Acceptance of this Waterways License shall constitute an agreement by the Licensee to conform with all terms and conditions stated herein.
2. This License is granted upon the express condition that any and all other applicable authorizations necessitated due to the provisions hereof shall be secured by the Licensee prior to the commencement of any activity or use authorized pursuant to this License.
3. Any change in use or any substantial structural alteration of any structure or fill authorized herein shall require the issuance by the Department of a new Waterways License in accordance with the provisions and procedures established in Chapter 91 of the Massachusetts General Laws. Any unauthorized substantial change in use or unauthorized substantial structural alteration of any structure or fill authorized herein shall render this Waterways License void.
4. This Waterways License shall be revocable by the Department for noncompliance with the terms and conditions set forth herein. This license may be revoked after the Department has given written notice of the alleged noncompliance to the Licensee and those persons who have filed a written request for such notice with the Department and afforded them a reasonable opportunity to correct said noncompliance. Failure to correct said noncompliance after the issuance of a written notice by the Department shall render this Waterways License void and the Commonwealth may proceed to remove or cause removal of any structure or fill authorized herein at the expense of the Licensee, its successors and assigns as an unauthorized and unlawful structure and/or fill.
5. The structures and/or fill authorized herein shall be maintained in good repair and in accordance with the terms and conditions stated herein and the details indicated on the accompanying license plans.
6. Nothing in this Waterways License shall be construed as authorizing encroachment in, on or over property not owned or controlled by the Licensee, except with the written consent of the owner or owners thereof.
7. This Waterways License is granted subject to all applicable Federal, State, County, and Municipal laws, ordinances and regulations including but not limited to a valid final Order of Conditions issued pursuant to the Wetlands Protection Act, G.L. Chapter 131, § 40.
8. This Waterways License is granted upon the express condition that the use of the structures and/or fill authorized hereby shall be in strict conformance with all applicable requirements and authorizations of the MassDEP.
9. This License authorizes structure(s) and/or fill on:

___ Private Tidelands. In accordance with the public easement that exists by law on private tidelands, the licensee shall allow the public to use and to pass freely upon the area of the subject property lying between the high and low water marks, for the purposes of fishing, fowling, navigation, and the natural derivatives thereof.

X Commonwealth Tidelands. The Licensee shall not restrict the public's right to use and to pass freely, for any lawful purpose, upon lands lying seaward of the low water mark. Said lands are held in trust by the Commonwealth for the benefit of the public.

___ a Great Pond of the Commonwealth. The Licensee shall not restrict the public's right to use and to pass freely upon lands lying seaward of the high water mark for any lawful purpose.

___ Navigable River and Streams. The Licensee shall not restrict the public's right to use and to pass freely, for any lawful purpose, in the waterway.

No restriction on the exercise of these public rights shall be imposed unless otherwise expressly provided in this license.
10. Unless otherwise expressly provided by this license, the licensee shall not limit the hours of availability of any areas of the subject property designated for public passage, nor place any gates, fences, or other structures on such areas in a manner that would impede or discourage the free flow of pedestrian movement thereon.

The amount of tide-water displaced by the work hereby authorized has been ascertained by said Department, and compensation thereof has been made by the said -- Mass Light, Ltd. -- by paying into the Treasury of the Commonwealth -- two dollars and zero cents (\$ 2.00) -- for each cubic yard so displaced, being the amount hereby assessed by said Department.
(0 cy = \$ 0.00)

Nothing in this License shall be so construed as to impair the legal rights of any person.

This License shall be void unless the same and the accompanying plan are recorded within 60 days from the date hereof, in the Registry of Deeds for the County of Bristol.

IN WITNESS WHEREAS, said Department of Environmental Protection have hereunto set their hands this 1st day of December in the year two thousand seventeen.

for Program Chief David E. Hill

Commissioner Mt f Sg

Department of Environmental Protection

THE COMMONWEALTH OF MASSACHUSETTS

This license is approved in consideration of the payment into the treasury of the Commonwealth by the said -- Mass Light, Ltd. --

of the further sum of -- zero dollars and zero cents (\$0.00) --

the amount determined by the Governor as a just and equitable charge for rights and privileges hereby granted in the land of the Commonwealth.

BOSTON,

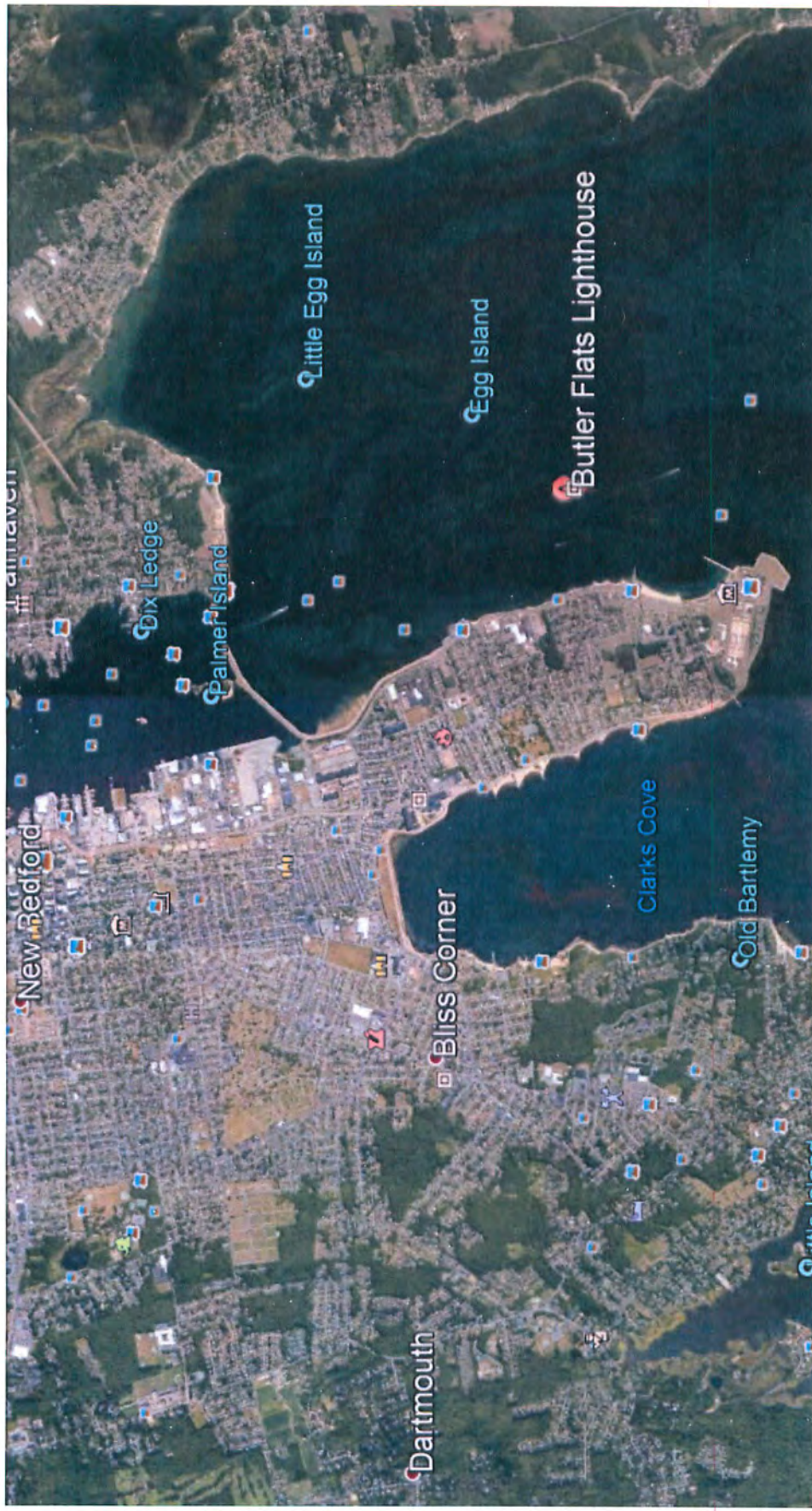
Approved by the Governor.

Charles D. Blawie

Governor

ATTACHMENT K

MAP OF SITE LOCATION



ATTACHMENT L

EXAMPLES OF SOURCES OF GRANT FUNDING FOR LIGHTHOUSE PRESERVATION

ATTACHMENT L-1

NATIONAL MARITIME HERITAGE GRANTS



Cape Hatteras Lighthouse, NC

Maritime Heritage

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National Maritime Heritage Grant Recipients

In the 2016 grant cycle, 27 grants in 13 states and the District of Columbia, totaling \$1,752,073 were awarded under the third round of the re-established Maritime Heritage Grants Program. Successful applicants include state and tribal governments as well as private non-profit organizations. The projects funded include maritime education and information access projects; exhibit and heritage trail development; preservation of ships, lighthouses, and other maritime properties; and survey and conservation of underwater archeological resources. The projects that received funding are listed by state below.

The list of [grant recipients from 1998](#) and [grant recipients from 2014](#) and [2015](#) is also available.

[California](#) | [Connecticut](#) | [District of Columbia](#) | [Florida](#) | [Iowa](#) | [Illinois](#) | [Maine](#)
[Maryland](#) | [Massachusetts](#) | [New York](#) | [North Carolina](#) | [Ohio](#) | [Pennsylvania](#) | [Rhode Island](#) | [Virginia](#)

California

Creating an Interpretive Master Plan for the tugboat *Angels Gate*

- Recipient: Friends of the Los Angeles Maritime Museum
- Amount: \$40,000
- Project: The Friends of the Los Angeles Maritime Museum seek to create an interpretive master plan for the Museum's tugboat ANGELS GATE (ex-ST-695, ex L.A.H.D. #10). On most days during Museum operating hours, ANGELS GATE is secured at the dock and inaccessible to the public. An interpretive master plan will create the capacity for the Museum to develop an enhanced visitor experience. Through physical access and interpretation of the tugboat, visitors will learn not only the history of ANGELS GATE but the vessel's role in the growth of the Port of Los Angeles and how that growth contributes to a broader, national maritime story.

Preservation of the Steam Ferry *Berkeley*

- Recipient: Maritime Museum Association of San Diego
- Amount: \$200,000
- Project: This project will support preservation of the steam ferry *Berkeley*, constructed in 1898 and, in its time, the largest steam ferry in service in the United States.

Educational Programs Interpreting Naval and Maritime History Through STEM

- Recipient: Aircraft Carrier *Hornet* Foundation
- Amount: \$15,000
- Project: This project will tell the stories of selected areas of maritime technology via the USS *Hornet* and her decorated history. Educational programming for both youth and adults will use a crossover approach to STEM and history to increase awareness and understanding of the connections between these areas. The \$15,000 amount requested will help develop curriculum and online resource materials, provide enhanced presentation tools and equipment, and conduct outreach and marketing for these programs.

Maritime Discoveries Along the California Coast

- Recipient: University of Southern California
- Amount: \$49,968
- Project: USC will digitize for free online public access via the USC Digital Library and Digital Public Library of America 380 16mm film reels and 3,000 photographic negatives and prints tracing Captain Allan Hancock's voyages of discovery from the 1930s to 1960s aboard the *Velero 3* and *Velero 4*. Capt. Hancock's maritime explorations contributed significantly to our understanding of U.S. coastal waters and the flora and fauna of the Eastern Pacific, and his films and photographs document important marine habitats like Catalina Island, the Channel Islands National Park, Baja California, and the Galapagos Islands.

The Living Brotherhood of the Tomol: Chumash Paddlers' Active Legacy

- Recipient: Channel Islands Maritime Museum
- Amount: \$35,562
- Project: A select handful of prestigious Tomol Captains and rowers with admirable strength and endurance make annual voyages from the Channel Islands Harbor to the local Channel Islands; nearly a twenty mile trek. It is not simply a single crossing to the island – it is an affirmation of Chumash tradition and culture. The Channel Islands Maritime Museum intends to present this experience to its visitors in the best way possible, short of having them balancing in a vessel off a dock in our Harbor. With the help of the Chumash Mariners, an interactive "display Tomol" will be created specifically for visitors to experience elements of an annual crossing.

Connecticut

From Clippers to Containers

- Recipient: Mystic Seaport Museum
- Amount: \$46,659
- Project: Mystic Seaport Museum will create a long-term exhibit that will deliver to a broad public audience important historic content about the changes in the maritime cargo trades from the mid-19th century to the present.

Maritime History Sails on Long Island Sound

- Recipient: Sound Waters
- Amount: \$43,500
- Project: SoundWaters Maritime History Sails on Long Island Sound are designed to accomplish the following: 1) to engage the general public, which is largely unaware of coastal maritime history, with an understanding of the significance of 19th and early 20th century oyster fisheries; 2) to recognize the historical design and use of the sharpie schooner in our region; 3) to highlight the role Long Island Sound played in the settlement of the coast and development of industry; and 4) to create a deeper understanding of place and local maritime culture.

Creating a Blue Heritage Trail for Southeast Connecticut

- Recipient: University of Connecticut
- Amount: \$22,780
- Project: The University of Connecticut will create a "Blue Heritage Trail" by integrating information resources related to Southeast Connecticut's maritime history, cultures, economy, ecology and scientific endeavors. The project will create at least a dozen interpretative panels as markers/stops of the trail for locations in the City of Groton, City of New London, and Town of Stonington.

District of Columbia

Naval Documents of the American Revolution Digitization Project

- Recipient: Naval Historical Foundation
- Amount: \$49,700
- Project: Since the 1950s, the Navy has maintained an ongoing effort to chronologically publish important naval documents of the American Revolution – the founding papers of the United States Navy. Volume I was published with an introduction written by President Kennedy. The most recent twelfth hard-bound volume with an introduction from President Obama takes the series up through May 1778. The series has provided primary source materials to numerous scholarly books and articles. Given the scarcity of complete hard-bound sets and the proven capabilities of the internet as a research repository, the Naval Historical Foundation desires to make these and future volumes accessible online.

Florida

The Pensacola Maritime Mural Gallery

- Recipient: University of West Florida Historical Trust
- Amount: \$26,630
- Project: The Pensacola Maritime Mural Gallery will consist of two installation spaces for large-scale murals in downtown Pensacola. These spaces will display a rotating collection of murals that highlight the importance of Pensacola's historic waterfront and maritime enterprises to the city, region, nation, and world. As part of a comprehensive interpretative program for the city's rich maritime heritage, the "Pensacola Maritime Mural Gallery" will complement the Pensacola Maritime Heritage Trail, the Historic Pensacola Village, and the FPAN's Destination Archaeology Resource Center.

The Florida Shellfish Trail Website and Signage

- Recipient: University of Florida
- Amount: \$34,332
- Project: This project will highlight the rich maritime history and culture of the shellfish harvesting industry in Florida by developing a website and educational signage for the Florida Shellfish Trail.

Maritime Archaeology Educational Field Program II

- Recipient: Diving With a Purpose
- Amount: \$50,000
- Project: Two field programs will be conducted in 2017. Each field program session is one week of classroom instruction, educational presentations, and three days of diving, documenting the wreck sites. In addition, continuous and multi-faceted public education and outreach efforts will be performed during the year. Forums include public lectures, seminars, school presentations as well as social media. Youth participating in the program will be required to participate in outreach activities at their school.

Iowa

Lead Based Paint Abatement of *William M. Black*

- Recipient: Dubuque County Historical Society
- Amount: \$66,999
- Project: The National Mississippi River Museum & Aquarium will employ a complete lead-based paint abatement process to permanently eliminate all deteriorated lead-based paint and its hazards on the steam dredge *William M. Black*, a National Historic Landmark.

Illinois

U-505 Submarine Preservation Accessibility Expansion Project

- Recipient: Museum of Science and Industry Chicago
- Amount: \$82,277
- Project: The U-505 Submarine Preservation and Accessibility Expansion Project will expand preservation efforts and create enhanced access to key areas of the WWII U-505 German Submarine that are historically

and technically significant. The project objectives include: 1) Increase public access to previously restricted elements of the historic *U-505*, 2) Increase access for ongoing preservation and research, and 3) Preserve the national historic landmark in perpetuity for the American people by promoting awareness and appreciation of our nation's maritime heritage associated with the submarine.

