



City of New Bedford

Department of Planning, Housing & Community Development

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PATRICK J. SULLIVAN

DIRECTOR

Revised STAFF REPORT

PLANNING BOARD MEETING

August 10, 2016

Case # 25-16: SPECIAL PERMIT & SITE PLAN MODIFICATION
Riverside Landing Multi-Use Development at the
Former Fairhaven Mills
Riverside Landing/Coggeshall Street
Map 93, Lots 261 & 292
(a/k/a Map 93-2, Lots 264 & 261)

Applicant/

Owner: D.W. New Bedford, LLC
1266 Furnace Brook Parkway, Ste. 310
Quincy, MA 02169

Applicant's

Agent: Prime Engineering
P.O. Box 1088
Lakeville, MA 02347

Overview of Request

Case 25-16: Riverside Landing Multi-Use Development at the Former Fairhaven Mills -Drive-thru Restaurant – This is a request by applicant for modification of Special Permit and Site Plan approval for **Case #15-13** for a medical ambulatory facility and drive thru bank in the Hicks-Logan-Sawyer District Interim Planning Overlay District (IPOD) and underlying Mixed Use Business zoning district. The Special Permit and Site Plan approval for Case #15-13 have lapsed. Applicant seeks to modify previously granted approval to permit a fast-food drive-thru restaurant.

All development proposals within the HLS IPOD require a Special Permit and Site Plan approval from the New Bedford Planning Board as described under the city's zoning ordinance, §4660.

Riverside Landing is strategically located along a secondary gateway area of New Bedford at Coggeshall Street and Exit 17 of Interstate Route 195. This project falls within the established interim planning overlay district known as the Hicks-Logan-Sawyer (HLS) IPOD and is located at the Riverside Landing site.



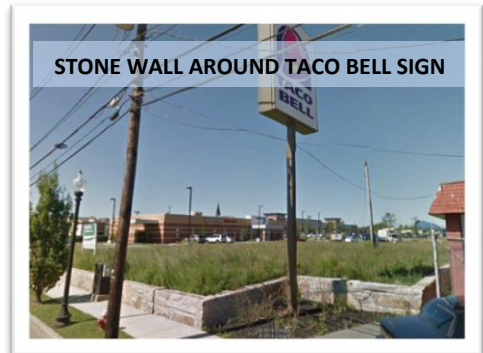
The project area is approximately 150 acres. Land associated with the I-195 interchange makes up about 15 acres dividing the project area into a larger 95-acre to the south of I-195, and a smaller 40-acre section to the north of I-195.

Background

Research finds the an application for Site Plan Review and Special Permit for a 10, 211 SF medical ambulatory (urgent care) facility and 2,400 SF future restaurant or bank, to be determined at a later date, had been submitted on May 15, 2013 which had been withdrawn without prejudice by the applicant on June 20, 2013.

According to the minutes of July 10, 2013 Planning Board meeting, the application was approved by a vote of four (4) to one (1) a subsequent Site Plan and Special Permit for Case 15-13 for Phase 3 of Riverside Landing on Coggeshall Street (Plot 93-2, Lot 261 and 264) with the following conditions:

- *That the stonewall follow the curb lines around the Taco Bell sign;*
- *That the sign be 85" by 54" as presented and be internally lit;*
- *That no dumpster is approved for the bank;*
- *That the S-curve bike rack be installed by the medicine building and it be blue;*
- *That the benches be installed as (sic) the remainder of the site and they be green as they are at Market Basket;*
- *That the silk sacs be placed along all the drives;*
- *That the bollards have plastic covers;*
- *That the handicap sign covers be installed on the site;*
- *That the Washingtonian light currently within the middle path of the access lane be moved easterly to be mirrored by the westerly Washingtonian light along the entrance;*
- *That the security light in the parking lot can be moved northerly as long as it does not interfere with the adjacent building;*
- *That granite walls be extended from the gateway area west to the southwest corner of Phase 1;*
- *That the board approves the curb cut and access into the lot as shown on the plan.*



Existing Conditions

Doctors Express Urgent Care facility has been constructed at the pad site and is in operation. The applicant has revised the proposal for a 1,722 +/- SF undetermined bank with drive-thru and now presents a plan for a drive-thru fast food restaurant at the pad area designated for the bank design layout of Phase Three for development of Riverside Landing. There is a curb cut facilitating ingress and egress from Coggeshall Street to the pad west of the main entrance to Riverside Landing.

The proposed construction site area is comprised of soil covered by grass, fronts Coggeshall Street and appears to have utility service, as evidenced by a utility pole. While a concrete sidewalk and stonewall enhancing the frontage along Coggeshall Street has been constructed, the granite wall at the Taco Bell sign is set behind the pole, rather than built to follow the curb line, as stipulated by the Planning Board on July 23, 2013. It also does not appear that benches have been placed as conditioned by the Board.

Applicant states in the narrative the storm water mitigation system which had previously been approved and constructed collects stormwater from a well-maintained, landscaped parking lot that is swept on a regular basis.

In addition to the Doctors Express Urgent Care facility, neighboring businesses include Taco Bell fast food restaurant (Phase 2), DeMoulas Market Basket supermarket (Phase 1), Little Caesars Pizza, Verizon Wireless,

Great Clips hair salon, McDonald's fast food restaurant, Subway fast food restaurant, GNC (General Nutrition Center), and a water kiosk for the purchase of potable water.

Proposed Conditions

Under the proposed revision, the applicant requests site plan modifications necessary to allow for the construction of a proposed 2, 265 +/- SF, one-story, forty-six (46) seat fast food restaurant known as Popeye's Louisiana Kitchen. Nine (9) parking spaces are dedicated for the restaurant's walk-in customers. A multiple window drive-thru order and pick up lane, with by-pass to facilitate traffic circulation, is part of this development proposal.



- **Parking.** Shared parking and shared driveways are encouraged as design policy, principles and goals set forth in the HLS District Master Plan and §4671 of municipal code. The proposed site plan illustrates nine (9) parking spaces at the southern boundary of the pad along Coggeshall Street having dimensions of 9' x 18'. Required dimensions are 9' x 20' under §3150. Drive aisle is shown as 22'. ***The applicant has submitted a petition for waiver under the dimensional requirements of §3150 for parking space dimension, with an understanding that by reducing the length of the parking space their action will allow the 22 foot aisle requirements to remain constant.***

Under Section 5.11-Shared Parking, the application notes "The criteria within the IPOD district is that the number of parking spaces normally required outside the IPOD be treated as a maximum number with the goal being the multiple uses share parking." The applicant notes 464 spaces are required for a project located outside the IPOD; 735 spaces are provided at Riverside Landing. However, as the adjacent waterfront lot (noted as Future Building Site C on Phase I Layout Plan dated September 14, 2009) is completed, 146 spaces will be absorbed by that future development, thereby reducing parking availability to 618 spaces. Staff notes the ongoing popularity of Market Basket makes parking competitive for customers of that supermarket under existing conditions.

- **Future Development.** The applicant describes the conceptual plan for the project's Phase 4-40,000 SF waterfront facility to be comprised of first floor retail with three upper floors of office mix. The applicant states that the City anticipates a fifth phase of development to include a rowing center at the foot of Sawyer Street that will share the waterfront facility parking lot.
- **Proposed Signage.** A rendering of the proposed Popeye's sign is shown on the Details-2 plan sheet. This sign copy will be added to the existing monument sign, which is located at the pad site ingress/egress. It appears from review of case submittal documents the building front elevation architectural design incorporates the proposed business' trademark signage.
- **Roofing materials.** are specified as a UNA-CLAD Metal Roof system with installation described on Front and Rear Elevations plan sheet K-2. The Planning Board, as standard policy, looks for applicants to use cool roof or reflective roofing material in construction; this standard is also a principle of the IPOD. The Planning Board may desire to invite further comment from the applicant as to the energy efficiency of the metal roofing material specified for construction.
- **Lighting.** Lighting Plan sheet 6 notes, "Lighting to match lighting installed in previous phases of Riverside Landing". As a standard of practice, the approved plan should note that no substitutions, additions or

changes may be made without approval by the Planning Board or City Planner. Two light poles are shown on the Lighting Plan sheet. It appears that the area in proximity to the water kiosk illuminated at 6.8 foot-candles is slightly above the limitations recommended under Dark Sky guidelines of 5 foot-candles. No landscape lighting is shown on Landscape Plan sheet 7. Wall lighting is illustrated on elevation plans.

- **Demand and Operations.** The applicant states twenty employees will be hired to serve 200 customers per day between the hours 6:00 a.m. – 12 midnight, seven days a week. Deliveries are scheduled between 6:00 a.m. and 6:00 p.m. The frequency of deliveries has not been disclosed by the applicant.
- **Development Timing.** Should the Planning Board approve the modification for Special Permit and Site Plan, construction completion is anticipated within six months of obtaining all necessary permits.