Maine

Educational Exhibit about the Fresnel Lens and the Lighthouse Lantern

- Recipient: Maine Maritime Museum
- Amount: \$42,141
- Project: The Maine Maritime Museum will create a full-scale replication of the lantern room of the Cape Elizabeth Light so that visitors can appreciate the lens within the context of its original setting. "Into the Lantern" will give visitors an historical perspective of the role of lighthouses and the evolution of lantern lighting, as well as a current-day view of what takes place in our busy harbors and an appreciation of the value of maritime industries to our economy and culture. This exhibit will make it possible for people—who might otherwise be physically unable to negotiate the steps of a real lighthouse tower or who have no access to a lighthouse—to have the simulated experience of being at the top of a lighthouse.

The Apprenticeshop: Maritime Skills Program

- Recipient: Atlantic Challenge Foundation
- Amount: \$50,000
- Project: This program targets high school students through hands-on curriculum that complements the standards and lessons necessary for graduation. Traditional boat building, sailing and seamanship are the experiences we use to challenge individuals to explore their personal potential. Our STEM-centered courses include such offerings as The Science of Boatbuilding, The Physics of Sailing, and Maritime History: Traditions and Storytelling.

Maryland

US Coast Guard *Taney* Hull Preservation Project

- Recipient: Living Classrooms
- Amount: \$152,389
- Project: The objective of the project is to place the historic US Coast Guard Cutter TANEY in drydock to clean, stabilize and preserve the underwater hull body. This is a continuation of a long-term effort to preserve the ship which includes a successful dry dock period in 2003 which was used as a model project for historic ships internationally via the Historic Naval Ship's Association's public knowledge base. At the time of this application, the riveted steel hull of USCGC TANEY has been afloat continuously for over 13 years by which time underwater protective coatings have begun to reach the limit of their effectiveness. By meeting the objective of this project, the hull will be effectively preserved, thus preventing more extensive and costly repairs in the future.

Massachusetts

USS *Constitution* in Context: Enhancing Visitor Experience Online and Onsite

- Recipient: USS Constitution Museum
- Amount: \$50,000
- Project: The goal of "USS *Constitution* in Context: Enhancing Visitor Experience Online and Onsite" is to present USS *Constitution* in the context of the Charlestown Navy Yard to inspire visitors, residents, and educators to view the Navy Yard and all its historic landmarks as an integrated learning opportunity and a must-see destination. The USS *Constitution* Museum seeks to develop seamless digital and physical resources to enhance visitor understanding and engagement connected to USS *Constitution* and the historic Charlestown Navy Yard. The project will produce a redesigned and reinterpreted web site that will serve as an inviting orientation and engaging interpretive gateway to *Constitution* and the national landmark Charlestown Navy Yard campus.

New York

1930 Tug *W. O. Decker*: Preservation of Deck and Certificate of Inspection

- Recipient: South Street Seaport Museum
- Amount: \$200,000
- Project: The goal of this project is to rebuild the deck and all associated sub-structures. Rebuilding the deck structure along with upgrading the mechanical systems on the tug will allow us to renew *Decker's* USCG Small Passenger Vessel certificate and obtain a Dockside Attraction Vessel certificate; these capabilities will enable the tug to reach a greater audience while providing her with a revenue stream to support future maintenance costs. Once restored, she will again take students and visitors on educational excursions around New York Harbor, help in the training of students (including those that will enter the maritime workforce and historic preservation), and serve as a functioning tugboat for the Museum's fleet of historic vessels.

Digitizing and Disseminating Whaling Collection Online

- Recipient: Whaling Museum Society
- Amount: \$49,557
- Project: The Whaling Museum proposes to digitize, preserve, and create publicly available online access to 2500 items from its permanent collection which document American whaling history, including digitally scanned and photographed scrimshaw, tools, and prints. The project will dramatically increase access to these significant and historical collections by producing publicly accessible digital archives on the Museum's website, and will enhance public awareness and appreciation for the key role whaling played in our country's maritime heritage. The website will also be enhanced with object-based curricula and teaching materials for schools which highlight using the online collection in classrooms.

Telling the Stories of Sailors' Snug Harbor

- Recipient: Stephen B. Luce Library Research Foundation
- Amount: \$50,000
- Project: The archives of Sailors' Snug Harbor, housed at SUNY Maritime's Stephen B. Luce Library, contain incredible and largely untapped riches that tell overlapping stories of maritime history, New York City history, nineteenth-century literature and art, and medical and social history. This project will digitize, preserve, and create online access to at least 4000 items from the collection for researchers, genealogists, and educators. Library faculty with expertise in metadata creation and access will create a sustainable, interactive digital collections platform to deliver content to the public.

Ohio

Enhancing Awareness and Educational Opportunities for Historic Lake Erie Lighthouses

- Recipient: Ohio Sea Grant and Stone Laboratory
- Amount: \$18,540
- Project: This project will enhance public awareness of the South Bass and Green Island lighthouses and their importance in maritime history by developing five educational signage stations. The South Bass Island lighthouse grounds are open to the public, but lack information on the rich history of the property, architecture, and light keeper history. Green Island is currently a wildlife refuge and is now closed to the public. Fortunately, Green Island can be easily viewed from the South Bass Island lighthouse property, making it the ideal location for telling the story and highlighting the connection between the two islands and lighthouses.

Pennsylvania

Cruiser *Olympia* Hull Repairs and Assessment Surveys

- Recipient: Independence Seaport Museum
- Amount: \$107,689
- Project: This project will continue critical repairs to the portside hull of *Olympia* along wind and waterline (area of most extreme corrosion). Using a surface piercing cofferdam, approximately 112 linear feet will be cleaned and scaled to bare metal using Sponge-Jet blasting media, coated with Enecon ceramic epoxy paste and painted with marine-quality epoxy paint. Additionally, we will conduct galvanic testing to determine the hull's electrical potential at her berth and undertake two post-application hull condition assessments.



Rhode Island

Southeast Lighthouse Tower Cast Iron Restoration, Block Island

- Recipient: South East Lighthouse Foundation
- Amount: \$105,000
- Project: During this phase of restoration work to historic Southeast Lighthouse, the watch level gallery cast iron deck plates, soffits, bi-leaf door, railings and beams will be restored, repaired and recast as necessary. The watch room cast iron drum and brick nogging will also be restored and repaired. Work will also be undertaken to restore, repair and replace as necessary the lantern level cast iron plates, railings, registers and ladder. The internal cast iron tower staircase will be restored and repainted. Masonry will be re-pointed as necessary on the lighthouse exterior, primarily on the north and west façades where mortar loss is significant.

Virginia

Expanding Analysis Capabilities for USS *Monitor* Conservation and Interpretation

- Recipient: The Mariners' Museum
- Amount: \$83,350
- Project: The primary objective of this project is to acquire a Fourier Transform Infrared Spectrometer, which will be used to identify organic-based compounds of USS *Monitor* artifacts via non-destructive analysis. The project also calls for hiring a degreed chemist, who will be tasked with supporting the conservation staff by performing all chemical analyses and materials characterization in the laboratory.

Yorktown Shipwrecks Education Program

- Recipient: The Watermens Museum
- Amount: \$30,000
- Project: The Yorktown Shipwrecks Project will develop and present hands on educational programs that will explore the maritime cultural history of the American Revolution focusing on the 1781 Yorktown shipwrecks. The objectives are 1. To develop and present non-invasive, hands on STEAM focused maritime history program focusing on the 1781 maritime campaign, the French attempts to raise the sunken ships, and the archeological record of the site 2. To develop and present STEAM focused outreach program and traveling exhibit that will allow students to learn about maritime heritage through participatory exploration of the Betsy archeological story, 3. To present the outreach program at regional schools, partner sites, community forums and regional special events.

Battleship *Wisconsin* Engineering Project: Life in the Engine Room

- Recipient: Nauticus Foundation
- Amount: \$50,000
- Project: With the use of multimedia, tour guides will simulate the technology used to power the battleship's steam engines. A life-size actor in a fire suit will simulate lighting a boiler in Boiler Room 1, where visitors will be instructed on safety, bringing the fire up to temperature and grading the smoke by utilizing a periscope. As guests move through the ship's most dynamic operating system, the path of steam generated from heating water in the Boiler Room will be conveyed with the use of running lights. Finally in Engine Room 1 guests will power up a turbine engine through direct projection and animated cutaway technology. Guests will depart with engineering knowledge as it pertains to the steam engine and respect for those that served below the *Wisconsin's* waterline.

ATTACHMENT L-2

U.S. LIGHTHOUSE SOCIETY GRANTS

Preservation Grants Program

U.S. Lighthouse Society Preservation Grant Awards.

Read an editorial from The Keeper's Log magazine about the Society's Preservation Grant Awards program. (</sites/default/files/assets/First%20Grant%20Recipients.pdf>)

The United States Lighthouse Society is proud to announce the awards made in the second year of our pioneering Lighthouse Preservation Grants Program, this year sending help to lighthouse projects in the Great Lakes, on the Gulf Coast and along the Atlantic coast.

Presenting the Recipients of 2016

1. MORRIS ISLAND LIGHTHOUSE IN CHARLESTON, S.C.



The Save the Light Foundation in Charleston will use the grant from a USLHS corporate partner, the SeaPak Division of Rich Products Corp., to fund a detailed study of the very historic but erosion-threatened and decaying Morris Island Lighthouse. USLHS will administer the grant as part of the Preservation Grant Program established last year to mark the

Society's 30th anniversary. SeaPak Shrimp & Seafood Co. is located on St. Simons Island, GA, just down the coast from Charleston.

The Charleston group will use the grant to gain a definitive assessment of the condition of the cofferdam-protected lighthouse and determine what steps can be taken, in what order and at what cost, to preserve the structure. International Chimney Corp., movers of the Cape Hatteras Light and other lighthouses, will do the work.



2. SPRING POINT LEDGE LIGHTHOUSE TRUST IN MAINE



A grant was awarded to the Spring Point Ledge Lighthouse Trust in Maine to replace the lantern vent ball on the breakwater lighthouse and repair damage in the lantern caused by water intrusion.

3. MICHIGAN LIGHTHOUSE CONSERVANCY, ON THE GREAT LAKES



The Michigan Lighthouse Conservancy will use our grant to complete a \$32,375 project to fabricate and install damaged or missing parts of the handrail system in the tower and on the lantern gallery at the Muskegon South Pierhead Light.

4. PENSACOLA LIGHTHOUSE FOUNDATION IN FLORIDA



current system.

This grant will be used to restore a long-lost iron pedestal to the top of the lighthouse and use it to support a lens that now is suspended from the lantern roof. The pedestal was found a few years ago in the woods near the tower, and has been restored. The grant will go toward the \$17,589 project to place it back in its proper location to support the lens and curtail the structural damage caused by the

5. BOSTON LIGHT'S ICONIC BOATHOUSE



Lands' End leaders traveled in late July to Little Brewster Island in Boston Bay to present an initial \$30,000 check to restore the foundation of the Boston Light's iconic boathouse; the work will allow the boathouse to reopen to the public, and the gift celebrates the 300th anniversary of the country's first lighthouse.

In addition, the Society's program will administer a second grant from its first corporate preservation partner, the Lands' End clothing company of Wisconsin. Last year, Lands' End committed funding to launch the Society's detailed study of preservation needs at the Alcatraz Island Lighthouse, a Society project in San Francisco Bay. This year, Society and

Here is a video about the event at Boston Light



Presenting the Recipients of 2015

1. BLOCK ISLAND SOUTHEAST LIGHTHOUSE



Built in 1874 in the ornate Victorian style of the time, the Southeast Lighthouse helped ships clear the dangerous shoals and ledges of the "stumbling block" of the New England coast. Because of ongoing erosion of the bluffs, in 1993 the 2,000 ton structure was moved 300 feet back from the cliffs. After the move, the Coast Guard could not reinstall the mercury float lens so they installed the

first order lens which was originally at the Cape Lookout Lighthouse. Through our partnership with Lands' End, our grant will be used to help restore the lantern room where the light shines.

2. SENTINEL ISLAND, ALASKA



In Sentinel island, an isolated station 30 miles northeast of Juneau, the Gastineau Channel Historical Society will using their grant toward their lantern restoration project and will help replace or repair glass panels, restore iron framing and mullions, and install a replicated ball vent.

3. TOLEDO HARBOR, OHIO



At Toledo Harbor in Lake Erie, the Toledo Harbor Lighthouse Preservation Society's overall project is to replace 12 windows on the first story, restore one window, and obtain and install shutters (our grant's primary role) for all of them. The Toledo Harbor and Sentinel Island work is critically needed to protect those lighthouses from the environment and make them

watertight, and in each place our funding will be a vital part of larger and already ongoing projects that will have dramatic impacts on the lighthouses' futures.

4. RACE ROCK, NEW YORK

At Race Rock in Long Island Sound, the New London Maritime Society will be using their grant for a detailed plan to restore the brick masonry basement. The Race Rock funding will allow that organization to finish their planning and be more prepared to obtain funds for their follow-on execution.



5. ALCATRAZ ISLAND, CALIFORNIA

On October 5, 2015, Lands' End chartered a boat tour to Alcatraz Lighthouse to kick off a rehabilitation project for the historic structure. Members of the media and local officials were invited to join CEO Federica Marchionni as she announced funding for the Historic Structures Report, administered by the U.S. Lighthouse Society, with further support from the U.S. Coast Guard and the National Park Service. The report will take the first critical steps in ensuring the structure can be enjoyed for generations to come.

Visit the Lands' End page to see a nice video about the event and

project: [Alcatraz Island Rehabilitation Project](http://www.landsend.com/sustainability/alcatraz/)
(<http://www.landsend.com/sustainability/alcatraz/>)

[Donate to the Grants Program \(/lh-preservation-grant-donation\)](#)

[Program Description \(/about/preservation-grants-program\)](#)

[Program guidelines for applicants \(/about/preservation-grants-program-guidelines\)](#)

[Check out our Preservation Grants Brochure](#)

(https://uslhs.org/sites/default/files/assets/USLHS-Lighthouse_Preservation_Fund.pdf)

If you have any questions, please contact:

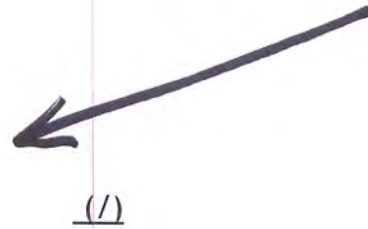
U.S. Lighthouse Society

Phone: (415) 362-7255

Email: grantsprogram@uslhs.org (<mailto:grantsprogram@uslhs.org>)

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(<http://illusio.com>).



Preservation Grants Program



Guidelines

Who is eligible to apply to the grant program?

Any non-profit organization that either owns or is legally responsible for the preservation of a lighthouse, light station, lightship or related entity that has an official 501(c)3 designation. Each eligible organization may submit only one grant

application during each grant cycle

For what purpose(s) can the grant funds be used?

1. Preservation planning (i.e., "non-capital" projects), for example, research at National Archives, designs, drawings, assessments, surveys, etc., and;
2. Preservation execution (i.e., "capital" or "bricks and mortar" projects)

How much grant funding can each applicant request?

Each application can request up to a maximum of \$10,000. We expect to award at least 2 grants with each application cycle.

Are matching funds required?

No match is required from the applicant. However, the applicant's availability of matching grants or funds will be given additional credit in the application evaluation.



How much time will the awardee(s) have to execute the grant project?

The target maximum time limit for the grant applicants to execute their project is 12 months. However, extensions can be considered and granted on a case-by-case basis if the work is progressing well but not fully completed within the 12 months.

What type of projects stand a better chance of being selected?