Site Plan Review

The submittal is named on the Title Sheet as Riverside Landing-Multi-Use Development at the Former Fairhaven Mills Site, Coggeshall Street, New Bedford, MA, Phase III Modification Site Plans Issued for Special Permit Modification dated June 13, 2016, as prepared by Prime Engineering, P.O. Box 1088, 350 Bedford Street, Lakeville, MA 02347, consisting of eleven (11) sheets;

and

Riverside Landing Popeye's, Coggeshall Street, New Bedford, MA dated 04.xx.2016, issued for Popeye's Louisiana Kitchen, 400 Perimeter Terrace, Suite 1000, Atlanta, GA 30346, as prepared by Michael Sudano, Architect P.C., 90 Senix Avenue, Center Moriches, NY 11934 consisting of three (3) sheets. *[NOTE: As this architect is from another jurisdiction, the Planning Board may desire to have on file confirmation that the architect has reciprocity with the Commonwealth of Massachusetts and can provide NCARB (National Council of Architectural Registration Boards) certification and current license status.]* Provided sheets with recommended modifications noted as follows:

Title Sheet (TS)	Sheet 1
Existing Conditions	Sheet 2
Layout Plan	Sheet 3
Grading/Drainage Plan	Sheet 4
Utilities Plan	Sheet 5
Lighting Plan	Sheet 6
Landscape Plan	Sheet 7
Erosion Control Plan	Sheet 8
Details – 1	Sheet 9
Details – 2	Sheet 10
Details – 3	Sheet 11

Floor Plan – K-1

- The architectural plans have not been stamped or signed. The Board will want to ensure plans are prepared and signed by a registered architect as stipulated under §5440.
- The floor plan provided by the architect is a mirror plan of the building footprint shown on the proposed site plan. The discrepancy should be clarified, corrected, and shown in revised plan sets and drawings.

Front and Rear Elevations – K-2

- The architectural plans have not been stamped or signed. Again; the Board will want to ensure plans are prepared and signed by a registered architect as stipulated under §5440.

Stormwater Management Report - On November 4, 2015 the City of New Bedford adopted new Stormwater Management regulations. The case submittal does not include revised drainage calculations. As this Site

Plan/Special Permit has lapsed, the Planning Board may request that an updated drainage report providing assurance that the proposed post-development meets the current standards for stormwater mitigation and that said development satisfies DPI's stormwater concerns in this regard. ***The applicant has submitted a waiver for new stormwater report.***

Review Comments

Plans for modification were distributed to City Clerk, City Solicitor, Health Department, Inspectional Services, Engineering, Public Infrastructure, Conservation Commission, Fire Department and School Department offices.

- The Conservation Commission finds the project is not located in or within 100 feet of State or Local Wetlands; therefore permits from the Conservation Commission are not required for the proposal.
- Department of Public Infrastructure comments have been provided and may be reviewed at Attachment 13.

Outside of this, no further comments from city offices were received in this matter.

HLS IPOD Guidelines

In order for a Special Permit to be approved under the stipulations of the IPOD, the Planning Board shall make a written finding that the proposed development is consistent with the objectives set forth under the HLS District Master Plan. In the submitted application, the applicant reports the design conforms to the design policies, principles and goals set forth in the HLS District Master Plan (*See Applicant's Narrative 4.0 Compliance with HLS IPOD Principles at Attachment 5.*

Guidelines are fully described under §4660. Special Permit and Site Plan Guidelines. The following highlights each of those six guidelines to be considered:

A. Relationship to Public Spaces

The vision articulated within the HLS ordinance and HLS Master Plan is the implementation of the tenets of Smart Growth principles and provide for diverse types and mixtures of land uses; therefore, the management of the transitions among private uses and between private and public spaces is critical. Twelve specific design goals are stipulated in this section ranging from the orientation of buildings and new developments to off-street parking.

Of particular relevance to the project development is the applicant's statement under 4.1 of the prepared Narrative that specific attention has been given to pedestrian access by integrating the proposed waterfront walkway with the other on-site walkways providing excellent north-south and east-west pedestrian paths throughout the site. Furthermore, the applicant reports the landscape strips constructed along the site's boundaries and building edges will frame the site, street edge and public spaces.

B. Historic Context

The objective set forth under the HLS IPOD relative to historic context speaks to the importance of adaptive reuse of some or all of the existing historic buildings.

The applicant has noted that preservation of the mill building was determined to be infeasible during the overall site assessment.

C. Relationship to the Waterfront

This precept articulates the importance of providing for connectivity to the waterfront. Public access to the waterfront and a community boathouse are anticipated in the final phases of multi-use development.

The applicant states the HLS Master Plan integrates the City's proposed waterfront pedestrian walkway with a waterfront park, whereby view sheds of the Acushnet River will be preserved and activated by use and new development proposals.

D. Environmental Goals

The HLS IPOD's environmental goals include compliance with Leadership in Energy and Environmental Design (LEED) certification standards and other evolving environmental efficiency standards. Green roof or alternative energy methods should be incorporated into the building design.

The applicant's narrative states building and parking lot lighting are designed for energy efficiency to the extent practicable. The proposed development will convert a vacant site to a landscaped, functional facility.

E. Economic Development Goals

The HLS' Master Plan Goals and the IPOD goals include the promotion of construction jobs and creation of permanent retail service sector jobs in the district.

The applicant states the proposed development will promote economic development by creating construction jobs and mixture of permanent restaurant and retail jobs in the HLS IPOD.

F. Master Plan Approach

All applications for development should be presented using the IPOD Master Plan approach and IPOD master plan goals which address various development phases and infrastructure improvements to coordinate and maximize the benefits of redevelopment and infrastructure improvements as much as possible.

The applicant states "the current proposal is for the construction of the building that will house a drive-thru restaurant; however, this is the third phase of a Master Development Plan. The Master Development Plan is still evolving, but by providing the interim Master Plan, driveways, walkways, utilities, and other requisite infrastructure elements were programmed in anticipation of the full development".

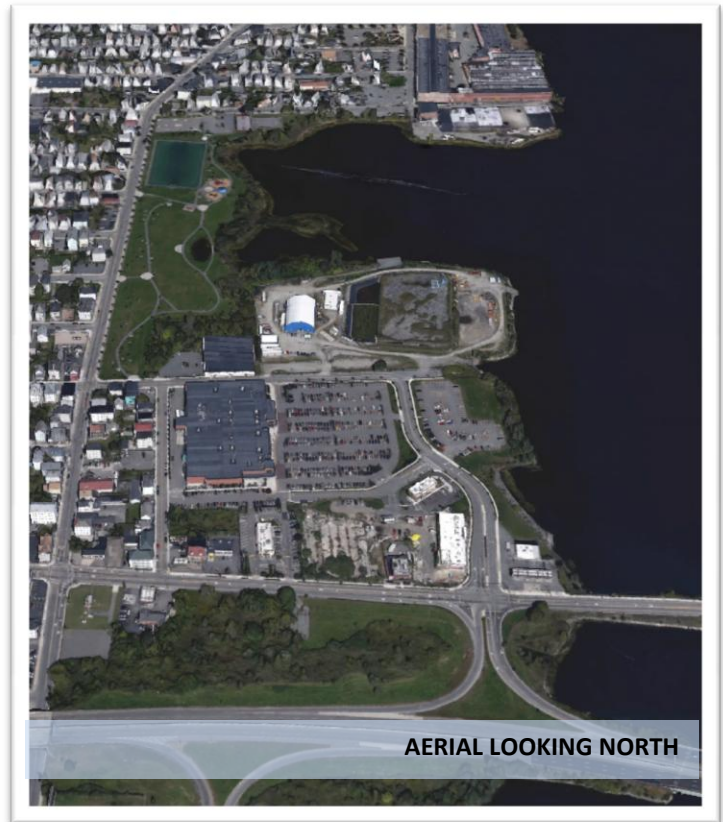
HLS IPOD Design Principles.

A set of nine design principles have been developed to assist in guiding the IPOD Master Plan. The nine design principles share equal weight and are as follow: History, Water Connection, Context, Framework, Mixed-Use, Street Grid, Parking, Reuse, and Pedestrian Friendly.

In reviewing the existing development, staff finds the design principles that have generally been met by the continued phased development of Riverside Landing Multi-Use project in the following ways:

- **Water Connection:** In optimizing the visual and physical connection to the Acushnet River, the final phase of development will offer public access to the waterfront and a community boathouse.
- **Context:** Riverside Landing, as a multi-use redevelopment of the former Fairhaven Mills, has improved access to Interstate 195, pedestrian connectivity via new sidewalk construction, roadway connectivity between Sawyer and Coggeshall Streets, connectivity to Riverfront Park, and created landscaping improvements to soften the hardscape and provide transpiration for the urban heat island within the block area.

- **Framework:** The overall project establishes a framework of streets, open space and infrastructure because of construction of a north-south public access road and east-west pedestrian access way which allows for public enjoyment of the Acushnet River and Riverfront Park.
- **Mixed-Use:** The overall multi-use development provides for a mix of uses by the phasing of retail, commercial and office use.
- **Street Grid:** The overall development extends the city street grid and creates an efficient block structure by the public access road.
- **Parking:** Shared parking is accommodated at grade.
- **Pedestrian Friendly:** The phased project development continues to create a pedestrian-friendly environment by landscaping, street trees, sidewalks, and pedestrian scale architecture and embellishments.



Staff Recommendation:

Having reviewed this request, the existing character of surrounding properties and thresholds required for Site Plan and Special Permit modification approval in the HLS IPOD, staff recommends approval of this application to include the following conditions:

1. That the architectural plans be stamped or signed by an architect registered in the Commonwealth of Massachusetts as stipulated under § 5440.
2. That the frequency of deliveries to the business be clarified.

Attachments:

1. Decision for Case 15-13
2. Minutes of Meeting Dated July 10, 2013
3. Modification for Special Permit Application
4. Modification for Site Plan Approval Application
5. Impact Report Dated July 14, 2016
6. Deed – Bristol County (S.D) Registry of Deeds Book 10843, Page 287
7. Approval Not Required Plan (ANR)-Bristol County (S.D) Registry of Plan Book 170, Page 6
8. Photos
9. Approved Layout Plan for Riverside Landing Phase III Dated 06/20/2016, revised thru 07/01/2013
10. Plan Set Revised thru August 4, 2016
11. Elevations & Floor Plan Dated 07/28/2016
12. Drive thru Circulation Plan
13. Department of Public Infrastructure Memorandum Dated August 1, 2016



PLANNING BOARD

CITY OF NEW BEDFORD
JONATHAN F. MITCHELL, MAYOR

AUG 03 2016

date

Date Issued **AUG 03 2016**
A TRUE COPY ATTEST

Stephane M. Macomber
CITY CLERK-NEW BEDFORD, MA

Stephane M. Macomber
Asst City Clerk

This is to certify that notice of the grant of this appeal was filed with City Clerk on July 23, 2016; and no appeal from the decision of the Board has been filed within the 21 day appeal period.

Stephane M. Macomber
Asst. City Clerk

CITY CLERK

2013 JUL 23 A 10:23

CITY CLERKS OFFICE
NEW BEDFORD, MA

Decision Granting Special Permit/Site Plan Approval

Application of,
Highway View & Coggeshall-New Bedford LLC
1266 Furnace Brook Parkway,
Quincy MA 02169

Procedural History

Case # 15-13 Application for Special Permit and Site Plan Review for a proposed Medical Ambulatory Facility and Drive-thru Bank with Ancillary Parking Lot and a Curb Cut located along Coggeshall Street within the Hicks Logan Sawyer (IPOD) Interim Planning Overlay District. Applicant: Highway View & Coggeshall-New Bedford LLC, 1266 Furnace Brook Parkway, Quincy MA 02169. The Proposed Project is located in the Riverside Landing Plot 93-2 Lot 264. Plans Submitted by: Prime Engineering 350 Bedford Street, Lakeville, MA 02347.

Plans titled "Riverside Landing - Multi-Use Development at the Former Fairhaven Mills Site, Coggeshall Street, New Bedford, Massachusetts, Phase III Site Plans Issued for Special Permit", in sufficient detail were filed with the City Clerk and the New Bedford Planning Board pursuant to Chapter 9, Sections 4610 and 5400 of the Code of Ordinances of the City of New Bedford on June 20, 2013, and revised July 1, 2013.

At a public meeting of the Planning Board held on July 10, 2013 the Planning Board reviewed the plans submitted along with the written comments from the various City Boards and Commissions. Throughout its deliberations, the Planning Board has been mindful of the testimony of the applicants and their representatives, and the written comments of the various City Boards, Commissions and Departments and the general public, all as made or offered at the public hearing.

Findings

A- General

1. The subject property is shown on Assessor's Map Plot 93-2 Lot 264, more specifically in the Riverside Landing - Multi-Use Development at the corner of Coggeshall Street and Veterans Memorial Drive.
2. Direct abutters and abutters to those direct abutters, as well as those directly opposite the street have been notified as per MGL, Ch. 40A, Section 11. The returned abutter notifications are located in the City Planning Office file for this project (15-13).
3. The project proposes to build a Medical Ambulatory Facility and Drive-thru Bank with Ancillary Parking Lot and a Curb Cut. (Approx. 10,165 S.F. for medical ambulatory facility and 1,722 S.F. for the bank; excluding the parking lot). The proposed project includes the use of off-street parking of sufficient size and quantity to meet the current site conditions, relative to the aggregated uses proposed at this time.
4. The project will be served by existing public ways.

HLS IPOD Review Criteria

1. Chapter 9 of the New Bedford Code of Ordinances, Section 4610 through 4671 set forth the criteria that the Planning Board shall apply during review of the site plans associated with this request for a special permit approval. The Planning Board has considered all of the above criteria and will impose reasonable conditions in their approval relating to these criteria.

B- Specific Findings

1. The proposal will provide medical and business uses which are appropriate under the current zoning. The private provision of urban streetscape amenities and infrastructure improvements are a tenet of the "smart growth" principles envisioned for this area. Local job creation will benefit living conditions in this neighborhood and the neighborhood will benefit directly from the goods and services provided by the proposal.
2. The traffic and circulation data for this project was based on the following reports/memorandums prepared by Vanasse and Associates as stated by Richard Rheame, Chief Engineer with Prime Engineering in his letter dated June 21, 2013 submitted along with this application. Additionally, the proposed project does not appear to adversely impact the existing or future flow of traffic on adjacent public ways and highways:

- a. Traffic Impact and Access Study prepared by for both Phases I and Phase II in 2009;
 - b. Traffic Impact and Access Study initially prepared by Vanasse and Associates for Phase III in 2013;
 - c. Supplemental Traffic Impact Assessment prepared by Vanasse and Associates submitted at the July 10, 2013 Planning Board hearing.
3. The parking areas have shown landscaping and screening meant to minimize the large expanse of parking. The colorized crosswalks alert motorists of the need for caution. Like treatments, less the grade change, shall be incorporated at each crosswalk within the project area. All crosswalks and pedestrian crossings shall be consistent with the treatments located in Phase I (Market Basket).
4. The applicant has committed to utilize demolished mill building features to construct the retaining wall located to the east side the proposed bank, and along Coggeshall Street on the south side of the project site.

Decision

In view of the foregoing, the Planning Board hereby decides that the applicant, **Highway View LLC**, has satisfied the conditions necessary for the granting of a Special Permit/Site Plan Approval, as authorized under Chapter 9, § 4660, The Hicks-Logan-Sawyer District Interim Planning Overlay District (HLS IPOD) of the New Bedford Code of City Ordinances, and therefore decided to grant the Special Permit/Site Plan Approval for the proposed construction, as submitted/ revised, in accordance with the conditions set forth below:

1. That all building materials are as represented on the plans, and sample materials provided.
2. That all lighting shall be consistent with those found in Phase 1, and Phase 2 of the Riverside Landing development.
3. That the medical ambulatory facility shall have one wall sign on the east-facing façade; and one approximately 7 foot high by 4.5 feet wide ground sign, internally lit, in the location depicted on the plans, with the design as presented during the Planning Board hearing (not as depicted in the plan set). That any additional signage shall require all applicable permits.
4. That the parking is shown as depicted on the plans and that all parking islands shall be landscaped.
5. That there is one dumpster as depicted on the site plan for the medical ambulatory facility; the bank shall not have a dumpster.

6. That the stonewall shall be constructed with the granite remaining from the mill building that was demolished on the Fairhaven Mills site; and shall be located along Coggeshall Street, to the east and west of the proposed cut, and curve in at the proposed curb cut on both sides, and travel along the property line on the west side of the existing brick mill building.
7. That all bike racks shall be of the s-curve style.
8. That all benches shall be consistent with the benches approved in Phase 1 and Phase 2.
9. That silt sacks shall be placed along all drives during construction.
10. That the bollards shall have plastic coverings.
11. That the handicap sign poles shall have plastic coverings.
12. That the Washingtonian light depicted on the site plan directly adjacent to the proposed curb cut shall be moved to the east side of the curb cut.
13. That the light depicted on the site plan shall be moved from its location next to the bank employee parking northerly along the property line of the existing brick mill building.
14. If there are any substantial changes required to be made to the Project, as approved by the New Bedford Planning Board on July 10, 2013, the Applicant must submit these changes to the Board for further approval.
15. The Applicant shall file a final, "as built" site plan, acceptable to the New Bedford Planning Board and the Department of Public Infrastructure, for the record, prior to the issuance of a Certificate of Occupancy by the Department of Inspectional Services.
16. The Planning Board incorporates into this decision, and by reference, the Traffic Impact Reports dated 2009, and 2013; and the supplemental information dated July 10, 2013, prepared by Vanasse and Associates and provided to the Planning Board by Prime Engineering Inc., on behalf of **Highway View LLC**.
17. The Planning Board further incorporates the letter from Ronald Labelle, Commissioner of the Department of Public Infrastructure, dated June 11, 2013 to be part of this decision.
18. The Planning Board recognizes that the maintenance of the landscaped elements of this project area is critical to the success of this project and to the success of the intent of the IPOD language and the overall Hicks Logan Sawyer Master Plan. The Board has asked and the Developer has agreed to create and maintain a

robust landscape maintenance schedule which includes the maintenance of all green spaces within the project limits.

19. This Special Permit/Site Plan Approval shall lapse if a substantial use thereof or construction there under has not begun, except for good cause, within twelve (12) months following the filing of the Special Permit approval. The Special Permit must be filed with the Bristol County S.D. Registry of Deeds.