Projects for lighthouses, light stations or lightships that are on the National Register of Historic Places;

Planning or execution projects that require near-term work to stop, prevent or repair deterioration or damage to vital elements; failure to conduct the work will lead to further deterioration and possible failure of vital elements;

For example, a project whose scope is to stop water intrusion in a tower lantern room with an active aid to navigation and to repair damage caused by the water intrusion would be preferred over a project whose scope is to repoint interior brick areas in a light station oil house;

Planning or execution projects that require near-term work to address issues that very adversely affect safety or health of light station personnel or visitors;

Planning or execution projects where the specific element/feature on which the work is proposed to be done is itself historic;

For example, work on a historic lantern on a historic tower would take priority over work on a modern replica lantern, even if it were on a historic tower;

Planning or execution projects where discussions with the State Historic Preservation Office have already occurred, if required or if appropriate, and approval, if not already obtained, is anticipated in the near-term;

Planning or execution projects where qualified professional contractors, if they are to be used, have already been sought, and valid quotes have been obtained;

Planning or execution projects that have a total budget greater than the requested grant and have the additional funding in hand or it will be available in the near-term;

Can grants funds be used for routine maintenance?

No. Letters of Interest where the scope of work consists primarily of routine maintenance are not acceptable. Routine maintenance includes such things as periodic scraping and painting and application of other coatings, periodic roof replacements, etc.

Application, Selection and Execution Process

<p align="center"><u>Letter of Intent form</u> (/sites/default/files/assets/USLHS%20Letter%20of%20Interest%202017.pdf)</p>
<p align="center"><u>Grant Application</u> (/sites/default/files/assets/USLHS%20Grant%20Application%202017.pdf)</p>

******Both the Letter of Intent and Application cannot be filled out online. You must download (save) them to your computer, then they can be filled out and saved for submission.***

Step 1: Submit form Letter of Intent (LOI) by March 24, 2017 (electronic format only)

Two pages maximum with basic applicant information, project information and project cost information

Step 2: The USLHS Grant Selection & Management Committee will review the LOIs against the grant selection criteria;

The Grants Committee will then send a letter of acceptance to selected applicants by May 7, 2017 and invite them to submit a full application.

Step 3: Applicants submit a full application by June 19, 2017 (electronic format only)

Keep to a maximum of 5-6 pages (not including photos or drawings).

Step 4: Grants Committee members review all applications and score each application against the criteria and make recommendations to the full Board for approval;

Note that the Grant Committee members could also decide to award a lesser amount than requested to some applicants, as a function of several different factors, including the quality of the application as well as the total amount of grant funding available to award.

Step 5: USLHS will notify the winning applicant(s) by August 7, 2017 in writing of their amount and will forward a Letter of Acceptance for them to sign and return.





The Letter of Acceptance will include provisions for project performance and for appropriate recognition of and publicity about the USLHS by the grant awardee.

Step 6: Grant disbursement timing:

50% of the grant amount will be disbursed with the Letter of Acceptance and the other 50% at mid-term, assuming satisfactory performance by the grant awardee.

Step 7: Awardee(s) submit a mid-term report and a final report to USLHS at completion of grant project:

Nominally 12 months after award; the due dates and general content requirements for the report will be stipulated in the Letter of Acceptance.

Letter of Interest Evaluation Criteria

Does the applicant organization meet the criteria?

Is it a current, legal 501(c)3 non-profit organization recognized by the IRS? Does it own or have the legal authority to conduct the proposed preservation work?

Does the proposed scope of work meet the criteria?

Is it exclusively for preservation planning or for preservation execution?

Is the grant amount requested less than the maximum of \$10,000?

How vital/important does the project appear to be to the overall preservation program for the lighthouse, light station or lightship?

How feasible does the project appear to be for the grant amount requested? Does it appear that it can be done within 12 months of grant award?

How knowledgeable does the organization appear to be about what is required in a project of this nature? Does it have a known track record or reputation? Is it positive or negative?



Application Evaluation Criteria



Historic significance of lighthouse/lightship/light station

For example, is the lighthouse on the National Register (or determined eligible)?

Impact of grant project to lighthouse "health"

For example, is this proposal for a project that requires immediate attention because of deteriorating conditions, or for activities that will help complete an ongoing project, or something that could be put off for a year or more?

Feasibility of project

Is the scope/schedule/cost reasonable and realistic?

Soundness of technical approach

Is the technical approach sound in terms of appropriate historic preservation (e.g., do it comply with the Secretary's Standards for the Treatment of Historic Structures, State Historic Preservation Office requirements, etc.) and in terms of use of qualified resources to execute the project, etc?

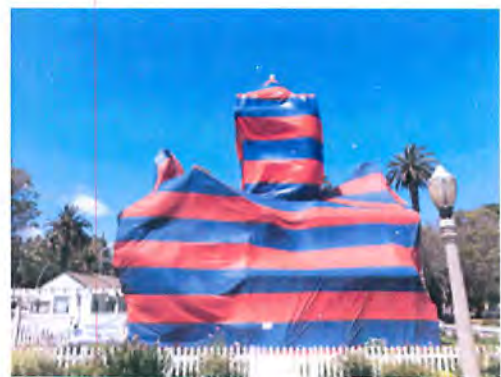
Management experience/track record of applicant

Does the applicant organization appear to have the requisite knowledge or experience to manage the project?

Does it have a track record? Is it positive or negative?

Does the selection of the key personnel indicate that the organization understands what skills are needed to execute the project?

Does the applicant organization appear to have adequate financial health to preserve this lighthouse, light station or lightship?



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[Program Description \(/about/preservation-grants-program\)](#)

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[\(https://uslhs.org/sites/default/files/assets/USLHS-Lighthouse_Preservation_Fund.pdf\)](https://uslhs.org/sites/default/files/assets/USLHS-Lighthouse_Preservation_Fund.pdf)

If you have any questions, please contact:

U.S. Lighthouse Society

Phone: (415) 362-7255

Email: grantsprogram@uslhs.org (<mailto:grantsprogram@uslhs.org>)

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United States Lighthouse Society / non-profit 501c3. Handcrafted by [Illusio Design](#)

[\(http://illusio.com\)](http://illusio.com).

ATTACHMENT L-3

AMERICAN LIGHTHOUSE FOUNDATION GRANTS

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Historic Preservation

“ Success in the field of lighthouse preservation is not realized in one moment in time, but rather by remaining passionately steadfast on a fascinating journey through time. Along the way is impossible not to recognize that this labor of love is as much about people and the communities in which we live, work and play, as the historic treasures themselves.” – Bob Trapani, Jr., A Executive Director

The American Lighthouse Foundation's historic preservation efforts have been going strong since 1994, and during this stretch of twenty-plus years, our organization has invested over \$3.5 million dollars in lighthouse restoration.

However, the amount of money spent on ALF lighthouses is only part of this inspiring achievement. Without our dedicated team of intrepid volunteers and professional staff – and the partnerships forged with entities like the U.S. Coast Guard, the State Historic Preservation Offices of Maine, New Hampshire, Massachusetts, Rhode Island and Connecticut, the National Park Service and Maine's Bureau of Parks and Lands, ALF would not be able to serve as a national leader in the field of lighthouse preservation.



**Wood Island Light Station
(Photo by Bob Trapani, Jr.)**



**ALF's volunteers make a meaningful difference at our
lights by helping with vital maintenance projects.
(Photo by Bob Trapani, Jr.)**

ALF's volunteers contribute 25,000 hours of their time to lighthouses on an annual basis. Thanks to their generous time, talents and passion, countless general maintenance projects are carried out, with vital fundraising and educational initiatives. The volunteers work in concert with ALF's staff to ensure that restoration projects adhere to the Secretary of the Interior Standards for Preservation.

Overall, the American Lighthouse Foundation cares for 32 historic light station structures (17 lighthouses, 6 keeper's houses, 3 boat houses and 1 whistle house).

There's more!

The American Lighthouse Foundation is keenly aware of the fact that while we work in the present to restore our historic light stations, the organization must simultaneously be working to cultivate a new generation of preservationists who will follow in the footsteps of our modern day "keepers."

This is accomplished through fun-learning public access programs at the light stations, educational initiatives and outreach efforts in the community that not only convey the importance of our lighthouse heritage, but show people how they can become involved and make a meaningful difference.



**ALF's public access programs are a lot of fun and show others about the importance of lighthouse preservation.
(Photo by Bob Trapani, Jr.)**

People working with people and the skills they exhibit – whether it be of time & talents or a financial gift, are the pillars of the American Lighthouse Foundation's ongoing success in the field of lighthouse preservation.

Join our team by volunteering your time & talents at one or more of our projects throughout New England or consider a donation to ALF's lighthouse preservation mission today!

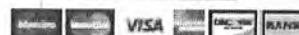


People working with other people is the key to ALF's success in the field of preservation.
(Photo by Ann-Marie Trapani)

MAKE A DONATION!

Help support the American Lighthouse Foundation by making a donation today!

Donate



WHAT WE DO...

ATTACHMENT L-4

**POTENTIALLY FREE WINDOWS
THROUGH JELD-WEN
LIGHTHOUSE RESTORATION
INITIATIVE**



PROJECTS

Lighthouse Restoration



Reliable Lighthouse Restoration Initiative

JELD-WEN's Reliable Lighthouse Restoration Initiative supports America's aging lighthouses by providing technologically superior windows and doors that preserve an historic aesthetic.

Four lighthouses have received JELD-WEN® windows and doors: Umpqua River Lighthouse, built in 1894 on Winchester Bay, Ore., received new JELD-WEN products as the pilot initiative in 2005. The iconic Thomas Point Shoal Lighthouse, built in 1875 on Chesapeake Bay near Annapolis, Md., received new windows and doors in 2007. Wind Point Lighthouse, built in 1880 on Lake Michigan near Racine, Wis., also received new windows and doors in 2007. New Canal Lighthouse, built in 1890 on Lake Pontchartrain in New Orleans, La., received new windows and doors in 2012.

JELD-WEN's proprietary AuraLast® Wood (/en-us/planning-projects/more-solutions/auralast-wood) was used in each lighthouse. Lighthouses are exposed to harsh weather day in and day out. This constant moisture can lead to wood rot. JELD-WEN® Windows with exclusive AuraLast® Wood offer to-the-core protection against wood rot. Because AuraLast withstands the rigors of coastal climates, it is a superior choice for lighthouse renovation.

New Canal Lighthouse



New Canal Lighthouse in New Orleans, built in 1890, was heavily damaged after Hurricanes Katrina and Rita. The Lake Pontchartrain Basin Foundation (<http://www.saveourlake.org/>) is reconstructing New Canal as an aid to navigation, museum and environmental education center.

JELD-WEN products used in this restoration were: Custom Wood windows, made with superior rot-resistant AuraLast® Wood to withstand the rigors of coastal climates, and Custom Wood Mahogany doors. Both windows and doors were designed to closely resemble the originals.



New Canal before Hurricane Katrina



After Katrina & Rita, 2005

Wind Point Lighthouse

The Wind Point Lighthouse is one of the tallest lighthouses on the Great Lakes. Breathtaking views of Lake Michigan were restored with new JELD-WEN® windows, and the 108-foot tower is more secure with a new JELD-WEN fiberglass door.



Collapsed in 2006

initiative)



Wind Point Lighthouse (after)

Click here to read
the related case
study.
([http://jeld-](http://jeld-wen.com/professional/tools-education/case-studies/256-wind-point-lighthouse-restoration-initiative)

[wen.com/professional/tools-education/case-studies/256-wind-point-lighthouse-restoration-](http://jeld-wen.com/professional/tools-education/case-studies/256-wind-point-lighthouse-restoration-initiative)

Thomas Point Shoal Lighthouse



Thomas Point Shoal Lighthouse (after)

This lighthouse is a National Historic Landmark on Chesapeake Bay. It opened its new JELD-WEN® doors (and windows) to tourists in 2007. It was built in 1875.

Click here to read the related case study. (<http://jeld-wen.com/professional/tools-education/case-studies/253-thomas-point-shoal-lighthouse-restoration-initiative>)

Umpqua River Lighthouse



Umpqua River Lighthouse (after)

Our 2004 pilot project, the Umpqua River Lighthouse, overlooks the Pacific Ocean. Oregon's oldest beacon now looks like new while staying true to its roots.

Click here to read the related case study. (<http://jeld-wen.com/professional/tools-education/case-studies/255-umpqua-river-lighthouse-restoration-initiative>)

JELD-WEN RELIABLE LIGHTHOUSE RESTORATION INITIATIVE

Frequently-Asked Questions and Answers

1. What is the JELD-WEN Reliable Lighthouse Restoration Initiative?

Over the past several decades, the U.S. Coast Guard has decommissioned or automated its aids to navigation and turned over maintenance of the towers and buildings to state, local, nonprofit entities and private individuals. As with any historic building, lighthouses are costly to maintain, and many fell into disrepair due to scarce funding and years of neglect. The JELD-WEN Reliable Lighthouse Restoration Initiative is a public-private partnership between the leading window and door manufacturer and the organizations who now maintain them. The initiative helps preserve lighthouses with the latest in window and door technology in styles that are architecturally compatible. In 2005, as a test project, JELD-WEN replaced the windows and the entry door at the historic Umpqua River Lighthouse on Oregon's coastline in cooperation with the Douglas County Parks Department. In 2006 two lighthouses were selected for product donations in 2007: Thomas Point Shoal Lighthouse built in 1875 on Chesapeake Bay near Annapolis, Md., and Wind Point Lighthouse built in 1880 on Lake Michigan near Racine, Wis. JELD-WEN is the only building products company with such a unique program to preserve treasured lighthouses nationwide.

2. Why is JELD-WEN involved in a Reliable Lighthouse Restoration Initiative?

Lighthouses are beloved landmarks across coastal America and a tangible way to demonstrate the reliability of JELD-WEN's windows and doors, which are designed to withstand even the harshest wind, moisture, salt air and UV exposure. JELD-WEN windows and doors have over 200 coastal certifications and the initiative has natural synergies. These lighthouse restorations underscore the significant challenges of renovating or maintaining any historic or coastal home. The largest exterior openings on any building, windows and doors are crucial to structural integrity and protect it from moisture, warping and cracking that can damage any building over time.

3. Why doesn't JELD-WEN simply select the winner based on the number of votes?

Each lighthouse has completely different and unique circumstances. The public has spoken, and now we must carefully consider all important factors, including the needs, overall restoration plans and historic preservation requirements of each of our top three lighthouse finalists. Working with local, state and federal government units, nonprofit organizations, architects and historic preservation experts involved in all three lighthouses requires thoughtful, careful consideration to thoroughly evaluate each potential project.

4. Which lighthouse generated the most votes?

Public support was amazing this year and we received a total of nearly a half million votes for our 12 lighthouse finalists. While we do not release the exact number of votes received by each lighthouse, we can say that the top three – Bodie Island, Grand Traverse and New Canal – together received nearly 70 percent of all votes cast. The remaining 30 percent was divided between the other nine finalists: Baltimore Harbor Lighthouse in Maryland; Cedar Island Lighthouse in New York; Grays Harbor Light Station in Washington; New Dungeness Light Station in Washington; Pemaquid Point Lighthouse in Maine; Plum Island Station in Wisconsin; Point Arena Light Station in California; Rose Island Lighthouse in Rhode Island; Toledo Harbor Lighthouse in Ohio.

5. How much do the public votes count in regard to the ultimate selection this year?

It was very important – our top three finalists were chosen exclusively by the number of votes they received, and collectively totaled nearly 70 percent of all votes cast. Public opinion is invaluable. Now, we must consider all factors to determine the ultimate winner, which we will announce at www.jeld-wen.com/lighthouse. These criteria are just as key to the process as the public votes that were generated. We're thrilled by the response our finalists received this year and encourage those passionate about helping save the nation's treasured lighthouses to continue to volunteer, raise funds and advocate for them. More information is available through many local lighthouse organizations, as well as the U.S. Lighthouse Society and American Lighthouse Foundation.

6. Are you going to select two lighthouses again this year?

Our plan is to select one winning lighthouse and complete the project within calendar year 2009. Updates will be posted on www.jeld-wen.com/lighthouse.

7. Will JELD-WEN continue to help lighthouses and will there be another opportunity to nominate others?

The JELD-WEN Reliable Lighthouse Restoration Initiative searches for the next lighthouse after each project is completed. This year's winner will follow in the footsteps of three impressive lighthouses that JELD-WEN has helped restore: Umpqua River Lighthouse in Oregon, Thomas Point Shoal Lighthouse in Maryland and Wind Point Lighthouse in Wisconsin. Lighthouses throughout the nation need help and resources, and we're very proud to be able to help these incredible architectural jewels, one by one.

8. How much are these donated products worth?

Custom windows and doors made for a lighthouse are significant gifts. However, JELD-WEN did not undertake the Reliable Lighthouse Restoration Initiative to receive credit for the dollar value of its donation. Their real value is beyond measure:

- They protect historic lighthouses from further deterioration and restore their original beauty.
- They inspire other companies and individuals to support the restoration of lighthouses as national treasures.
- They showcase the reliability of JELD-WEN windows and doors in harsh coastal climates and historic buildings.

9. Is JELD-WEN donating installation, too?

JELD-WEN installation services experts meet with the winning lighthouse's professional contractor to plan the installation and share best practices to pave the way for peak product performance. In some cases, our experts have advised onsite during installation. The winning lighthouse is responsible for professionally installing the new windows and doors as part of its overall restoration plan.

10. Are the windows and doors historically accurate?

We work with each lighthouse and its state historic preservation office to design historically appropriate, custom windows and doors. JELD-WEN windows and doors are not exact reproductions of the original fixtures. They complement the original historic designs while using the latest technology to preserve and protect these buildings for many years to come.

11. How will new windows and doors benefit these lighthouses?

JELD-WEN windows and doors have a major impact on the architectural and structural integrity of these lighthouses. Windows and doors are the largest openings of any building, and present the greatest risk for moisture, wind and insects to cause damage. They are also key visual elements in restoring a lighthouse to its original appearance, or other period.

12. How do these restoration projects benefit the communities?

The impact of the JELD-WEN Reliable Lighthouse Restoration Initiative goes far beyond helping protect the historic and physical integrity of these lighthouses themselves. More than important aids to navigation and maritime safety, lighthouses are also symbols of their communities' identities and beacons of hope for their local economies. Nationally, lighthouses attract millions of tourists and help capture and preserve the unique history of their areas.

13. Who is JELD-WEN? What's the history of the company?

JELD-WEN, inc., is the world's leading manufacturer of reliable windows and doors. Based in Klamath Falls, Ore., JELD-WEN began as a small Oregon millwork plant in 1960 and has grown into a company with nearly 150 divisions and more than 20,000 employees worldwide. Eleven JELD-WEN® product lines and all windows and doors constructed with AuraLast® wood have earned the Good Housekeeping Seal. In 2005, JELD-WEN® Custom Fiberglass exterior doors earned a Consumers Digest Best Buy rating.

14. Who do news media contact?

CMD Public Relations Group for JELD-WEN, 503-274-7006 or jeld-wen@cmdagency.com.

ATTACHMENT M

CONTRACTOR PROPOSALS

ATTACHMENT M-1

ABCORE RESTORATION

Phone (401) 952-5857
Fax (401) 782-2578

abcore@cox.net
www.abcorerestoration.com

ABCORE RESTORATION
COMPANY, INC.

2 SECLUDED DRIVE
NARRAGANSETT, RI
02882

Estimate

Date	Estimate #
12/10/2017	2

Phase One Only ✓✓✓

Name / Address		
Project		
Butler Flats Lighthouse New Bedford, MA		
Description	Qty	Total
Butler Flats Lighthouse New Bedford, MA -- Phase One EXTERIOR RESTORATION Planning & Permitting Temporary landing apparatus (Hoisting davits, floating dock, stairs) Mobilization: All required water transportation for materials, equipment, demolition and personnel Staging: Complete perimeter All necessary Enclosures necessary for environmental protection Selective Demolition: Remove, transport & properly dispose Ladder/Stairway balcony to mean low tide Structural Caisson Work: Repair and Replacement of the following: Caisson balcony deck plates. reproductions to match original Caisson deck nosing Caisson cracks: Install 1/4" mending plates Caisson structural bands stainless steel 1/2" by 6" full perimeter (3) total Structural beam attachment repair Caisson water line hatch cover Spot repairs metal Concrete Repair & apply water catalyzed neoprene membrane at caisson deck Painting: High performance coatings Exterior Includes all sand blasting preparation, priming, intermediate & finish coats. All work practices according to local, State and O.S.H.A. regulations Total		326,000.00
Page 1		Total 326,000.00

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abcore@cox.net
www.abcorerestoration.com

ABCORE RESTORATION
COMPANY, INC.

2 SECLUDED DRIVE
NARRAGANSETT, RI
02882

Estimate

Date	Estimate #
12/10/2017	1

PROPOSAL FOR
ENTIRE, BROADER
SCOPE
PRESERVATION
PROJECT

\$1,174,096. —

Name / Address
Butler Flats Lighthouse New Bedford, MA

Project

Description	Qty	Total
Butler Flats Lighthouse New Bedford, MA EXTERIOR RESTORATION Planning & Permitting Temporary landing apparatus (Hoisting davits, floating dock, stairs) Mobilization: All required water transportation for materials, equipment, demolition and personnel Staging: Complete perimeter All necessary Enclosures necessary for environmental protection Selective Demolition: Remove, transport & properly dispose Ladder/Stairway balcony to mean low tide Structural Caisson Work: Repair and Replacement of the following: Caisson balcony deck plates. reproductions to match original Columns: Radius gutter matching Balcony roof Caisson deck nosing Balcony rails complete perimeter custom radius Caisson cracks: Install 1/4" mending plates Caisson structural bands stainless steel 1/2" by 6" full perimeter (3) total Structural beam attachment repair Caisson water line hatch cover Tower panel cracks, mending plates Upper railings removal, repair & reinstallation Spot repairs metal Column, post and rail repair and replacement Caisson roof: Repair and epoxy coat Concrete Repair & apply water catalyzed neoprene membrane at caisson deck Windows: Double- hung, thermo pane, hurricane rated. Includes rough opening framing installation, interior & exterior trim Porthole windows: bronze framed laminate Door repairs Lantern room glazing, replacement of broken glass. Remove, restore & re-bed stops Lantern room roof: Repair and epoxy coat Painting: High performance coatings		

Phone (401) 952-5857 Fax (401) 782-2578
abcore@cox.net www.abcorerestoration.com

ABCORE RESTORATION COMPANY, INC.
2 SECLUDED DRIVE NARRAGANSETT, RI 02882

Estimate

Date	Estimate #
12/10/2017	1

Name / Address
Butler Flats Lighthouse New Bedford, MA

Project

Description	Qty	Total
Exterior Includes all sand blasting preparation, priming, intermediate & finish coats. All work practices according to local, State and O.S.H.A. regulations		
Total		1,174,096.00



ABCORE RESTORATION COMPANY, INC.

401 - 952 - 5857

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Abcore is one of the only contractors nationwide that restores lighthouses. Lighthouse restoration has many logistical challenges that make normal tasks close to impossible at times. Abcore's thorough project planning process and coastal experience enable us to always remain productive. Along with the right construction machinery and an offsite shop we are equipped with the right resources to always remain working efficiently. Abcore received the Preserve Rhode Island Rhody Award for restoring Beavertail Lighthouse, Keith Lescarbeau Abcore President, won the Artisan of the year award, as well as another Rhody Award for the Rose Island Lighthouse project of the year. We have the experience, skills, and resources to continually deliver successful award winning projects .

Plum Beach Lighthouse



Rose Island Lighthouse



Beavertail Lighthouse





ABCORE RESTORATION COMPANY, INC.

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**Rhody
Award
Winner**

Whether your interested in a historic restoration or renovation to any part of your home, commercial building, or structure there is no job too big or too small for us. Our project management process is thorough at handling all aspects of your project from the initial free estimate until the project completion. We have a full time staff and great relationships with architects and designers, enabling us to meet any of your project needs. Abcore has heavy construction machinery and equipment, along with an offsite workshop so we are equipped with the right resources to always remain working efficiently.



Historic Restoration



Residential



Commercial



Lighthouses



ABCORE RESTORATION SPECIALIZES IN HISTORIC RESTORATION AND RENOVATION IN RHODE ISLAND

We restore or renovate any interior or exterior part of your home. We work on the following items:

Foundations / Slabs / Site work / Framing / Walls / Siding / Soffits / Fascia's / Trim Work / Gutters / Dormers / Gable Ends / Flooring / Decking / Ramps / Railings / Stairways / Doors / Windows / Roofing / Chimneys / Bathrooms / Kitchens / Architectural Mouldings / Millwork / Finish Work / Electrical / Plumbing / HVAC / Mechanical / Metal Work



ABOUT

Abcore was created 37 years ago by owner Keith Lescarbeau in 1978. Keith started the company due to his passion for carpentry and working on historic buildings. Keith started off working with SWAP (Stop Wasting Abandoned Property) for the city of Providence under the Cianci administration. Keith created specifications and estimates to fix old buildings for the state in order to keep them from being demolished.

While working for SWAP, Keith attended Johnson & Wales University to obtain his real estate degree, and worked under master carpenter Amadeo Tella. The culmination of these interests inspired Keith to create Abcore Restoration. Ever since then Keith has been dedicated to restoring and renovating historic homes, landmarks, and buildings. In 2010 Keith was the recipient of the Preserve RI Artisan of the year award, while also winning project of the year for the Beavertail lighthouse Restoration, honored by the RI Historical Preservation & Heritage Commission. In 2013 Abcore was recognized for its ongoing work in Narragansett and received the Rhody Stewardship award along with the town of Narragansett. In 2014 Abcore won another Preserve RI Rhody award for the project of the year for restoring Rose Island Lighthouse. Abcore is currently a Rhode Island award winning contractor.

Keith Lescarbeau
President/Owner Abcore Restoration
Company, Inc.
Chair Narragansett Historic District
Commission
Captain Landing Craft "Lightkeeper"
Johnson & Wales University



Cheryl Lescarbeau
Business Manager
Bryant University



Cheryl has helped build the infrastructure of the company by managing the office, doing company accounting, cost control, and managing all construction documents. Cheryl works with you to make the construction document process easier with her 35 years of experience. Cheryl is up to date with all of the latest financial software.

Travis Lescarbeau
Project Manager
Roger Williams University - Construction Management
United States Green Building Council - Green Associate
Sigma Lambda Chi International Construction Honor Society Member



Travis has worked with Abcore since he was a teenager picking up carpentry skills at a young age from his father, Keith Lescarbeau. Travis pursued



Plum Beach Lighthouse

A Renewed Beacon of Hope

Narragansett Bay, North Kingstown, Rhode Island



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2003 Construction



The lighthouse as it sat in May 2003, prior to construction. Note its deteriorated condition, paint all but missing from the structure.

< Click photo for larger image

According to Keith Lescarbeau, the contractor hired by the state Department of Transportation to handle the renovations, the rusting wasn't a bad thing. Inherent in iron construction is oxidation that, while its appearance looks bad, is actually protecting the structure from further deterioration.



Federal Grant

An aerial shot in June prior to reconstruction. The Friends were awarded a \$500,000 Transportation Equity Act for the 21st Century (TEA-21) federal grant in conjunction with the Rhode Island State Department of Transportation. The grant was administered by RIDOT in the form of construction credits.

Newport Collaborative was the architect for the project, and the contractor hired was Abcore Restoration of Narragansett, among whose other notable projects include the restoration of the Towers in Narragansett.

Clean Harbors

In the 62 years since it was last inhabited, the lighthouse was the home to generations of pigeons, cormorants and seagulls. An estimate of an amazing 52 tons of guano was abated by Clean Harbors, a hazardous waste removal company hired to do the job. Much to the dismay of local farmers, the guano was required by contract to be disposed.

Discolored Wall

In the basement level, the guano depth was over 4 feet in some places. While it may look like the wall is painted, the discoloration is actually from the effects of the guano.



pictured above - discolored wall and wooden flooring in disrepair

Flooring

The upper levels of the structure show severe damage from the guano. The wooden floors and ceilings are soaked in bird droppings, and the metal walls have been corroded from the acidic property of the guano. The original contract called for the removal of the guano and the painting of the interior, but because of extensive interior damage discovered after the guano abatement, the contract was rewritten to exclude any additional interior reconstruction. The contract credit slated for the interior was used instead to rebuild exterior decking and columns which were initially to be removed but not replaced. It is estimated that the amount needed to rebuild the interior may equal or exceed the \$500,000 used to repaint the exterior.



pictured above - cabinets, conical shape, and a rotted wall

Lantern Room & Cabinets

The lantern room was devoid of its glass, allowing the birds free reign of the lighthouse. Glass in all of the windows and portholes in the structure was missing, the effects of 62 years of storms damage and vandalism. Ironically, there was very little vandalism at the lighthouse since it is such a difficult place to get to. Wooden cabinets are still in place inside the structure.

Conical Shape

The lighthouse is an interesting architectural structure in its conical shape. While the shell is made of iron, the interior is made mostly of brick, steel, and some wood. Here the staircase is shown on the second level.

Rotted Wall

The guano levels on the upper floors reached as high as the window sills, and this metal wall decayed because of the guano.



Collaborative People

Keith Lescarbeau met regularly with representatives of Newport Collaborative, the architects for the project.

Shrouded Light

Throughout the summer of 2003, Abcore Restoration removed the iron scaling from the lighthouse, then sandblasted the entire structure to prepare it for painting. A coat of marine epoxy primer was then applied directly to the iron.



pictured above - shrouded light, missing paint, painting the light

Missing Paint

The first phase of the repainting was to contain the rust and old paint being removed from the lighthouse. Because there was so little paint remaining on the structure, the lead abatement procedure wasn't a factor, making the job easier for the contractor.

Painting Light

In September, Bass River Painting of Middletown applied the final coat of marine epoxy in the original lighthouse color scheme.



Replacing Glass

Keith Lescarbeau and Kenny Reid of Abcore Restoration place glass in the lantern room in October. It was imperative to seal the lighthouse from the birds, so the lantern room was finished with tempered double pane glass. The windows were sealed with original brass molding that had been refinished by Lescarbeau. Each porthole, window, and door in the lower levels was sealed with marine painted stainless steel, making access impossible for birds and humans.



Grinding Columns

Metal railings on two of the levels and the columns and roof line of the main deck were demolished then replaced. Here Rob Winter of Coastal Iron Works welds part of the railing on the upper deck. The original cast iron columns of the main deck were replicated by Beckman Boatbuilders of Exeter who cast the new columns in gel coated fiberglass, making them impervious to the salt water elements.

1918 Crack

The base of the lighthouse had been badly damaged by the severe winter of 1918 when Narragansett Bay was encased in ice. After being repaired in 1922, the cracks reformed during the 1938 hurricane and were never repaired, allowing birds more access to the structure. During the renovations, Abcore Restoration placed two 6 inch stainless steel bands around the base to prevent further storm damage to the structure.



Solar Beacon

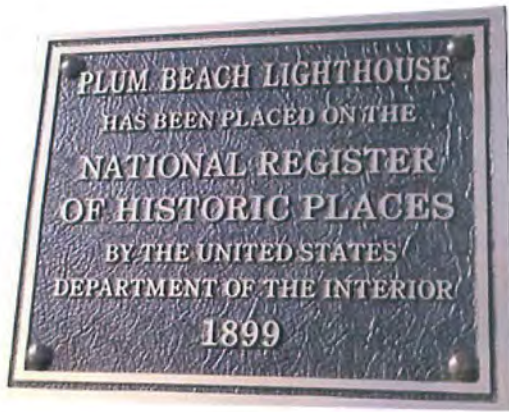
The Coast Guard Aid to Navigation approved a flashing light for the lighthouse in October, and in December a solar powered beacon was placed in the lantern room, lighting the structure for the first time in 62 years. The official navigational charts for Narragansett Bay will now show that Plum Beach Lighthouse is a private aid to navigation, #19310. Its flashing configuration is the same as it was in 1899--it flashes every 5 seconds with a one second duration followed by 4 seconds of darkness. The beacon was purchased from Waterways Navigation of Guilford, NH, and is the type of beacon generally used for buoys, but was chosen because of its maintenance free qualities and is designed to run for 5 years. It can work up to six days without sunshine.