20. A copy of this decision will be filed with the City Clerk of the City of New Bedford on July 23, 2013. Any person aggrieved by this decision has twenty (20) days to appeal this decision in accordance with the procedures set forth in M.G.L. Chapter 40A § 17.

Record of Vote

The following members of the New Bedford Planning Board vote to grant Special Permit/Site Plan Approval for this project as submitted subject to the aforementioned conditions.

Arthur Glassman, Janine da Silva, Kathryn Duff, George Smith.

The following members of the New Bedford Planning Board vote in opposition to granting Special Permit/Site Plan Approval for this project.

Peter Cruz.

The motion to accept this decision to approve the Special Permit/Site Plan for Highway View LLC passed by a vote of 4-1.

Dated this twenty-third day of July 2013

New Bedford Planning Board


Kathryn Duff, Clerk

WEDNESDAY, July 10, 2013

MEMBERS PRESENT:

Chairman Arthur Glassman
Janine Da Silva
Kathryn Duff
Peter Cruz
George Smith, Associate Member

ALSO IN ATTENDANCE:

Jill Maclean, City Planner

CITY CLERKS OFFICE
NEW BEDFORD, MA
2013 APR - 7 P 3:36
CITY CLERK

Chairman Glassman called the meeting to order and called the role.

A motion was made (KD) and seconded (JD) to accept the June 10, 2013 minutes.

Public Hearings

CASE #10-13 - Continued Public Hearing - Special Permit and Site Plan Review

A motion was made (JD) and seconded (KD) to open the public hearing on Case 10-13, applicant having withdrawn the review.

A motion was made (JD) and seconded (KD) to accept the withdrawal.

A motion was made (JD) and seconded (KD) to close the special permit and site plan review hearing for Case 10-13.

Motion passed unopposed.

CASE # 14-13 - Written Motion, Councilor Martins, on behalf of WINN Development

Ms. Maclean provided background as follows: Councilor Martins is asking that the two parking lots associated with Cliftex and Whalers Place be rezoned from their current Mixed Use Business to Residential Zoning. The recommendation did not specify what type of residential, but the surrounding area is zoned Residential C. This would not change the special permit which would still remain parking and would not allow for building on the lots, but would change their tax rate.

Upon invitation from Chairman Glassman, Attorney Chris Saunders of 700 Pleasant Street, New Bedford addressed the board on behalf of WINN Development. The parking lots are utilized for the residential dwelling units, and are now being taxed at a commercial rate. Because there are special permits associated with the projects, the lots can only be used for a residential purpose.

Case # 15-13 - Special Permit and Site Plan Review

Ms. Maclean noted that the protocol again, due to the IPOD location of the project, will require the board take up the site plan review as one vote and the special permit as a second vote, both votes needing four in the affirmative to pass. Voting no on the site plan requires a no vote on the special permit.

Mark White of White Construction for the developers representing Harborview Coggeshall LLC, accompanied by Rich Rheume of Prime Engineering, Eric Juliano and Steven Vanasse from Stucky and Bradley Architects, Michael Radner representing Bradley Landscape Design, and Sean Kelly from Vanasse Associates, addressed the board. Essentially it's the same design team from Phase 1 and Phase 2.

Mr. White indicated they had done a successful Phase 1 and are now under construction of Phase 2 and the mixed use building. They are now proposing the medical building as Phase 3 of the proposal. Mr. White introduced Rick Rheume to further address the board.

Mr. Rheume stated the project is to the northern end of the Hicks/Logan Complex, just north of Coggeshall St. The city's master plan of re-developing the area contained goals and principles that applicants should try to achieve, and the applicant showed a master plan of the block between Sawyer and Coggeshall, and made the buildings compatible with the historic character. The applicant has developed a new public boulevard through the site to develop and strengthen the connection. Phase 1, the Market Basket Site, has been completed. The original master plan saw two buildings and the tree-lined boulevard as Phase 1. The area east of Memorial Way has been used temporarily by Market Basket as parking. The design plans for the Mill Tower complex was fully built as proposed. He stated that Phase 2, the Taco Bell and the multi-use building, also saw overall goals met and was constructed as designed and permitted. The Phase 3 design, the Hawthorn Medical Complex and proposed bank, will include a relocated curb cut on the east side designed for optimum safety and has egress from Phase 2, blending the phases together. Goals were to acquire and utilize the vacant land adjacent to Riverside, compliment existing architecture, add medical use, and add a drive thru-bank with parking in the front. Working with the planning staff and DPI, the applicant evolved the current plan set presented this evening. He stated the existing site is predominately a concrete pad with an old unfriendly chain-link fence running along Coggeshall St. The applicant proposes to define the edge, do nice landscaping, and bring the site into the neighborhood.

Mr. Rheume indicated the westerly neighbor is McDonalds, Market Basket to the north and Taco Bell to the east. The applicant will employ the same elements, same style crosswalk, same size sidewalks, same style ramps, benches, trash receptacles and bike racks, to create uniformity between all the phases. Additionally, parking lot lighting will have the same uniform standards. Along Coggeshall St. the Washingtonian style lights and street trees have already been installed.

Mr. Rheume indicated that a key element the planning board worked with the applicant on was having a granite wall defining the street edge. It was done along Sawyer Street and is being done along Veteran's Memorial Way, and it will be continued, defining the street edge with landscaping behind it along the frontage, again giving a consistency to the look and feel, and more pedestrian friendly. The applicant proposes sidewalks on both sides along Sawyer and

Mitchell Streets, Bellville and all along Coggeshall Street over to Veteran's Memorial Way. An additional sidewalk system is being developed to run along the face of Taco Bell and along the face of the multi-use building and between the two, continuing the element from the bus stop to Coggeshall Street, unifying the entire development from a pedestrian standpoint.

Mr. Rheume continued, stating the minimum parking in the underlying district is the maximum allowed in the IPOD district; the concept being the use of shared parking, minimizing asphalt and maximizing landscaping. Mr. Rheume presented a table showing minimum required spaces of 151 for the specific tenants. The total the applicant has is 147, meeting the IPOD goal.

Due to concerns about large vehicles circulating throughout the site, Mr. Rheume indicated that Vanasse Associates did a computer model assuring the trash vehicles can enter and exit safely and effectively. Mr. Rheume again indicated that the applicant had met all of the IPOD goals and principles. He then introduced Sean Kelly from Vanasse Associates to address the traffic analysis.

Sean Kelly, traffic engineer with Vanasse Associates addressed the board, and submitted supplemental material as a result of a meeting with city staff per Mr. Cruz's requests.

Located between the existing McDonalds and the access to Riverside Landing, the project analyzed is a medical office building and drive-thru bank. The study area included the intersections accommodating the majority of project traffic. The study looked at two signals on Coggeshall Street; Bellville Avenue and the signal across from the ramps and Veteran's Memorial Way. The study also evaluated the proposed site driveway onto Coggeshall and the internal driveway as part of the supplemental analysis.

Mr. Kelly stated the study was done in accordance with industry and state guidelines; including current traffic counts, weekday morning 7:00-9:00 and weekday evening 4:00-6:00, as well as Saturday midday 11:00 – 2:00 the commercial shopping peak. Volumes were adjusted to account for traffic growth and to account for the unbuilt development within Riverside Landing. Their previous filings having been reviewed by Mass DOT and MEBA. Their future conditional analysis includes everything present today and everything in the additional site build out. The site analysis data relied on the ITE database, and Mr. Kelly then broke down the expected visits for both the medical and bank buildings. The analysis incorporated a 25% "pass by" trips. He stated the site provides multiple entrance options and overall project delay impacts are estimated at a second or two per vehicle. Mr. Kelly stated the focus has been access, and what became clear was the exiting left turn traffic would not work from a full front access plan. Exiting traffic from that driveway is now proposed to be right turn only. Safe inbound movement for a left-turn in already exists. The city staff suggested modifying the island to further restrict the left turn movement, and it has been extended as far as possible while still being able to accommodate a truck pulling out. The driveway will have a pedestrian cut out in the middle of the island. To address Mr. Cruz' intersection concerns, counts were done and those observations were presented to the board in a memo. Indicating the traffic study did not see a lot of impact and the site access works safely.

Steve Vanasse of Stucky and Bradley Architects addressed the board, stating the firm is the architect for the 10,000 s.f. urgent care center as a satellite of St. Anne's Hospital in Fall River. The facility will have exam rooms, treatment rooms and radiology rooms. Mr. Vanasse showed the primary entrance and covered drop-off on the plans, as well as the handicap parking. He indicated the location of the service entry and ambulance bay. Mr. Vanasse then went over the interior floor plan. The front entry will have two colors of brick for the face and punched window openings. The flat canopy will be supported on brick piers. All four sides of the building now have masonry up to approximately the fourteen foot line. There is a beige roof screen as well, that serves as a screen both visually and acoustically. The signage is a matching silver metal panel common to St. Anne's/Stewart facilities. The intent in the selection of the brick and colors was to be consistent with some of the other buildings on the site. Mr. Vanasse invited questions.