After Dark

Once the lantern room received its new light, a trip at night over the Jamestown Bridge revealed that the flashing beacon alone wasn't enough to highlight the lighthouse. It was decided to add accent lighting to the structure, so three solar powered fluorescent lights were purchased from Solar Direct of Stuart, Florida. They were added to the lighthouse during Christmas week, giving the lighthouse a new look from dusk to six hours into the evening. A generous donation by Jeff and Kathy Camans of Jamestown made the accent lighting possible.



pictured above - Lighthouse After Dark and Winter 2004



Ice & Light

The winter of 2004 was one of the coldest on record in the Northeast and the lighthouse was captured amidst sea smoke on January 16. Air temperature was -4, wind was out of the north at 25 mph.

Historic Plaque

In 1988, the RI Historic Commission placed all of the State's lighthouses on the National Registry of Historic Places, and a commemorative plaque was placed on the structure in December.

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**ABCORE RESTORATION CO., INC.
2 SECLUDED DRIVE
NARRAGANSETT, RI 02882**

Phone: (401) 421-5656 Fax: (401) 782-2578

Email: abc core@cox.net Website: www.abcorerestoration.com

**HISTORICALLY REGISTERED PROPERTY RESTORATIONS COMPLETED
REFERENCES**

**PLUM BEACH LIGHTHOUSE
NARRAGANSETT BAY, RI
HISTORICAL PRESERVATION & HERITAGE
COMMISSION 2004 RESTORATION AWARD**

**RI DOT/BARBARA PETRACA
222-2023 Ext. 4090
NEWPORT COLLABORATIVE
RICHARD VENTRONE: 846-9583**

**DUTCH ISLAND LIGHTHOUSE
NO. KINGSTOWN, RI**

**DUTCH ISLAND LIGHTHOUSE
AL POTTER, SVP GILBANE CO.
456-5426**

**BEAVERTAIL LIGHTHOUSE
JAMESTOWN, RI
PRESERVE RI & RI HISTORICAL &
PRESERVATION COMMISSION:
RHODY AWARD 2010**

**GEORGE WARNER
864-3101**

**ROSE ISLAND FORT HAMILTON
BARRACKS REROOF
NEWPORT, RI**

**RICHARD VENTRONE
846-9583**

**ROSE ISLAND LIGHTHOUSE
EXTERIOR
NEWPORT, RI**

**O. J. Silas
222-2450
State RI: D.O.T.**

**NEWPORT HARBOR LIGHT
MAHOGANY FENCE**

**AMERICAN LIGHTHOUSE
FOUNDATION: BOB TRAPANI**

**POMHAM ROCKS LIGHTHOUSE
EAST PROVIDENCE, RI**

**FRIENDS POMHAM ROCKS
NATE CHACE**

**FORT ADAMS
NEWPORT, RI**

**ARNOLD ROBINSON
846-9583**

**STONE LEA ROOF
40 NEWTON AVE.
NARRAGANSETT**

**GUY LANCELLOTTI
741-1619**

**JOHN A. TOWNSEND HOUSE: 1839
127 POWER ST., PROVIDENCE, RI**

**ANTOINETTE DOWNING
PRESIDENT R.I. PRESERVATION
SOCIETY (FORMER)**

**SHAKESPEARE HEAD: 1772
PROVIDENCE PRESERVATION SOCIETY
21 MEETING STREET**

**CLARKE SCHOETTLE
DIRECTOR
272-2720**

**THOMAS PIERCE HOUSE: 1867
PROV. PRESERVATION SOCIETY
2000 AWARD**

202 BROADWAY, PROV., RI

**NARRAGANSETT CONSTRUCTION &
RESTORATION CONTRACTOR**

**JEFF CEASRINE
789-1044**

**NARRAGANSETT TOWERS RESTORATION
35 OCEAN ROAD
NARRAGANSETT, RI**

**KATE VIVIAN
TOWERS COORDINATOR
295-5270**

**NARRAGANSETT TOWERS ROOF
35 OCEAN ROAD
NARRAGANSETT, RI**

**JEFF CEASRINE
TOWN ENGINEER 789-1044
NARRAGANSETT, RI**

**TOWN OF NARRAGANSETT
FIRE STATIONS
NARRAGANSETT, RI 02882**

**CHIEF SCOTT PARTINGTON
789-1044**

**TOWN OF NARRAGANSETT
NORTH BEACH PAVILION
77 BOSTON NECK ROAD
NARRAGANSETT, RI**

**TOWN OF NARRAGANSETT
JOSEPH AMATORE
ENGINEERING DEPT
782-0636**

**TOWN OF NARRAGANSETT
CANONCHET NORTH BEACH CLUB
79 BOSTON NECK ROAD
NARRAGANSETT, RI**

**JOSEPH AMATORE
ENGINEERING DEPT
782-0636
NARRAGANSETT RI**

**TOWN OF NARRAGANSETT
SOUTH BEACH PAVILION
39 BOSTON NECK ROAD
NARRAGANSETT, RI**

**JEFF CEASRINE
TOWN ENGINEER
789-1044
NARRAGANSETT, RI**

**SUNSET FARM: KINNEY BUNGALOW
POINT JUDITH ROAD
NARRAGANSETT, RI**

**TOWN OF NARRAGANSETT
JEFF CEASRINE
789-1044 TOWN ENGINEER**

**STATE OF RHODE ISLAND
SCARBOROUGH BEACH BATH HOUSES
970 OCEAN ROAD
NARRAGANSETT, RI 02882**

**TYLER WHETSTONE
SENIOR CIVIL ENGINEER
222-2776 EXT. 7321**

**TOWN OF SOUTH KINGSTOWN
EMS SOUTH STATION
509 COMMODORE PERRY HIGHWAY
WAKEFIELD, RI**

**JON SCHOCK
PUBLIC SERVICES DEPT.
789-9331 EXT. 2250**

**WAKEFIELD BAPTIST CHURCH
BELL TOWER, MAIN STREET
WAKEFIELD, RI**

FATHER CLAY

**ST. FRANCIS OF ASSISI CHURCH
FATHER GREENAN HALL ROOF
WAKEFIELD, RI**

**FATHER JOE UPTON
783-4411**

**WESTERLY ARMORY
WESTERLY, RI**

ROBERTA MUDGE HUMBLE

PROVIDENCE ART CLUB

7 THOMAS ST., PROV., RI

WHITMAN FARM: 1749

93 EATON ST., PROV., RI

FLUER DE LI BUILDING

THOMAS ST., PROV. RI

SERIL DODGE BUILDING

9 THOMAS ST., PROV., RI

**OLD BRICK SCHOOL HOUSE: 1739
PROVIDENCE PRESERVATION SOCIETY
REVOLVING FUND
24 MEETING ST., PROVIDENCE, RI**

**WAYNE TRESSLER
272-2720**

ELIZABETH & JOHN WALSH HOUSE: 1921

4 UNIVERSITY AVE., PROV., RI

HENRY B. METCALF HOUSE: 1855	12 KEENE ST., PROV., RI
ALFRED HIGGINS HOUSE: 1868	47 PITMAN ST., PROV., RI
EBERNEZAR KNIGHT DEXTER HOUSE: 1799	300 ANGELL ST., PROV., RI
WILLIAMS ANDREW HOUSE: 1843	144 POWER ST., PROV., RI
GEORGE C. LYON HOUSE: 1898	93 ARLINGTON ST., PROV., RI
CHARLES S. & MATTY M. PETTEE HOUSE 1895	83 ARLINGTON ST., PROV., RI
THOMAS GUSHEE HOUSE: 1894	190 BOWEN ST., PROV., RI
THE GREAT HOUSE: 1810 NARRAGANSETT FIRE STATION BOSTON NECK ROAD NARRAGANSETT, RI 02882	76 GREAT RD. N.SMITHFIELD, RI CHIEF SCOTT PARTINGTON 789-1040
NARRAGANSETT CHAMBER OF COMMERCE 35 OCEAN ROAD NARRAGANSETT, RI	DEB KELSO 783-7121
119 BENEFIT STREET	PROVIDENCE, RI
CONSTITUTION HILL	NO. MAIN ST., PROV., RI
E. M. STRAUSS MANSION	PARADISE AVE., MIDDLETOWN, RI
SOUTH COUNTY MUSEUM CANONCHET FARM, NARR. RI	DARRYL ANDERSON 783-5400
NANCOOK FARM	OLD BOSTON NECK RD NARRAGANSETT, RI
AGAWAM HUNT COUNTRY CLUB	E. PROVIDENCE, RI
UNIVERSITY OF RI	BURNSIDE/ELLERY HALLS KINGSTON, RI

ATTACHMENT M-2

ENVIROVANTAGE



December 9, 2017

Rob Draper
Rincon Etal Investments, Inc/Mass Light, Ltd
PO Box 31000
Tuscan, AZ 85751

Re: Butler Flats Lighthouse Restoration, New Bedford Harbor, MA

Dear Rob,

Thank you for the opportunity to bid on the Butler Flats Lighthouse Restoration, New Bedford Harbor, MA project for your firm. Please review the following quote and contact me with any questions you may have. In order to safeguard our partners and clients under the new OSHA Silica Standard (OSHA 29 CFR 126.1153), EnviroVantage has implemented required compliance processes in the performance of our assigned scope of work to manage liability and risk to all project teams and the public.

Our proposed scope is to supply the required labor, materials, equipment and disposal required to do the following scope:

- Inspect/survey underwater caisson plates
- Pump out sea water from annular space
- In stall stainless steel restraining straps at all bands of caisson plates
- Grout annular space
- Needle gun all exposed caisson prior to painting to remove all loose and deteriorated material (rust)
- Apply two coats of marine grade paint
- At main concrete deck we will remove and replace deck hatches
- At main concrete deck remove and replace deteriorated concrete

Cost of labor, materials, equipment and disposal: \$292,400.00

Description of work to be performed: Prep Caisson Surface:

- Needle gun area to remove loose rust
- Chemically clean surface prior to paint application

Description of work to be performed: Demo:

- GC/Owner to provide NESHAP survey prior to demo
- Supply properly trained supervisor and workers
- Supply proper fall protection while working
- Supply proper PPE for EnviroVantage employees
- Adhering to OSHA 29 CFR 1926.1153
- Additional 'Silica Exposure Zone' containment may be required
- 'Silica Exclusion Zone' begins where silica action level (25ug/m3) is met
- All other trades cannot be within 'Silica Exposure Zone' during silica exposure work
- Dumpster to be placed on site
- Remove and dispose of failing concrete at main deck
- Properly dispose of materials generated



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SURFACE PREPARATION & CLEANING • ENVIRONMENTAL CLEAN-UP

629 Calef Highway (Rt. 125), Epping, NH 03042 Toll Free: 800-640-5323 (p) 603-679-9682 (f) 603-679-9685

www.EnviroVantage.com



- No removal of any structural components
- All MEP's to be cut, capped and made safe by appropriate trades

Quotation Notes:

- Owner/GC responsible for any additional city and town permits
- Owner/GC to provide temporary protection at the end of each day if required
- Owner/GC to supply access to work area, water, and electricity throughout duration of project (20 AMPS per 1,000 SF is minimum required)
- Owner/GC to salvage items prior to start of demo
- All items will need to be removed from work area by owner prior to start of work
- Excludes all hazardous materials unless otherwise specified
- EnviroVantage carries standard \$6 million pollution insurance
- No performance and payment bond included
- Not based on Davis Bacon rates
- Excludes any/all disconnection of utilities
- Lock out tag out by Owner/GC
- Excludes any/all winter conditions

Quotation Exclusions:

- This quote is based on doing our scope of work in one mobilization
- If additional mobilizations are required there will be an additional charge of \$3,800.00 per mobilization
- This quote is based on straight time
- This quote is based on project schedule represented at bid time
- Any project schedule changes made after the date of this proposal will require a review of our estimated cost

General Information

EnviroVantage is an award winning Specialty and licensed Environmental Contractor with over 30 years of experience and success with projects of this nature, including many of New England's most recognizable landmark projects. We've achieved that level of accomplishment through our continued desire to go above and beyond the expectations of our clients with high quality workmanship, teamwork, communication, dedication to safety and environmental responsibility. We are committed to doing what it takes to get the project done right the first time. For further information on the benefits of working with us, I encourage you to visit our website at www.envirovantage.com.

I appreciate your interest in doing business with EnviroVantage and thank you for your consideration. We look forward to working with you and being a valuable part of your project team.

Feel free to contact me with any questions.

Sincerely,

Scott

Scott Knightly



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www.EnviroVantage.com



President



Asbestos-Demolition-Lead-Mold-PCBs

24/7 Emergency Services

Office: 603-679-9682

Cell: 603-231-7048

Toll-Free: 1-800-640-5323

www.EnviroVantage.com

scottk@EnviroVantage.com

2009 & 2014 Business of the Year

Celebrating 30 Years in Business!

Terms and Conditions:

The above price(s), specifications and conditions are satisfactory and are hereby accepted.

EnviroVantage is authorized to perform the work as of _____, 2017

Payment Terms: Net 30

Overdue payments will bear interest at two (2) percent per month. Costs of collecting overdue invoices, including reasonable attorney's fees will be added to the invoice for collection.

Authorized Signature: _____

*Price quoted is only valid for 30 days after date of proposal. Signature required prior to start of project.

QID:: 17807



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www.EnviroVantage.com

Butler Flats Light

Statement of Qualifications



A Leading New England Environmental Specialty Contractor

Scott Knightly
scottk@envirovantage.com
603-231-7048
800-640-5323

November 22, 2017

KNOWLEDGE. SAFETY. PREPAREDNESS. FOLLOWTHRU.



EnviroVantage is New England's most trusted Environmental & Specialty Services contractor with over 33 years of experience. We specialize in the removal of asbestos, lead, mold & PCB's, decontamination & infection control, environmental clean-up, structure drying, duct cleaning, disaster recovery, select demolition and complete structure removal. **EnviroVantage services ALL of New England, completing OVER 4.5M sq. /ft. in environmental services within our target markets: Commercial, Industrial, Government, Historic and Residential sectors. Combining Abatement and Demolition Services, EnviroVantage offers clients the security of utilizing one contractor with expertise across the environmental vertical.**

EnviroVantage has proudly participated in many of New England's most recognizable landmark projects including multiple lighthouse restoration projects. We have a lighthouse in our log for that very reason. Our level of accomplishment is achieved through a continued desire to go above and beyond the expectations of clients with exceptional workmanship, professionalism and the dedication to Safety & Environmental Responsibility.

EnviroVantage's commitment to service includes keeping up with the latest technology (ex. on screen take offs, e-sign documents and daily digital report generation to clients) this reduces delays and creates a more responsive work flow. EnviroVantage industrial equipment upgrades: the BROKK robotic demolition machine, 2 electric driven Sherpa's from the Netherlands and a dual head floor machines that eliminate the need for liquid mastic removers; Each other state-of-the-art equipment addition allows us to provide a level of service and expertise to exceed our clients' expectations.

Our reputation is backed by numerous Industry & Community awards:

- 2017 ABC Excellence in Construction Award- ABC Massachusetts
- 2017 Excellence in Construction Award- ABC NH/VT
- 2016 Business Excellence Award
- 2016 Excellence in Construction Award- ABC NH/VT
- 2015 Business of the Year
- 2015 Safety and creativity from ABC NH/VT
- 2014 Excellence in construction from ABC NH/VT

EnviroVantage will offer many advantages to The Butler Flats Project Team that included:

- **24/7 Direct Access-** Our phones ring direct to a 24 hour person. The phone will ring through a chain so there will always be a live person answering.
- **Full Disaster Preparedness-** All information, phones and desktops are cloud based with 100% access even if our office/shop is affected by the same disaster.
- **Disaster Response Experience** – Our experience includes working on Hurricane Sandy in NY and CT, Hurricanes Katrina & Rita in LA & TX and flooding events in NH & VT.
- **33 years of Experience** – EnviroVantage has performed work at Pemaquid Point Lighthouse, Annisquam Lighthouse, West Quoddy Head Light, Historic North Church Boston, Wood Island Lifesaving Station, Harvard, Northeastern, Tufts, UVM, University of New Hampshire, Plymouth State, Keene, EMMC, Concord Hospital, Eliot Hospital, VAMC (Togas, ME; Manchester, NH; New Bedford, MA; Providence, RI; White River Junction, VT), multiple military bases and government buildings.

Additional Information:

- 33 years of Experience
- Fully Staffed Accounting Dept., Estimating Dept. and an Environmental Compliance Officer.
- Full time Safety Officer
- Employees average 15 years with the company
- Bonded single limit \$10 million and \$6 million in Environmental Pollution insurance
- NADCA Certified Duct Cleaning Professionals
- All Supervisors/workers are cross trained in Lead, Asbestos, Mold, PCB and Select Demo

KNOWLEDGE. SAFETY. PREPAREDNESS. FOLLOWTHRU.

**EnviroVantage®**
ASBESTOS ABATEMENT. REMEDIATION. DEMOLITION. .com**WOOD ISLAND**
LIFE SAVING STATION ASSOCIATION
POST OFFICE BOX 11 • KITTERY POINT, ME 03905

March 16, 2017

Sam Reid
President

Dear Sirs:

Jake Winebaum
Vice President

Working in cooperation with the Town of Kittery Maine, the owner of the property, our charity recently enjoyed the services of EnviroVantage for the Wood Island Life Saving Station reclamation and renovation project.

Laurence Bussey
*Treasurer*Steve Wood
Secretary

This building is eligible for listing on the National Register of Historic Places. Beginning in 1908 it was a place where brave men would row out in rescue craft to help save mariners in distress. It is our mission to fully restore the station and open it for the enjoyment of the public as a maritime museum.

William Howells
*Director*Dave Kaselauskas
Director

Sadly, the station had been left with minimal maintenance for decades and was in need of significant hazardous materials clean up and structural repair. This project was complex with multiple priorities overlapping between logistics, demolition and what needed to be preserved.

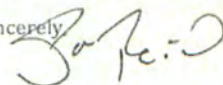
Beth Segers
Director

Given the unusual and uncommon work location of public park on a small island with no pier, this project necessitated an innovative and forward thinking company to come up with a pre-demolition plan that would include complicated logistics of securing and transporting all work related equipment and personnel from the mainland to the island.

This project frequently needed creative solutions as each layer peeled back revealed additional remediation challenges. These abundant "surprises" were met with patience and expertise by the EnviroVantage team as they removed and properly disposed of Wood Island Station's hazardous materials on time and on budget.

EnviroVantage's flexibility was certainly critical to our success as we worked to save this rare building for future generations to enjoy. We have greatly enjoyed the services of EnviroVantage and gladly recommend them.

Sincerely,

Samuel S. Reid
President

Featured Relevant Project: Wood Island Life Saving Station (WILSS) – Historic Restoration Project

In Historical Restoration, it is critical to know and understand the project's history. The Wood Island story dates back to 1827 when the State of Maine ceded to the United States Government a small island to build a barracks. In 1889, the island was used as a hospital for the Spanish American War prisoners who were quarantined with Yellow Fever. In 1908, the US Lifesaving Service (which later became part of the US Coast Guard) erected a life-saving station. Groups of volunteers in small row boats would monitor the Piscataqua River and save mariners from shipwrecks. In early WWII, the US Navy took over the building to help protect the submarine manufacturing at Portsmouth Naval Shipyard from German U-Boats patrolling the Atlantic coast. By 1955, the island and its building were declared surplus and deeded to the town of Kittery Maine. The Wood Island Life Saving Station was left abandoned for decades, falling into disrepair.

In 2009, the town of Kittery's plan to demolish the structure motivated a small group of locals to form The Wood Island Life Saving Station Association (**WILSSA**) with the declared intention of restoration and reuse rather than the demolition of this piece of history.

Using money donated locally while they searched for federal funds and grants, WILSSA undertook the challenge, with approval from the town of Kittery, and embarked on a restoration plan.

Historic Renovation is the most exciting and most challenging work in the demolition field, and EnviroVantage now had the chance to be a part of one of the most exhilarating and strenuous Historical Renovation and Restoration projects in the history of the State of Maine. Prior to any abatement work, WILSSA prioritized that all fragile, intricate historic trim from the original 1908 main building, the boat house, chimney, sea wall and the distinctive and most treasured feature, the viewing tower had to be removed, cleaned and catalogued for condition and location in the build and wrapped and preserved for reinstallation. All this was done amongst complications created by 50+ years of neglect and deterioration. The EnviroVantage team performed abatement of asbestos, plaster and wood -inclusive of the millwork removal for restoration and buildings shoring.

During an assessment of the status of WILSS before work began, the EnviroVantage team encountered a badly damaged and highly neglected Duluth-style building (WILSS is a "Duluth Type" which is a specific architectural design first built in Duluth, MN in 1894. There were only 28 ever constructed, with only 12 remaining). The entire east wall was completely rotted, 90% of the 1st floor and, 100% of the second floor joists unstable, severely damaged rafters, structural beams decaying, the west façade severely water damaged, floor systems that were so rotted and dilapidated that temporary staging and planks were needed to be erected to access areas and roofs that were collapsing.

In evaluating the work to be performed, each area needed to be fully assessed using the criteria of, what could be saved, cleaned, photographed and catalogued for room designation and reinstallation; what could be saved and used as patterns for reproductions and what hazardous materials needed to be removed from this 100+ year old structure that was in total disrepair, battered by water damage, rough seas, lightning strikes, fire damage and inhibited by seagulls and other fine feathered friends that had used it as their own personal bath house during that span.

With oversight, guidance, and rules and regulations from the Maine Historical Preservation Society, EV's team was subject to work conditions that included transportation to work in boats, equipment arriving on skiffs and barges, army trucks used to move dumpsters and materials around the site, buildings with no electricity, no running water or telecommunication. WILSS was 90 -100% rotted, in various forms of serious damage/decay, BUT still had historical society oversight for preservation purposes and an aggressive, uncompromising time line- EnviroVantage was ready to begin and excel in it's part of the Historical Restoration of The Wood Island Life Saving Station.

KNOWLEDGE. SAFETY. PREPAREDNESS. FOLLOWTHRU.

Relevant Project Experience:

1. **Project:** Pemaquid Point Lighthouse
Address: 3115 Bristol Rd; Pemaquid, ME
Contact: Jean Bretz, US Coast Guard
Phone: 401-736-1765
Contract Dates: 05/08/00-9/19/00
Contract Amount: \$53,290
2. **Project:** Annisquam Lighthouse
Address: Lighthouse Rd; Gloucester, MA
Contact: Jean Bretz, US Coast Guard
Phone: 401-736-1765
Contract Dates: 4/6/01-5/8/01
Contract Amount: \$18,600
3. **Project:** West Quoddy Head Light
Address: S. Lubec Rd Off; Lubec, ME
Contact: David Campbell – Campbell Construction
Phone: 978-922-1945
Contract Dates: 09/27/04-10/27/04
Contract Amount: \$17,500
4. **Project:** Nobska Light
Address: Woods Hole, Massachusetts
Contact: Katherine Bumpus, Friends of Nobska Light
Phone: 978-922-1945
Contract Dates: 9/11/2017-11/27/2017
Contract Amount: \$235,000



References:

Scott Allocca, Environmental Health & Safety Manager
Fairpoint Communication
1 Davis Farm Rd; South Portland, ME
Sallocca@fairpoint.com
207-648-3405

Kevin Moreau, Safety Officer
BAE Systems; NCA- 14
65 Spit Brook Rd; Nashua, NH
Kevin.moreau@baesystems.com
603-765-1948



Qualifications of Key Personnel:



Scott Knightly – Founder and President

- 33 years experience in Lead, Asbestos, PCB's, Mold and Demolition
- Holds the first lead license issued in NH # C-001
- Member of the NH Asbestos Advisory Board
- Member of the NH Lead Advisory Board
- President of the National Demolition Associations
- Former President of ABC NH/VT
- Current speaker at UNH Paul College School of Business



Scott Sansoucie – Vice President of Operations

- 27 years experience in Lead, Asbestos, and Mold & Demolition
- Served in the USAF
- Responsible for project management & all field operations
- Handles Key Accounts -BAE, Phillips Exeter, Public Service Company of NH, Fairpoint
- Active Member of AGC, ABC



Mike Rodgers – Senior Estimator

- 50 years experience in all aspects of construction
- 3rd generation home builder
- 28 years commercial masonry
- 35 years commercial estimating
- Currently overseeing all estimates from conceptual to hard bid
- Past president of NH Home Builders Association



Nick Ryba – Controller

- Oversees the internal accounting functions & future growth of EnviroVantage
- 6 years of accounting experience
- 6 years of business and construction experience
- Member of CFMA (Construction Finance Management Association)
- Currently pursuing CCIFP Certification

Envirovantage's Sales, Budget Development & Estimating Team:

- 5 full time estimators(Utilize digital take off & spreadsheet estimating)
- 5 full time sales representatives on the road



Full time Compliance Officer

- 17 years experience
- Asbestos project Designer
- Asbestos/Lead Trained
- CQM Training
- OSHA 30 Trained
- NH Asbestos Advisory Board
- Certified Hazardous Waste Coordinator
- ADS Trained

5 Full time Project Managers:

Troy Purington – Haverhill, MA
17 years construction experience
Peter Carlberg - Dover, NH
31 years construction experience
David Massaro – Portland, ME
26 years construction experience

James Legros- Ossipee, NH
33 years construction experience
Chris Davidson – Hillsborough, NH
21 years construction experience

16 Full time In-House Supervisors:

Starlin Herrera – Lawrence, MA
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME
OSHA 30, Hazwopper 40, Mold

Elder Duarte – Methuen, MA
Asbestos Licenses – MA, NH, VT, CT, RI, ME
OSHA 30, Hazwopper 40, Mold

Omar Lazu – Dracut, MA
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME
OSHA 30, Hazwopper 40, Mold

Franklin Lopez – Lawrence, MA
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME
OSHA 30, Hazwopper 40, Mold

Victor Sanchez- Lawrence, MA
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME
OSHA 30, Hazwopper 40, Mold

Darrell Rabito – Methuen, MA
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME
OSHA 30, Hazwopper 40, Mold

Bruce Kelleher- Rochester, NH
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME
OSHA 30, Hazwopper 40, Mold

Craig Capra- Hudson, NH
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME
OSHA 30, Hazwopper 40, Mold

Ryan Morancy – Amherst, NH
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME
OSHA 30, Hazwopper 40, Mold

Joseph Fowler – Salem, NH
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME

Barry Cadorette – Chester, NH
Asbestos Licenses – MA, NH, VT, CT, RI, ME, Mold

Eric Daugherty – Manchester, NH
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME
OSHA 30, Hazwopper 40, Mold

John Moore – Dover, NH
Asbestos Licenses – MA, NH, ME

Brandon Merkley – Barrington, NH
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME
OSHA 30, Hazwopper 40, Mold

Dean Jordan – Lewiston, ME
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME
OSHA 30, Hazwopper 40, Mold

Owen Chamberlain – Portland, ME
Asbestos Licenses – MA, NH, VT, CT, RI, ME
Lead licensed - MA, NH, VT, CT, ME
OSHA 30, Hazwopper 40, Mold

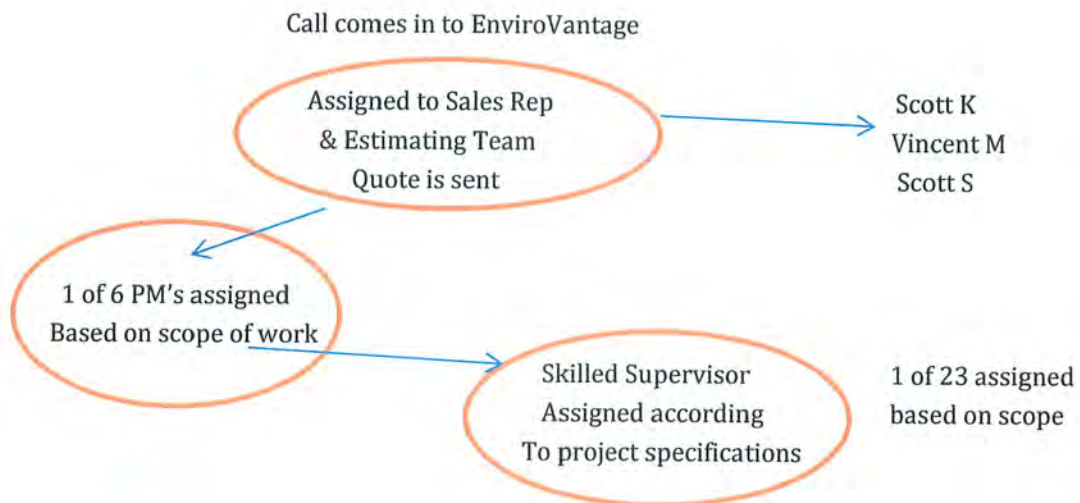


EnviroVantage will meet with your on-site staff and introduce our team to your organization. The lines of communication are established as The Butler Flats Light Team would like to see the flow happen. The Butler Flats Light Team's call will come into either our 800 number, serviced 24/7, or directly to your EnviroVantage Team Representative. EnviroVantage will meet on site at your convenience to go over the project and respond to your specific needs for that particular project. The project would then be estimated and a proposal would be submitted. Once the scope has been determined and the proper paperwork/ notification are active, the project will begin. Prior to project execution, an onsite walk through is done with our Project Manager and Project Supervisor to insure all items are in place to start the project and it is ready to go.

EnviroVantage generates reports each day that include: project status, crews on site, equipment on site, how far along the project is and any other aspect the Butler Flats Light Team would like to know about. These would be sent each day to keep you informed and up to date on progress. Picture documentation is included with all of the reports to clarify and document the work.

At the project's completion, a final walk through would be performed with the entire project team to insure all items have been completed to the Historic Society, USCG and Butler Flats Light Team's standards and satisfaction. A close out package will be generated to document the work listing items installed, cleaned or removed as well as all labor, equipment and materials that were used for the successful completion of the work.

Organization Chart

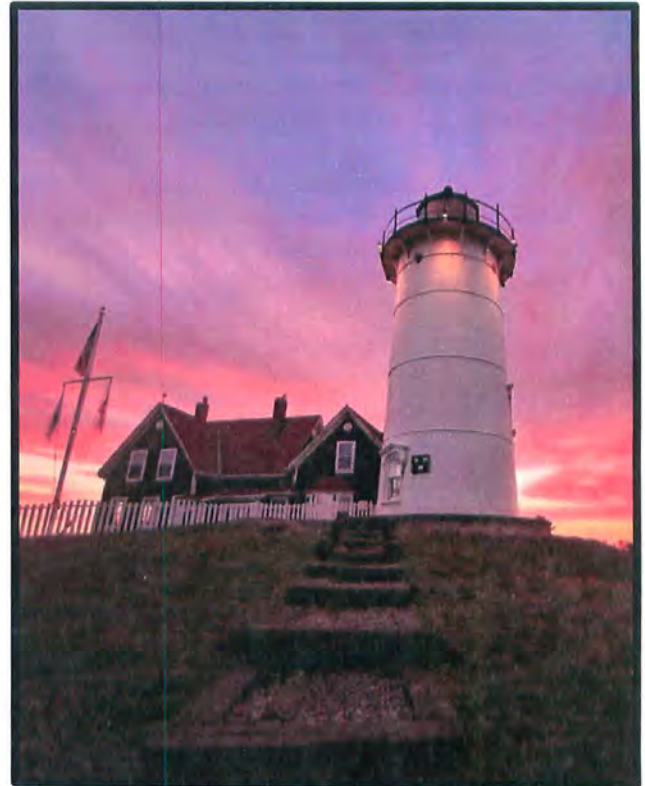


KNOWLEDGE. SAFETY. PREPAREDNESS. FOLLOWTHRU.