The board wishing to see the entire presentation before questions then heard from Michael Radner of Radner Design Associates with regard to landscaping. The overall landscape approach is to plant durable, colorful, drought tolerant plants with year-round interest. This is a continuation of Phase 2 and the plant palate is essentially the same with some variation. Basic landscape design principles are about rhythm and repetition. For example, using 10-12 shrubs of the same type blocked together. This is particularly true for a commercial landscape of this type and along Coggeshall St., which is basically a highway. You want large drifts of planting to provide maximum visual impact. All of the proposed landscape islands will have a large shade tree. Mr. Radner stated they had tried to minimize conventional lawn by filling with low maintenance plantings rather than lawn. The parking lot and the trees within it are, again, a continuation of Phase 1 and Phase 2, and will contain Zelcova trees, pin oaks and sugar maples, all drought tolerant species. Along the front of the medical building will be three tree pits in the sidewalk in tree grates providing additional vegetation between the parking and the building.

Mr. Radner stated that screening and softening of the hard edges on the eastern site border will be done with Giant Arborvitaes, providing a thick evergreen screen, with forsythia interspersed. There will be another Arborvitae row along Coggeshall Street, as well as at the service bay and trash enclosure for screening. The site entrance will have a low stone wall, so shrubs and perennials will be planted, further increasing the height of the screening to the parking lot.

Mr. Radner continued by citing the plantings proposed along the medical building. In the small islands there will be a planting of day lilies. Planting around the bank are plant beds of the same palate seen along the street, with a seasonal variety and fall colors as well.

Mr. Rheume stated that originally a pylon sign was proposed for the area, but it met with objection from planning board members and staff. It was converted to a ground sign and applicant is predominantly relying on the signage on the building. In conclusion, Phase 3 is additional redevelopment consistent with the original master plan, and buildings are compatible with the historic character and compatible with other buildings on the site. The applicant is providing new functions that will benefit the city and the neighborhood, and are meeting the design policies, principles and goals of the IPOD district. Board questions were invited.

Mr. Smith inquired about the landscaping in the back of the building.

Mr. Rheume stated that McDonalds has a 6' arborvitae hedge for the entire length. The project developer at the behest of the planning department has agreed to put in a brick wall.

Mr. Smith confirmed that the facility will be urgent care and inquired if the urgent care at Mendel Electric on Rte. 6 is the same as this. Mr. Smith explained that a number of seniors and others go to urgent care for mammograms and Coumadin blood level checks, and this contributes to a traffic concern for him.

Speaker explained that the facility will be a hospital satellite with a little different focus than Hawthorn on Rte. 6 and would not be a "blood work" facility for Coumadin patients. He continued that the Rte. 6 Hawthorn is a much larger facility and has much more radiology capabilities as well.

Ms. Maclean suggested addressing architecture and landscaping first.

Mr. Smith had no landscaping questions.

Mr. Cruz likewise had not landscaping questions.

Ms. Duff inquired about the stone wall at the entrance along Coggeshall Street and stated she would like a slight curve rather than an abrupt ending, so that the wall follows the curb line. She asked if the sign shown was the Taco Bell sign.

Speaker indicated one was a ground sign and one was a pylon sign. Ms. Duff requested communication with the neighbor about moving his vehicles before he is boxed in with the wall. Ms. Duff further discussed the wall and its end at the loss of grade.

Ms. Duff stated that the sign shown did not seem to correspond with the timber and brick sign in the board's set, which the applicant confirmed was correct. The applicant stated the one shown this evening is the one currently before the board, which is 85 high x 54 wide with internal lighting. The applicant stated there would not be an additional ground sign for the bank, and requested that staff be allowed to approve the final elevations of the bank once developed.

Ms. Duff recommended the bike rack be an s-curve and mimic the Stewart sign colors. Applicant agreed. Ms. Duff thanked the team for their thorough presentation. In answer to Ms. Duff's inquiry, applicant stated the flat roof was white.

Ms. DaSilva inquired about the granite wall and the reclaiming comment. She wondered what material would be used once the reclaimed granite ran out. Applicant stated they would use comparable granite from other sites in that event, but they expected to have enough for the project.

Mr. Glassman commended the presentation and stated it answered his landscaping and architecture questions.

Ms. Maclean suggested the next round of questions address parking and traffic.

Mr. Smith inquired if there was any survey done at noontime regarding traffic affecting McDonalds. Applicant stated there was not, but that if traffic works at the study peaks it will work just as well at noon.

Mr. Smith inquired of the notation that June traffic is 3-10% higher per Mass DOT. The applicant stated that in a survey each month has variations, June volume being higher according to state data.

Mr. Smith inquired about whether the source of information for traffic accidents was New Bedford Police or DOT. Applicant stated they use the DOT data, which is often behind a couple of years. Much of the data available was prior to the signal at the ramp being in place. Mr. Smith also inquired about new exit travel. Applicant stated the exit design attempts to force them to the right. He stated a signal turn would force more traffic and unnecessary site circulation. Mr. Smith stated the report shows some 800 cars from 4:00-6:00 traveling eastbound. Applicant acknowledged the cars heading into Fairhaven will queue back as far as the driveway, as is the reason lefts out cannot be endorsed.

Mr. Cruz thanked the presenters. He expressed concern about the 1.28 crash rate, which exceeds the state average, at the single intersection at Coggeshall and the site drive. Mr. Kelly did not believe newer data would reflect that crash rate, as many were angle collisions and are now protected moves with the phasing of the lanes present today. Applicant did not believe the rate would increase with the anticipated traffic volume.

Mr. Cruz stated the original report stated the collisions occurred before the traffic signal improvements at Coggeshall and Veteran's Memorial. Though permitted in March 2009, the grand opening was in 2010. Mr. Cruz inquired of the effect on the report. Applicant stated the data is hybrid in that some of the data had the signal in place, and some did not, and he acknowledged the 13 crashes in 2010, but noted the drop from 19 crashes in 2008. Mr. Cruz also expressed concerns about the intersection of Coggeshall and Bellville Avenue, with a crash rate at the threshold of .74 and sought any applicant recommendations. Applicant stated there have been maximum improvements to the intersection since the older data, and that the intersection has no room for any additional lanes. Mr. Cruz inquired about signal timing modifications improving the rate and making it a safer intersection. Applicant stated the intersection is under the Mass DOT jurisdiction and has been signed off by them with regard to the timing and phasing.

Mr. Cruz inquired whether the site growth included the future build of the water view property. Applicant stated that a certain build out was assumed at the permitting phase some 4 years ago. Currently there is another 31,000 in retail, space and 19 in office space still out there and all that traffic was generated for and includes development of all the land in the original submission.

Mr. Cruz then inquired as to the level of service at the signal intersection, and expressed concern that the 2018 build noted in the traffic plan was a level of service on the threshold of becoming an E. Applicant stated he had no suggestions on how to further improve it absent adding more

lanes, and that before the light went in, it was an F and had one of the highest crash rates in the Commonwealth.

Mr. Cruz wanted to know why the Sawyer Street/Bellville Ave intersection wasn't looked at as part of the report when some of the traffic taking the left onto the site is going to the Market Basket. Applicant stated it was all looked at in the master plan and studied in the MEBA process, but they focused on the intersections leading to the proposed access drive and queuing from those points. Mr. Cruz stated the level of service on the westbound movement is at a D on the verge of an E on a brand new roadway. Applicant clarified it looks at the intersection in a vacuum and he expects more gaps in traffic than suggested in the analysis, as it does not take fully into account the effect of the light metering traffic on Veteran's Memorial Way.

Ms. Duff stated her biggest concern is the left-hand turn coming into the site inviting more accidents, as she feels a left-hand turn in invites drivers to take a left-hand turn out regardless of the right directing curbing. Applicant stated in the evening peak there is a queue that is not convenient for the left-hand turn.

Ms. Duff commented that she expects more than a 1% growth rate. Applicant stated it is a MEBA 1% compounded over five years, and estimates it may be closer to 10%.

Ms. DaSilva thanked the applicant for the presentation. She stated her concern is that the intersection is not pedestrian friendly. She wanted to know about the entering of the trash management vehicles. Applicant stated they expect trash trucks to take a left turn into the site, pick up the trash, go around and out.

Ms. DaSilva thought the trucks were too big, and the lane should be narrowed in order to be a more a pedestrian friendly area, allowing width only for cars and vans; and have trash management go through Veteran's Memorial. Applicant stated they could direct trash vehicle direction and movement, as well as times of day. He believes that by allowing left turns the intersection is relieved. He again mentioned the island that protects the pedestrian travel across the driveway. Ms. DaSilva clarified that she is only talking about limiting trash trucks and delivery trucks, and directing them to the main intersection designed for their use. Mr. Cruz also noted the Mitchell Street back entrance. Ms. DaSilva wanted to know why the access is designated at 15' and 17' wide. Applicant noted that in turning movements the width is needed for emergency vehicles, etc.

Ms. DaSilva inquired as to the size of the vehicles, to which the applicant acknowledge truck size. The applicant stated the deliveries are small van deliveries and not semi-trailers coming in; the largest expected to be the trash vehicles perhaps twice a week.

Mr. Glassman also expressed a small concern about the left turn, but wants to rely on the traffic commission review. Ms. Maclean clarified that traffic, as part of the site plan, is within the board's purview. Mr. Glassman suggested the motion contain language that it is subject to the traffic department.