Training Provided for ALL Staff:

- Asbestos Abatement
- Lead Abatement
- Mold Abatement
- RRP
- Hazwopper Training
- OSHA 30
- Confined Space
- Proper Rigging
- Signaling
- Scaffolding
- BBP
- First Aid-CPR
- Haz-Com
- GHS
- Respirator Protection
- Hazardous conditions & Atmospheres
- Welding- Cutting Safety
- Make Safe Awareness
- Safe Equipment
- Lift Operations
- Accident-Incident Investigations
- Traffic Control
- Electrical Hazards and Awareness



Currently Company Licenses:

- MA Lead License #: DC001478
- MA Lead-Safe Renovation License #: LW000107
- MA Asbestos License #: AC000510
- NH Lead License #: C-001
- NH Asbestos License #: AS-098
- NH ADS License #: SC-039
- ME Lead License #: LC-0020
- ME Asbestos License #: CF-0069
- VT Lead License #: CT397130
- VT Asbestos License #: AE397130
- CT Lead License #: 002246
- CT Asbestos License #: 000538
- RI Asbestos License #: LAC-241-000
- CMR: Cert. # IAQA 04-10-1071



KNOWLEDGE. SAFETY. PREPAREDNESS. FOLLOWTHRU.



Current Term Contracts:

EnviroVantage currently services several term contracts throughout New England. EnviroVantage is on-call 24/7. A sampling of EnviroVantage current term contracts:

- State of New Hampshire – Asbestos & Mold contracts
- BAE Systems
- Fairpoint Communications
- Eversource
- Unitil
- Harvard
- University of New Hampshire
- Tufts University
- UMASS
- Plymouth State University
- United State Postal Service
- AECOM
- Delphi

Thank you for the opportunity to bid this work. We look forward to working with you and you staff in the near future. Please feel free to reach out to me with any question that you may have.

Sincerely,

Scott Knightly

Scott Knightly
President

KNOWLEDGE. SAFETY. PREPAREDNESS. FOLLOWTHRU.

ATTACHMENT M-3

BAY DIVING AND MARINE SERVICES

Bay Diving & Marine Services
208 Blackmore Pond Rd. W. Wareham, Ma. 02576

774-263-5002, baydiving@yahoo.com

December, 8, 2017

Robert Draper
Rincon Etal Investments Inc./Mass Light, Ltd.
P.O.Box 31000 Tucson, AZ. 85751

Dear Robert Draper,

Here are the numbers for initial repairs of structure to stabilize and protect the lower exterior.

Item	Time	Men	Total
Staging set up	5 Days	4	\$10,825
Sandblast and disposal	5 Days	4	\$33,120
Paint	10 Days	5	\$43,330
u/w pressure wash	2 Days	4	\$5,125
Apply u/w epoxy	2 Days	4	\$5,775
Apply epoxy above lwl	5 Days	4	\$13,025
Pump epoxy grout	2 Days	5	\$11,000
Jackhammer upper deck	4 Days	4	\$16,135
Dowel & install rebar	2 Days	4	\$11,355
Pump concrete deck	1 Day	6	\$9,425
Barges & Transportation	38 Days		\$19,590
Replenish base stone	2 Days	3	\$10,125
Total	38 Days		\$188,830

Sincerely; Bill & T.J.



Sincerely,

ay Diving & Marine Services



ATTACHMENT M-4

**CHESTERFIELD ASSOCIATES
MARINE CONTRACTORS AND ENGINEERS, INC.**

robert.draper@comcast.net

From: Chesterfield Maine <boathouse@zwi.net>
Sent: Wednesday, December 6, 2017 8:18 AM
To: robert.draper@comcast.net
Cc: 'Lucinda Tilas'; 'Scott Knightly'; 'Mike Rodgers'; 'Seth Allan'; 'Alex Allan'
Attachments: Butler Flats LH initial estimate.doc

Good morning Rob-

Thanks for forwarding the drawings. The attached narrative and pricing includes a small portion of crew & equipment downtime due to weather.

I personally have worked on (6) of these structures, from Maine to Delaware Bay. Once the new landing is in place, and the source of electricity is confirmed, the interior work should be able to proceed without much concern for weather; however, wind conditions will dictate the safety of landing and departing.

We look forward to working with you.

Yours truly,

Dave Allan

- Clean/descalc caisson : \$44,000.-
(not including sandblasting)
- 2 coats of epoxy paint : \$32,000.-
on caisson
- mobilization/demobilization : \$9,600.-
- stainless steel bands : \$32,000.-
- grout annular spaces : \$40,000.-
- install injection ports : \$9,600.-
- remove/replace concrete deck : \$22,000.-

\$189,000.-

Mr. Rob Draper
Mass Light, Ltd.
P.O. Box 31000
Tucson, AZ 85751

12/6/17

Re: Butler Flats Lighthouse, New Bedford

Dear Mr. Draper-

later phase -
potential dock/stairs

Having received the conceptual drawings for the new landing on this structure, our estimate to provide all components and install same is \$82,500. The implementation of this work will facilitate all the remaining tasks you require, and should be your initial expenditure.

To that end, removing scale from all portions below the main deck we estimate to cost \$44,000. This does not include sandblasting. To apply two coats of epoxy paint we estimate to be around \$32,000.

From the main deck up, cleaning and painting the exterior we estimate to cost \$47,000.

Each mobilization/demobilization will be priced at \$9600.00

later phase

We and EnviroVantage would be pleased to quote the interior work required once we have performed a site inspection.

Yours truly,

Davies Allan, pres.

robert.draper@comcast.net

From: D. Allan~Maine <boathouse@gwi.net>
Sent: Thursday, December 7, 2017 9:00 AM
To: Robert Draper
Cc: Lucinda
Subject: Butler Flats

Rob, I was able to find time to research our records:

To install (4) stainless bands, each 3/8" x 8" , with at least two take-up locations per band
\$8500. Each

Pump grout into annular spaces: (Quantity <i>estimate</i> : 40 cu yds)	\$1000/cu yd
Installation of injection ports: (Quantity <i>estimate</i> : 8)	\$1200 each
Remove concrete deck and replace: (Quantity <i>estimate</i> : 10 cu yds)	\$2200/cu yd

Hope this helps.

Regards
Dave



MARINE CONSTRUCTION



SITE DEVELOPMENT



SPECIALTY SERVICES

WELCOME TO CHESTERFIELD ASSOCIATES INC.

For more than 40 years, Chesterfield Associates has provided professional marine construction and specialty foundation services.

From Virginia to Maine, Chesterfield Associates have been leaders in the marine industry delivering projects on time and on budget. Whether providing docks or bulkheads to a residential customer or completing major bridge installations, piling, or concrete restoration, professional quality service is what you will receive.

The company has received many awards for the quality of its workmanship and takes great pride in our work. Chesterfield is well staffed with professionals and has the requisite resources to handle the vast majority of the projects presented to it.

Marine construction is the main thrust of Chesterfield Associates Inc. The Company's reputation in the industry with clients and engineers is tops in the field.

Our expertise in marine construction includes planning and construction for residential and commercial docks, piers, all types of bulkhead structures and the rehabilitation of existing structures.

Demolition and excavating is an essential step of any large-scale construction project.

Site development requires highly specialized techniques and proper pre-construction groundwork. The combination of hazardous materials and dangerous working conditions are always present.



MARINE CONSTRUCTION



SITE DEVELOPMENT



SPECIALTY SERVICES

COMPANY PROFILE

Chesterfield Associates is one of the largest non-union contractors on Long Island.

The company is privately owned and is lead by 4 management officers with over a century of combined experience. Among the staff are employees that hold NY Welding Certificates, Boating Captain Licenses, and Crane Operating Licenses. Safety is our primary concern and all personnel hold OSHA training and Hazardous Material Operations certificates.

The Westhampton Beach, New York based headquarters maintains a fleet of heavy equipment and construction vehicles on over 16 acres of industrial space. Chesterfield Associates also has a branch office in Westport Island, Maine.

Chesterfield is a member in good standing with Associates Builders & Contractors, Inc. Clients include the U.S. Coast Guard, the National Park Service, New York State, Nassau & Suffolk County counties and many other mid-Atlantic states, counties and municipalities a large base of private clients.



Chesterfield Associates has received many awards for quality workmanship and takes great pride in delivering projects on time and within budget.

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Chesterfield Associates supports a variety of community services.

They have sponsored charitable events and have provided parade vehicles and floats for community and school events. They have donated banner installation services and permit assistance to the local Chamber of Commerce and the local Historical Society. "Chesterfield Boulder" is a Chesterfield Associates sponsored fundraising program. The winner of the charity auction can choose a large landscape boulder and have it installed in the yard. Chesterfield Associates donates the boulder, the transportation and installation of it.

Preserving History



Chesterfield Associates is the company of choice for any type of historical preservation and restoration projects. Bridges, lighthouses and other landmarks have been brought back to operational and safety standards while retaining their original and historical look.

First Responders



Chesterfield Associates was one of the first responders to the crash site of flight 800.

The company quickly deployed marine landing crafts with staff to assist authorities in the rescue and recovery efforts.

ATTACHMENT N

FUNDING COMMITMENT

Funding Commitment

To date, Rincon Etal Investments, Inc., has funded the Butler Flats historic preservation project, in an amount totaling \$108,936.22 to date, either directly or through contributions to Mass Light, Ltd., the non-profit corporation formed to carry out the project.

The project sponsors are confident that the requested grant from the Community Preservation Fund will be sufficient to enable them to complete this first phase of the project and are committed to providing or securing any additional funds needed to cover unanticipated expenses associated with this first phase of the project.

The project sponsors are also confident that, with the many other sources of funding for such projects, as outlined in the application -- including grant funding from maritime heritage and lighthouse foundations, public funding, tax credit funding, internal funding, and other sources of funding -- they will be able to raise all amounts necessary to complete the comprehensive historic preservation project.

ATTACHMENT O

COMMUNITY NEED

In a recent feature on WCVB Boston's Chronicle series, called "New Bedford Renewal: A New Vitality," the Butler Flats lighthouse was shown six (6) times in the first 90 seconds of the show. The feature emphasized the economic resurgence of New Bedford and its visual transformation and "new bones." Unfortunately, Butler Flats looked like a rusty can; but its appearance in so many of the shots speaks to its iconic, attractive nature and its central place in the history and symbolism of the City and the region. As Mayor Mitchell said on the show, "the condition of the built environment means a lot." Wouldn't it be nice if the next such video feature showed a shining, structurally sound Butler Flats lighthouse, welcoming mariners to New Bedford Harbor and tourists and residents home to the great city of New Bedford?

Here's a link to the WCVB Chronicle show:

<http://www.wcvb.com/article/new-bedford-renewal-a-new-vitality/12194099>

ATTACHMENT P

LIST OF PROJECT PARTICIPANTS

List of Project Participants

Rincon Etal Investments, Inc., Tucson, Arizona, Robert Draper, President – high bidder in the GSA auction of Butler Flats and funder of the Butler Flats historic preservation project to date.

Mass Light, Ltd., Massachusetts non-profit corporation, Robert Draper and David Draper, Directors – organized to pursue the Butler Flats lighthouse historic preservation project.

Becker Structural Engineers, Paul Becker, P.E., President – Becker Structural Engineers prepared a condition assessment report for Butler Flats and are available to advise on the project. They have extensive experience in lighthouse inspection, engineering, and restoration, including with respect to the Spring Point Ledge Light in Portland, Maine, which is nearly identical to Butler Flats in construction and appearance and presents many of the same preservation challenges.

Boucher & Associates, Inc., Civil Engineering, Westport, MA, Alan Heureux, Mark Boucher, Tim Gauvin, Craig Marshall, and Associates – Boucher & Associates managed the Chapter 91 Waterways licensing process and obtained the license from MassDEP.

Law Offices of Perry Hicks, LLC, New Bedford, MA, Daniel Perry, Esq. – Perry Hicks advises the project on matters of licensing, entity formation, regulatory compliance, and historic preservation.

Abcore Restoration Company, Inc., Keith Lescarbeau, R.I. – Abcore Restoration is one of the preeminent lighthouse restoration companies in the nation, headed by Keith Lescarbeau who is knowledgeable and experienced not only with lighthouse restoration but a wide variety of historic preservation projects and the underlying regulatory framework. Their work on Plum Beach lighthouse is a great illustration of what this

project will involve and what Butler Flats can become. See Attachment M-1. They have proposed to work on the project.

EnviroVantage, Epping, NH, Scott Kingsley, N.H. – EnviroVantage has extensive experience with lighthouse restoration, including most recently with the Nobska Point Light. The firm also has deep experience with all manner of historic preservation projects, including the Old North Church. A statement of their qualifications is included behind their proposal, at Attachment M-2.

Bay Diving and Marine Services, New Bedford, MA, Bill Avila and TJ Johnson (Fortress Marine Services) – Bay Diving has experience with the restoration of the Palmer’s Island Lighthouse and bring expertise in diving, underwater welding, and metal and concrete repair, especially in a marine environment. They have submitted a proposal for this project. See Attachment M-3.

Chesterfield Associates, Inc., NY and ME, Davies Allan, President – Chesterfield Associates have extensive experience with lighthouse preservation in the Northeast. See Attachment M-4.

New Bedford Harbor Development Commission, Ed Anthes-Washburn, Port Director, Brian Joseph, Harbormaster – It is hoped that the HDC will be actively involved in the rehabilitation of Butler Flats and its operation and maintenance as a private aid to navigation, functions it performed admirably for the City of New Bedford for 36 years, from 1978 through 2014.

Waterfront Historic Area League, Teri Bernert, Executive Director – The project sponsors hope to engage the services of WHALE to help navigate the project and ensure compliance with all historic preservation and other regulatory requirements.

ATTACHMENT Q

MATRIX OF CONFORMITY TO CPP PRIORITIES

BUTLER FLATS LIGHTHOUSE PRESERVATION PROJECT -- MATRIX OF CONFORMANCE WITH CPA FUNDING CRITERIA

Primary Criteria

Consistency with the current CPP	Yes, local lighthouses are listed as key resources in the CPP
Capacity to serve multiple needs and populations	Yes, mariners, tourism, open space users, all residents
Addresses long-standing or urgent needs in the community	Yes, stabilization and repair are urgent; reliance on Butler Flats is longstanding
Could serve as catalysts for transformative change	Yes, see Attachment O discussion
Demonstrates practicality and feasibility within reasonable, feasible schedule & budget	Yes, project is doable and similar to other successful lighthouse projects

Secondary Criteria

Acquisition of threatened resources or urgency to seize time-sensitive opportunities	Yes, saved from GSA auction and need to repair is urgent
Leverages other public and private funding	Yes, 100% of expenses to date, and about 25% of overall cost, privately funded
Addresses more than one CPA focus area	Yes, historic preservation and potentially open space and recreation (see Attachment R)
Capacity to serve under-served population	
Provides evidence of community support	Yes, see Attachment E
Improves accessibility for all members of the community	Potentially, through future projects; see Attachment R
Incorporates sustainable practices and design	Yes, the light is solar-powered and the structure uses little other energy
Applicant/team has successfully implemented projects of similar type and scale	Yes, two of the three bidding contractors have deep experience and expertise with lighthouse restoration and rehabilitation

Historic Preservation Criteria

Project is located at a pivotal or visually prominent site	Yes, for sure; see Attachments E and O
Project has the potential to benefit the community at large	Yes, see Attachments E and O, R, S and T
Resource is of notable and distinctive architectural or cultural significance	Yes, listed on National Register; a major historical asset to the region

ATTACHMENT R

**“ALTERNATIVE PROGRAMMING” EXAMPLE --
VIRTUAL Lighthouse EXPERIENCE**

Into the Light

The virtual lighthouse experience created at the Maine Maritime Museum in June 2017 is an excellent example of the ways in which an offshore lighthouse like Butler Flats could be brought closer to the onshore community. See attached pages and www.mainemaritimemuseum.org for more details.