Mr. Smith questioned whether there was any possible compromise on the driveway. Edmund Watson from Prime Engineering stated originally full access was planned, and after analysis, changes were made, and the submission before the board already represents a compromise. After a meeting with planning staff, the island was extended and the turn was made even more drastic.

In response to a question by Mr. Cruz, Mr. Watson stated that curb to curb was about 100 feet. Ms. Maclean clarified that the board needs to determine whether or not to approve this deferred plat as part of the site plan and what that would be, and then the traffic commission would approve that. But a recommendation to the traffic commission is needed.

Mr. Duff asked what the intent for the original full access design was. Applicant stated to give adequate access to the site and a full visual to the site to get as many people to the site as possible. In a compromise, the applicant has dropped one of the movements out and then tried to enhance and develop the street edge.

Mr. Cruz inquired about issues with cars queuing using the bank and queuing back into Coggeshall. Applicant stated they do not expect any issues for the use there and had done studies for other banks.

In response to a question from Ms. DaSilva, the applicant stated the width of the drive-thru lanes curb to curb were 9 feet, the painted lane being 11 feet, standard for any bank.

Mr. Smith wanted to know who the compromise was with. Applicant stated the compromise was internally in the project designs; that originally there was no island forcing drivers right. There was a full access/full egress drive that had lefts in, lefts out, etc.

Mr. Smith sought to clarify applicant's comment for the television audience, stating "You made it sound like the compromise was with this board." Applicant clarified it was with the tenant.

Ms. Duff inquired if two of the streetlights were removed with the curb cut. Applicant stated there was a Washingtonian light they did not want to remove and so it misses that light. Ms. Duff asked if there was another in the middle of drive that was removed. Applicant stated any Washingtonian lights removed were replaced or relocated.

Mr. Cruz inquired if there was a dumpster for the bank on the site. Applicant stated they shred and have it picked up, and have no dumpsters.

Mr. Smith questioned whether handicapped parking for five cars was enough. Applicant stated they added the drop off area for handicap and 3 van accessible spaces, above the state requirement. So, the site has four handicap spaces, though only three are required. In response to Mr. Smith's inquiry on putting more, the applicant stated there is room to add more if warranted.

Mr. Smith stated the applicant has made it look like he has more parking than needed when he actually has 13 spaces under the parking requirement, as it includes the Market Basket parking.

Applicant acknowledge they were asked to provide the overall, and on another table eliminated Market Basket and showed only Phase 2 and Phase 3 to meet the IPOD goal. He stated they were 4 spaces shy of the amount required in the underlying zoning district.

Mr. Cruz shared Ms. Duff's concern about the wall not wrapping. Applicant stated it was a nice touch and they would commit to it. Mr. Cruz inquired if there was excess granite could the small section in Phase 2 be closed off with wall. The applicant clarified the area in question and explained there were drainage concerns if any dirt was used in conjunction with the wall, but they would consider adding the section of wall only.

Mr. Cruz suggested the applicant take a second look at centering the accessible ramps. Mr. Cruz noted the insulation of the silk sac was addressed on Phase 2 and stated he wanted to see the overall project, such as silk sacs along the access road, Veteran's Memorial and anything along the frontage on Coggeshall, as they are not a budget breaker. In response to Mr. Cruz's comment, the applicant committed to bollard covers.

Mr. Cruz stated he was hoping for a more unique sign better matching the Phase 2 approval. He also stated he had some issues with the curb cut in general, as expressed on June 25, the 3 gateways should facilitate all moves on the site and he saw no need for the curb cut. Applicant stated it was important for developments to the north of Riverside Landing, firmly stating the left turn in will alleviate the left turn movements at the main intersection, now and in the future.

Ms. Duff felt her questions had been covered. She reconfirmed that one light would be taken out with the curb cut, and that the light for the bank, as shown, is right in front of the street light. Applicant stated the Washingtonian lights, while decorative, were not especially effective in casting a proper lighting of the lot, but they would look at the area. Ms. Duff suggested adding the second Washingtonian light to the right of the curb cut to frame the entrance.

Ms. DaSilva questioned building lighting and the applicant stated there would be wall packs comparable to the other buildings. Applicant further stated the majority of entry lighting would be under the canopy, but would be on the building to light the sidewalks for safety. The bank likewise would have on-building lighting and sconces to light the front walkway.

Mr. Glassman felt the questions had been asked and answered.

Mr. Cruz requested a copy of the presentation for the record. He also requested his comments be placed on record for the traffic study.

A motion to receive was made (GS) and seconded (KD).

A motion was made (JD) and seconded (PC) to open the public hearing.

In response to an invitation by Mr. Glassman to speak in favor of the project, Derek Santos of New Bedford Economic Development Council addressed the board, stating the EDC supports the project and has from the beginning, and the council is eager to work with the developers. He feels the project is important, with the water edge piece, the river walk, and the mills going into the upper harbor.

There was no response to a further invitation by Mr. Glassman to speak in favor of the project.

There was no response to an invitation by Mr. Glassman to be recorded in favor.

There was no response to an invitation by Mr. Glassman to speak in opposition.

There was no response to an invitation by Mr. Glassman to be recorded as opposed.

A motion was made (JD) and seconded (PC) to close the public hearing.

The board discussed their notes with regard to the various proposed contents of a motion. Ms. Maclean clarified that per solicitors, the board could not sever the curb cut as it is part of the site plan.

In response to an inquiry, applicant added the standard minimum cut width was 12'.

After further board member discussion, a motion was made (JD) and seconded (GS) to approve the special permit for Case #15-13 for Riverside Landing on Coggeshall Street, Plot 93-2, Lot 261 and 264, with the following conditions:

- That the stonewall follow the curb lines around the Taco Bell sign
- That the sign be 85" by 54" as presented today and be internally lit
- That no dumpster is approved for the bank
- That the S-curve bike rack be installed by the medicine building and it be blue
- That the benches be installed as the remainder of the site and they be green as they are at Market Basket
- That the silk sacs be placed along all the drives
- That bollards have plastic covers
- That handicap sign covers be installed on the site
- That the Washingtonian light currently within the middle path of the access lane be moved easterly to be mirrored by the westerly Washingtonian light along the entrance
- That the security light in the parking lot can be moved northerly as long as it does not interfere with the adjacent building
- That granite walls be extended from the gateway area west to the southwest corner of Phase 1
- That the board approve the curb cut and access into the lot as shown on the plan

Motion passed 4-1

Mr. Glassman invited a motion for the site plan. Ms. Maclean stated the previous vote was for the site plan.

Ms. DaSilva introduced the same motion, seconded by Ms. Duff, for the special permit for the IPOD overlay district.

Motion passed 4-1



CITY OF NEW BEDFORD
JONATHAN F. MITCHELL, MAYOR

PLANNING BOARD

SUBMIT TO:
Planning Department
133 William Street
Room 303
New Bedford, MA 0274

SPECIAL PERMIT APPLICATION FOR MODIFICATION

The undersigned, being the Applicant, seeks Special Permit Approval for property depicted on a plan entitled: "Riverside Landing Multi-Use Development at the Former Fairhaven Mills" by: Prime Engineering, Inc. dated: June 22, 2016

1. Application Information

Street Address: Riverside Landing, Coggeshall Street

Assessor's Map(s): 93 Lot(s) 292

Registry of Deeds Book: 10843 Page: 287

Zoning District: Mixed use and Hicks Logan Sawyer IPOD

Applicant's Name (printed): D.W. New Bedford LLC

Mailing Address: 1266 Furnace Brook Parkway, Quincy, MA 02169

(Street) (City) (State) (Zip)

Contact Information: 617 285-4780

Telephone Number Email Address

Applicant's Relationship to Property: ☒ Owner ☐ Contract Vendee ☐ Other _____

List all submitted materials (include document titles & volume numbers where applicable) below:

Refer to cover letter.

By signing below, I/we acknowledge that all information presented herein is true to the best of my/our knowledge. I/we further understand that any false information intentionally provided or omitted is grounds for the revocation of the approval (s). I/we also give Planning Department staff and Planning Board Members the right to access the premises (both interior and exterior) at reasonable times and upon reasonable notice for the purpose of taking photographs and conducting other visual inspections.

July 14, 2016
Date

[Signature]
Signature of Applicant

City Hall • 133 William Street • Room 303 • New Bedford, MA 02740 • www.newbedford-ma.gov
PH: (508)979-1488 • FX: (508)979-1576

2. Zoning Classifications

Present Use of Premises: Vacant parcel

Proposed Use of Premises: Drive through restaurant

Zoning Relief Previously Granted (Variances, Special Permits, with Dates Granted):
Special Permit and Site Plan Review - summer 2013

3. Briefly Describe the Proposed Project and Specify all Requested Special Permits:

In the summer of 2013, the New Bedford Planning Board issued a Special Permit and Site Plan Review approval for a 1,722 square foot bank and for a 10,165 square foot urgent care facility building on a 2.6 acre parcel which is referenced as Assessors' Map 93 Lots 261 and 292 on Coggeshall Street. It is now proposed to substitute a 2,265 square foot drive through restaurant for the previously approved 1,722 square foot drive through bank.
(Case #15-13)

4. Please complete the following:

	<u>Existing</u>	<u>Allowed/Required</u>	<u>Proposed</u>
Lot Area (sq ft)			
Lot Width (ft)			
Number of Dwelling Units	N/A	IPOD	
Total Gross Floor Area (sq ft)			
Residential Gross Floor Area (sq ft)			
Non-Residential Gross Floor Area (sq ft)			
Building Height (ft)			
Front Setback (ft)			
Side Setback (ft)			
Side Setback (ft)			
Rear Setback (ft)			
Lot Coverage by Buildings (% of Lot Area)			
Permeable Open Space (% of Lot Area)			
Green Space (% of Lot Area)			
Off-Street Parking Spaces			
Long-Term Bicycle Parking Spaces			
Short-Term Bicycle Parking Spaces			
Loading Bays			

5. Please complete the following:

	Existing	Proposed
a) Number of customers per day:	<u>0</u>	<u>200</u>
b) Number of employees:	<u>0</u>	<u>20</u>
c) Hours of operation:	<u>0</u>	<u>6 a.m. - 12 p.m.</u>
d) Days of operation:	<u>0</u>	<u>7 days/week</u>
e) Hours of deliveries:	<u>0</u>	<u>6 a.m. - 6 p.m.</u>
f) Frequency of deliveries:	<input checked="" type="checkbox"/> Daily <input type="checkbox"/> Weekly <input type="checkbox"/> Monthly <input type="checkbox"/> Other: _____	

6. OWNERSHIP VERIFICATION

This section is to be completed & signed by the property owner:

I hereby authorize the following Applicant: D.W. New Bedford LLC

at the following address: 1266 Furnace Brook Parkway, Quincy, MA 02169

to apply for: Modification to Special Permit

on premises located at: Riverside Landing, Coggeshall Street

in current ownership since: July 23, 2016

whose address is: Same as above

for which the record title stands in the name of: D.W. New Bedford LLC

whose address is: Same as above

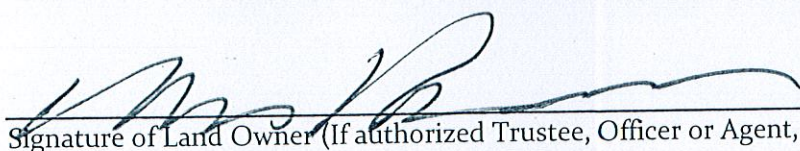
by a deed duly recorded in the:

Registry of Deeds of County: Bristol Book: 10843 Page: 289

OR Registry District of the Land Court, Certificate No.: _____ Book: _____ Page: _____

I/we acknowledge that all information presented herein is true to the best of my/our knowledge. I/we further understand that any false information intentionally provided or omitted is grounds for the revocation of the approval(s). I/we also give Planning Department staff and Planning Board Members the right to access the premises (both interior and exterior) at reasonable times and upon reasonable notice for the purpose of taking photographs and conducting other visual inspections.

7/14/16
Date


Signature of Land Owner (If authorized Trustee, Officer or Agent, so identify)



CITY OF NEW BEDFORD
JONATHAN F. MITCHELL, MAYOR

PLANNING BOARD

SUBMIT TO:
Planning Department
133 William Street
Room 303
New Bedford, MA 0274

SITE PLAN REVIEW APPLICATION FOR MODIFICATION

The undersigned, being the Applicant, seeks Site Plan Approval for property depicted on a plan entitled: "Riverside Landing Multi-Use Development at the Former Fairhaven Mills" by: Prime Engineering, Inc. dated: June 22, 2016

1. Application Information

Street Address: Riverside Landing, Coggeshall Street

Assessor's Map(s): 93 Lot(s) 292 + 261

Registry of Deeds Book: 10843 Page: 287

Zoning District: Mixed use and Hicks Logan Sawyer IPOD

Applicant's Name (printed): D.W. New Bedford LLC

Mailing Address: 1266 Furnace Brook Parkway Quincy MA 02169
(Street) (City) (State) (Zip)

Contact Information: 617 285-4780

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List all submitted materials (include document titles & volume numbers where applicable) below:

Refer to cover letter.

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July 14, 2016
Date

[Signature]
Signature of Applicant

City Hall • 133 William Street • Room 303 • New Bedford, MA 02740 • www.newbedford-ma.gov
PH: (508)979-1488 • FX: (508)979-1576

2. Review Applicability (Check All That Apply to Your Proposal)

Category

- ☐ Residential
- ☒ Commercial
- ☐ Industrial
- ☒ Mixed (Check all categories that apply)

Construction

- ☒ New Construction
- ☐ Expansion of Existing
- ☐ Conversion
- ☐ Rehabilitation

Scale

- ☐ < 2,000 gross sq feet
- ☒ > 2,000 gross sq feet
- ☐ 3 or more new residential units
- ☐ 1 or more new units in existing res. multi-unit
- ☐ Drive Thru Proposed
- ☐ Ground Sign Proposed
- ☐ Residential Driveway With > 1 curbcut

3. Zoning Classifications

Present Use of Premises: Vacant parcel

Proposed Use of Premises: Drive through restaurant

Zoning Relief Previously Granted (Variances, Special Permits, with Dates Granted):

Special Permit and Site Plan Review - summer 2013

4. Briefly Describe the Proposed Project:

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Assessors' Map 93 Lots 261 and 292 on Coggeshall Street. It is now proposed to substitute a 2,265

square foot drive through restaurant for the previously approved 1,722 square foot drive through bank.

(Case #15-13)

5. Please complete the following:

	Existing	Allowed/Required	Proposed
Lot Area (sq ft)			
Lot Width (ft)			
Number of Dwelling Units	N/A	IPOD	
Total Gross Floor Area (sq ft)			
Residential Gross Floor Area (sq ft)			
Non-Residential Gross Floor Area (sq ft)			
Building Height (ft)			
Front Setback (ft)			
Side Setback (ft)			
Side Setback (ft)			

Rear Setback (ft)			
Lot Coverage by Buildings (% of Lot Area)			
Permeable Open Space (% of Lot Area)			
Green Space (% of Lot Area)			
Off-Street Parking Spaces			
Long-Term Bicycle Parking Spaces			
Short-Term Bicycle Parking Spaces			
Loading Bays			

6. Please complete the following:

	Existing	Proposed
a) Number of customers per day:	0	200
b) Number of employees:	0	20
c) Hours of operation:	0	6 a.m. - 12 p.m.
d) Days of operation:	0	7 days/week
e) Hours of deliveries:	0	6 a.m. - 6 p.m.
f) Frequency of deliveries:	<input type="checkbox"/> Daily <input type="checkbox"/> Weekly <input type="checkbox"/> Monthly <input type="checkbox"/> Other: _____	

7. Planning Board Special Permits:



The applicant is also requesting a Special Permit from the Planning Board.

Specify the requested Special Permit(s) below, and set forth within attached Development Impact Statement how the request meets approval criteria listed in §5320 of the zoning code.

A modified Special Permit is also being requested.

8. ZBA Variances and Special Permits:

NOTICE: Checking below does not constitute application for a special permit or a variance. The applicant must also file the proper application form and fee with the Zoning Board of Appeals.



The applicant is also requesting a special permit from the ZBA:

N/A

Specify zoning code section & title



The applicant is also requesting a variance from the ZBA:

N/A

Specify zoning code section & title

9. OWNERSHIP VERIFICATION

This section is to be completed & signed by the property owner:

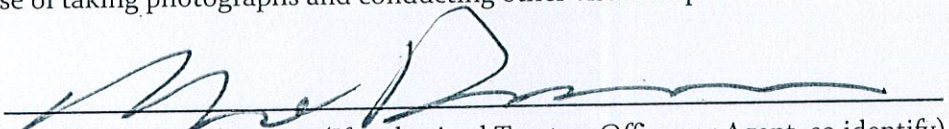
I hereby authorize the following Applicant: D.W. New Bedford LLC
at the following address: 1266 Furnace Brook Parkway, Quincy, MA 02169
to apply for: Modification to Site Plan Review
on premises located at: Riverside Landing, Coggeshall Street
in current ownership since: July 23, 2013
whose address is: Same as above
for which the record title stands in the name of: D.W. New Bedford LLC
whose address is: Same as above

by a deed duly recorded in the:
Registry of Deeds of County: Bristol Book: 10843 Page: 287

OR Registry District of the Land Court, Certificate No.: _____ Book: _____ Page: _____

I/we acknowledge that all information presented herein is true to the best of my/our knowledge. I/we further understand that any false information intentionally provided or omitted is grounds for the revocation of the approval(s). I/we also give Planning Department staff and Planning Board Members the right to access the premises (both interior and exterior) at reasonable times and upon reasonable notice for the purpose of taking photographs and conducting other visual inspections.

7/14/2014
Date


Signature of Land Owner (If authorized Trustee, Officer or Agent, so identify)

**IMPACT REPORT
ON
PROPOSED DRIVE THROUGH RESTAURANT
AT
COGGESHALL STREET
NEW BEDFORD, MA**

PREPARED FOR:

**D.W. NEW BEDFORD LLC
1266 FURNACE BROOK PARKWAY
QUINCY, MA 02169**

PREPARED BY:

**PRIME ENGINEERING, INC.
P.O. BOX 1088
LAKEVILLE, MASSACHUSETTS**

JULY 14, 2016

ATTACHMENT 5

Case 25-16
07/15/2016

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1.0 INTRODUCTION

In the summer of 2013, the New Bedford Planning board issued a Special Permit and Site Plan Review approval for a 1,722 square foot bank and for a 10,165 square foot urgent care facility building on a 2.6 acre parcel which is referenced as Assessors' Map 93 Lots 261 and 292 which is on Coggeshall Street. It is now proposed to substitute a 2,265 square foot drive through restaurant for the previously approved 1,722 square foot drive through bank.