The project sponsors are also exploring the possibility of creating a lighthouse viewing station and/or park of some sort on the pier that extends out into the harbor from East Rodney French Boulevard near Butler Flats. At least two, and possibly all three, of the City's historic lighthouses can be seen from this location, and the City has preliminary plans for a different sort of open space on that pier – a fisherman's memorial – that could likely be modified to suit this purpose.

These kinds of projects would make Butler Flats more accessible to residents and tourists, both visually and through the use of informational resources, and would extend the benefits of the historic preservation project into other areas of emphasis under the Community Preservation Act, such as open space and recreation.

- Current
- Upcoming
- Past
- Permanent

Into the Lantern: A Lighthouse Experience

Permanent



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The Apprenticeship at 40



The SS Portland and the Great Gale of November 1898



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Into the Lantern: A Lighthouse Experience

Permanent



Into the Lantern: A Lighthouse Experience, a full-scale replication of the Cape Elizabeth Two Lights lighthouse tower lantern room, allows visitors to see the original second-order Fresnel lens from the east tower at Two Lights and experience the environment of the lantern room through time-lapsed video projections featuring a panorama of the Gulf of Maine that changes with the weather and seasons. This immersive exhibit is the first of its kind anywhere.



"The Into the Lantern exhibit at the Maine Maritime Museum is a beautifully designed, immersive experience that does justice to the 'jewel of the lighthouse,' the Fresnel lens. These beautiful works of functional art were one of the pinnacles of 19th century

*technology, and it's wonderful to see one displayed in such a thoughtful way. **I have visited many lens exhibits in many museums, and this is the best I've seen.***" -
Jeremy D'Entremont, author and historian for the American Lighthouse Foundation.



*"Simply put, if you have imagined what it might be like to experience serene and stormy scenes from atop a lighthouse – all the while basking in the warm glow of a breathtaking Fresnel lens, then the Maine Maritime Museum's Into the Lantern exhibit is for YOU! The recreated majesty of Cape Elizabeth's lantern is literally stunning. This one-of-a-kind experience will astound the visitor with its exquisite beauty, detail and ability to transport admirers back in time when lighthouse keepers kept a vigilant eye seaward as they helped protect the mariner from the perils of the sea. If you desire one lighthouse experience that will prove to be both amazing and memorable, then **Into the Lantern is a must-see!**" –*
Bob Trapani, Executive Director, American Lighthouse Foundation

Thank you to the generous donors who supported this project:

Anonymous

Charles and Elizabeth Barker

Geoffrey and Lindsay Alexander

Anonymous

Stephen and Joanne Caulfield

Harry W. Konkel

National Park Service, Maritime Heritage Program

Susan and Jackson Parker

Portland Head Light Fund

William and Sally Gemmill

The Edgard and Geraldine Feder Foundation, Inc.

The Morton-Kelly Charitable Trust

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John, Elizabeth, Louis & Kelley Frumer

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Elena D. Vandervoort

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Kimberley and Christopher Byrd

Peter and Susan Dublin

Julie Sheehan and Caroline, Charles, and Charlotte Emerson

Bill and Karen Gallagher

Richard and Cate Gilbane

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Virgil and Nancy Hancock

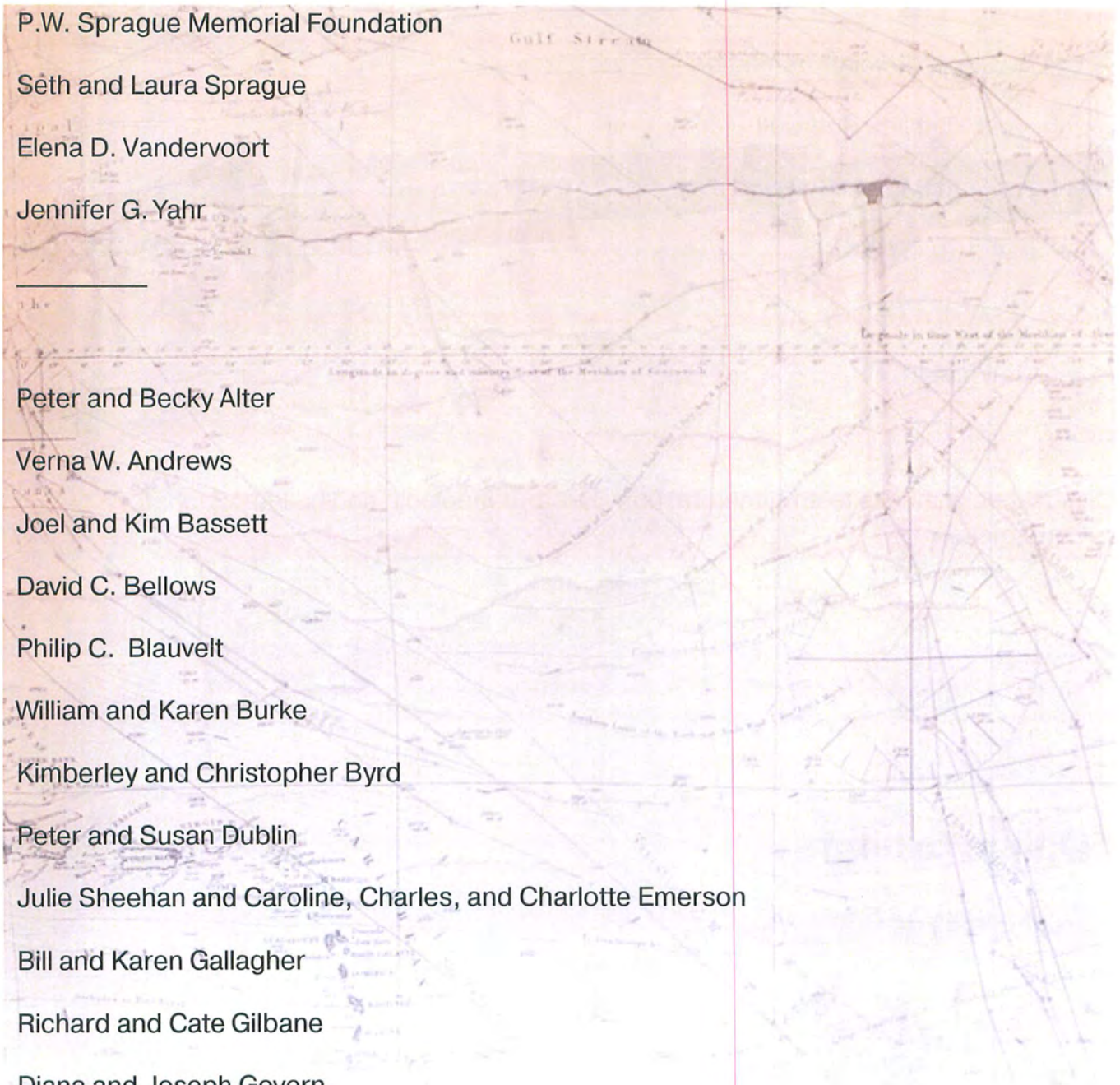
Anne K. Hewes

Nick and Joan Hurd

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Wendell and Suelle Large

Kent and Ernestine Lawrence



Marjorie N. Platou

The Redmond Family Foundation

John and Mathilde Rothwell

Gregory and Pamela Salerno

David and Trisha Sheaff

Jack and Sibyl Tingley

Jay M. Trudeau

With special thanks to architectural firm Barba + Wheelock; and builder, Zachau Construction

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Maine Maritime Museum

243 Washington Street
Bath, Maine 04530

ATTACHMENT S

INTENDED USE

Intended Use

Initially, the project sponsors considered a range wide of private and commercial uses for Butler Flats. It quickly became clear, however, that the most appropriate use for the lighthouse – the one that best ensures its relevance and therefore long-term importance to the local community -- is its continued use as an aid to navigation – that is, as a functioning lighthouse. That is the purpose which has made Butler Flats an important fixture in the maritime landscape of the region and which will best carry it forward.

Mass Light, Ltd., was formed as a non-profit corporation in recognition of the fact that the proper historic preservation of Butler Flats will require devotion of considerable time, attention, and funding, with some of the best sources of funding, such as this CPA funding opportunity, being available only to non-profit endeavors seeking to complete first-quality historic preservation projects for the benefit of the general public. That is the objective of this Butler Flats lighthouse preservation project.

Other uses consistent with this mission can and should be considered, such as educational and research-related uses, especially if they have the potential to generate revenue for the preservation project. Examples include the past use of Butler Flats as an ocean data-gathering station by the UMass Dartmouth School for Marine Science and Technology (SMAST). Other similar uses have been suggested, but only those consistent with the community-oriented preservation effort and the not-for-profit status of Mass Light, Ltd., will be considered.

ATTACHMENT T

RE-LIGHTING THE AMERICAN FLAG

Relighting the American Flag

It has been written that Butler Flats is the only lighthouse in the United States to have flown a lighted American flag twenty four hours a day. The project sponsors intend to restore this unique feature of Butler Flats early in the preservation project, as a signal that something good is happening at the lighthouse and as a first step toward its return to top condition. See attached article.



BUTLER FLATS LIGHT

This lighthouse replaced the old Clark's Point Light onshore, which had been established in 1797. The tower was designed by F. Hopkinson Smith, who was also an artist and writer. Smith also built the foundation of the Statue of Liberty.

Accessibility: ♿

Geographic coordinates:
41° 36' 12" N 70° 53' 42" W

Nearest city: New Bedford.
Located in New Bedford Channel, at the mouth of the Acushnet River.

Established: 1898.
Present lighthouse built: 1898. Automated: 1978.

Height of tower:
53 feet. Height of focal plane: 53 feet.

Previous optic: Fifth-order Fresnel lens. Present optic: LED.

Characteristic: White flash every 4 seconds.

[125]

[MASSACHUSETTS]



Volunteer Hugh Murray

The sparkplug-style light has four stories, including living space. Until 1942, when the Coast Guard took over from the civilian Lighthouse Service, the lighthouse had

only two keepers: Captain Amos Baker Jr. and his son, Charles A. Baker. Amos Baker Jr. had been in charge at Clark's Point Light for some years earlier, and his father was keeper there before him. The Bakers kept the two lights for about eighty years.

Some of the logs of Amos Baker are in the possession of the Old Dartmouth Historical Society. The entry for Christmas 1907 reads, "A pleasant Christmas Day . . . Squally in the evening, but we had some music from the phonograph, so we had sunshine inside."

Visitors' signatures in the register while Amos Baker was keeper included President Grover Cleveland.

Charles Baker retired in 1941, and the Coast Guard took over. In 1975, a new automatic light was placed on New Bedford's hurricane barrier. The Coast Guard deemed the lighthouse unnecessary, and it came under the control of the City of New Bedford in 1978. It was automated and became one of the first

solar-powered lighthouses in the nation. Local volunteers took responsibility for the maintenance of the light.

On April 30, 1998, more than 600 people attended a celebration of Butler Flats Light's 100th birthday. A new, brighter optic was installed by Hugh Murray, a retired New Bedford wire inspector who in recent years has headed up the preservation efforts.

The lighthouse can be seen distantly from the New Bedford waterfront along East Rodney French Boulevard. A good view can be obtained from the New Bedford-Martha's Vineyard ferry, see www.nefastferry.com or call 866-683-3779 for information. The ferry from New Bedford to Cuttyhunk Island also passes close by; see www.cuttyhunkferryco.com or call 508-992-0200.

Fascinating Fact

This is said to be the only lighthouse in the United States flying a lighted American flag twenty-four hours a day.

[126]

Courtesy of Jeremy D'Entremont © 2012

ATTACHMENT U

DETAILED EXPLANATION OF REQUESTED FUNDING AMOUNT

Detailed Explanation of Requested Funding Amount

In their December 2016 condition assessment report, Becker Structural Engineers provided an “order of magnitude” estimate of the total cost of restoration of Butler Flats lighthouse of \$1,550,500, including a 25% contingency and 15% engineering fee.

The same report estimates that the portion of the work relating to stabilizing and repairing the caisson and deck level of the lighthouse (the scope of the current project) will cost about \$250,000 plus mobilization/demob and crane barge. If the estimates for those mobilization/demob and crane barge costs are pro-rated to, say, 15 days or \$75,000, the total projected “order of magnitude” cost for this project is \$325,000 before engineering and contingency fees. And another \$80,000 to replace the ladders, or \$405,000 total. With a 25% contingency and 15% engineering fees, the estimates come to \$455,000 (no ladders) and \$567,000 (with ladders).

The project sponsors have procured four bids for the project – stabilizing and repairing the caisson and concrete deck and possibly replacing the two ladders.

Two of the proposals come from historic preservation contractors with extensive experience in New England lighthouse restoration. Abcore’s proposal is for \$326,000, which includes replacement of the ladders. EnviroVantage’s proposal is for \$292,400 and does not include ladder replacement (which was not specified as part of this phase of work). Both proposals address environmental and regulatory compliance. Both are well below the “order of magnitude” estimate in the condition assessment report but don’t include engineering fees or a contingency line item.

Abcore also presented a proposal to complete the entire preservation project for \$1,107,500, which seems in line with the Becker report “order of magnitude” numbers (before contingency and engineering fees).

The other two proposals are simpler and seem to address specific elements of the work but perhaps not some of the details or “overhead” items. Bay Diving proposes to do the key elements of the work for \$188,8300 and Chesterfield Associates (by piecing together several proposals) for \$189,000.

Our inclination is to gear our funding request based on the Abcore proposal, since their proposal seems detailed and comprehensive rather than an itemization of only key elements; their competence in this area is unquestionable; and their work on the Plum Beach Light project so successful and similar to what is required at Butler Flats. We recognize that all of the proposals and condition reports include an element of uncertainty, until the contractors actually get into the caisson and other areas of the lighthouse to know exactly what challenges are faced. We expect that the Abcore proposal might be sufficient to include replacement of the ladders, or might not, depending on whether the other elements of the work are fairly straightforward and/or less costly than anticipated in these proposals. We are inclined to subtract \$80,000 from the Abcore bid (on the assumption that the ladders will be beyond the scope of the project), reducing the proposal to \$246,000, to leave room in the CPA funding request for engineering and historic preservation consulting fees (15% or \$36,900) and a 17.5% contingency (\$43,050), for a total request of \$325,950. If engineering and consulting fees and/or contingencies and/or estimated costs prove to be lower than proposed, and/or elements of the work can be performed less expensively or with

assistance from other entities, then perhaps the ladders can be completed in this phase of the project.

We also intend to explore the possibility of having one of the larger, more experienced firms engage the services of one or more of the other bidders to gain the benefit of their particular expertise and potentially lower cost and, in the case of Bay Diving, the efficiencies associated with their unique capabilities and local presence.

The attached matrix contains the numbers discussed above.

ANALYSIS OF COST ESTIMATES AND PROPOSALS FOR FIRST PHASE OF BUTLER FLATS PRESERVATION PROJECT

	First Phase (CPA-Funded Project)			Broader Preservation Project	
Becker Report (12/16)		\$325,000	*	\$	1,107,500.00 no eng fees or contingency
		\$405,000	**		
	\$	455,000	***		
	\$	567,000	****	\$	1,550,500.00 15% eng fees; 25% contingency
Abcore	\$	326,000		\$	1,174,096.00
	\$	246,000	excluding ladders		
	\$	282,900	with 15% engineering		
	\$	325,950	excluding ladders; with 15% engineering fees and 17.5% contingency		
EnviroVantage	\$	292,400	no ladders		
Bay Diving	\$	188,830	no ladders		
Chesterfield Associates	\$	189,000	no ladders		

*including \$75k for mobilization; no engineering or contingency

**including \$75k for mobilization; no engineering or contingency; includes ladders

*** including \$75k for mobilization, 25% contingency, 15% engineering; no ladders

**** including \$75k for mobilization, 25% contingency, 15% engineering, includes ladders