The site falls in the Hicks-Logan-Sawyer Interim Planning Overlay District (IPOD), therefore, a modification of the special permit and site plan approval from the Planning Board is required. The IPOD standards, site plan review criteria and the special permit specific design requirements must be met. This document addresses how the site design meets all of these standards.

2.0 SITE HISTORY AND MASTER PLAN

In 2009, the developer had assembled most of the parcels that then comprised the former Fairhaven Mills on Coggeshall Street in New Bedford, Massachusetts. It then proposed to construct a multi-use development consisting of a mixture of retail, services, and offices in addition to a waterfront park. The total building area of the overall development was projected to be over 150,000 square feet, the exact area being dependent on the final mix of restaurants, retail, and office space that was to be constructed. A 97,340 square foot supermarket was constructed as Phase I. A 2,600 square foot Taco Bell restaurant has been completed, as well as an 11,967 square foot retail/service building and a 10,165 square foot urgent care center. This brings the total currently approved building area to 123,884 square feet. If this proposed restaurant is approved, the total approved building area will be 124,527 square feet.

The site had been a predominantly abandoned, former textile mill site at which the old mill buildings had been razed. Rubble fill was left in the basements of the razed mills that had basements. Some of the razed mills that had no basements were left as slabs partially covered with rubble fill. There were numerous piles of debris and demolition material throughout the site.

The site was devoid of any significant vegetation. A sparse, herbaceous growth had partially established itself in the abandoned rubble and there was a small patch of common reed (*Phragmites australis*) at the northeast corner of the site where historic dredged soil stockpiles were removed leaving a shallow depression. All in all, the property was a highly degraded site.

There were no unusual geological features on the site. Numerous test pits and borings have demonstrated that there is no near surface bedrock. The near surface soil was topsoil over fill and rubble generally underlain by medium sand. The water table is generally two feet below the surface with the general gradient from west to east toward the Acushnet River.

There are no known historic or archeological features on the site. There are no stone walls, significant mature trees, trails, or other features that should be preserved. Due to the fact that the site was generally barren, it did not provide suitable habitat for any wildlife. The area currently proposed to be developed is a gently sloping lawn.

A master plan was developed that introduced a sidewalk lined Veterans Memorial Way, running from the Route 195 ramps northerly to Sawyer Street with the understanding that it could be extended northerly as areas to the north were developed. An east/west sidewalk lined drive from Veterans Memorial Way to Mitchell Street provided a good vehicle and pedestrian access to the supermarket and to the other proposed buildings. The master plan included comprehensive landscape design for both function and aesthetics. The currently proposed building will have nine parking spaces for walk-in customers and will have a drive through order and pick up lane, as well as a bypass lane. The sidewalk system provides a direct connection to the roofed bus stop and to the Coggeshall Street sidewalks, as well as to the Veterans Memorial Way sidewalk system. Cross access, utility and parking easements with the abutting lots will make the facilities on the lots work together as if they were one lot.

3.0 COMPLIANCE WITH SPECIAL PERMIT CRITERIA

The following subsection presents how the proposed development complies with the criteria for Special Permit as presented in Section 5300 of the New Bedford Zoning Ordinance.

3.1 The Proposed Development Impact on the Physical Environment

The project will not have any negative impacts on the site's physical environment. The site currently is a lawn. Landscaped strips will be provided along abutting roads and driveways and along abutting properties. In addition, landscaping will be provided in front of and/or alongside the proposed building. The topography will be slightly modified so the parking lots can be constructed at a minimum slope of one percent in order to provide effective surface drainage and avoid localized puddling.

3.2 Surface Water and Subsurface Conditions

The storm system which has already been approved and constructed collects stormwater from a well maintained, landscaped parking lot that is swept on a regular basis. The stormwater passes through a Stormceptor brand stormwater treatment system that has been demonstrated to remove 80 percent of the total suspended solids. Thus, the development results in a stormwater system that provides better environmental benefits compared to the pre-existing conditions.

The groundwater was sampled through numerous monitoring wells and, though there were some very low level detections, none of the groundwater exhibited concentrations that exceeded the

reportable thresholds of the MCP. Nevertheless, the development improves the groundwater quality by raising the grade, so infiltrated water passes through a greater depth of filtering sand.

The on-site soils and groundwater conditions do not impose any limitations on site development. As stated previously, the development resulted in improved ground and surface water quality.

3.3 Circulation Systems

The development has been carefully designed to accommodate both vehicular traffic and pedestrian access. The main entrance drive from Coggeshall Street has been located as far as possible from the entrance/exit ramps from Interstate Route 195. A traffic signal at the Route 195 ramp intersection has greatly improved turning movements at that location. Lane striping from west of Belleville Avenue to the Coggeshall Street bridge provided left turn stacking lanes and through lanes.

The main entrance drive to Riverside Landing, Veterans Memorial Way, has been constructed as a tree lined boulevard to Sawyer Street. As part of the master plan, a complete traffic impact report had been prepared by Vanasse and Associates. This traffic report was based on the full build-out of the development consisting of 150,000 square feet, including restaurants, office, service, and retail use. The currently proposed improvements will bring the total building area to 124,527 square feet, thereby generating less traffic than previously projected.

The proposed development has and will provide physical improvements to pedestrian and vehicular facilities and the design, access, and safety of public spaces is consistent with the HLS District Master Plan.

3.4 Support Systems

The following subsections present a description of the systems that will support the proposed development.

3.4.1 Utilities

The roads and drives surrounding the site have adequate utilities and support systems. The site will be served by municipal water. Hydrant flow tests have confirmed that there is adequate volume and pressure in the existing mains to accommodate both the domestic and fire suppression systems. There is existing sewer service to the proposed building pad. The main has adequate capacity to accommodate the site by gravity.

3.4.2 Refuse Disposal

Refuse will be stored in screened trash enclosures located as shown on the plans. The trash will be collected and disposed in licensed facilities by commercial trash haulers.

3.4.3 Fire Protection

There is no proposed fuel storage on-site. The buildings will be heated by natural gas. The nearest fire station is Fire Station Number 8 at 1599 Acushnet Avenue which is located .64 miles from the site. There are no unusual characteristics of the proposed buildings that would present an obstacle to normal fire fighting procedures. The buildings will be accessible on all sides. The driveway intersections have been designed to accommodate the turning radii of the city's fire apparatus.

3.4.4 Recreation

The Master Development Plan includes provisions for a waterfront park to be designed and constructed as part of Phase IV. The walkway will be bordered by lawn and landscaped areas. Park benches and a gazebo will be provided.

3.4.5 School Impacts

The proposed development will have no impact on the school system since there are no proposed residential units on the site.

3.4.6 Phasing

The current proposal represents less than 2 percent of the overall development program. It is projected it will be completed in 6 months after obtaining all permits.

4.0 COMPLIANCE WITH HLS IPOD PRINCIPLES

The Hicks-Logan-Sawyer Interim Planning Overlay District has specific design policies, principles and goals as set forth in the HLS Master Plan. This section of the report presents how the proposed design complies with those goals.

4.1 Relationship To Open Space

PUBLIC

The highway ramp improvements, off-site sidewalk and roadway improvements and the on-site tree lined boulevard formed the primary access to the proposed waterfront park, the development's multi-use buildings and to future development projected for areas north of the subject site. It also provided improved access to the existing Riverfront Park that is located to the northwest of the subject site.

Specific attention has been given to pedestrian access by integrating the proposed waterfront walkway with the other on-site walkways thereby providing excellent north-south and east-west pedestrian paths throughout the site.

Landscaped strips will be constructed along the site's boundaries, thereby framing the site and the public spaces. The street edge will be created along the sidewalks by a continuous wide landscaped strip. Landscaping will also be provided along the sides of the proposed building. The design follows and retains the basic city block system. The site's overall development plan includes shared parking and shared driveways.

4.2 Historic Context

An extensive assessment of the viability of preserving the mill buildings was conducted during the permitting of Phase I. It was determined that such preservation was infeasible.

4.3 Relationship To The Waterfront

The proposed development Master Plan integrates the city's proposed waterfront walkway with a proposed on-site waterfront park. Both of the above are being integrated with the proposed development's pedestrian walkway system.

The views from the site to the water were greatly enhanced by removal of the dilapidated buildings and piles of debris. Phase IV proposed multi-use building will be near the waterfront. It will provide active ground level uses such as restaurants and retail destinations along the waterfront.

4.4 Environmental Goals

The building and parking lot lighting are being designed to be energy efficient. They will meet evolving environmental efficiency standards to the extent practicable. The development will convert the site from a vacant site to a nicely landscaped functional facility.

4.5 Economic Development Goals

The proposed development will promote economic development by creating construction jobs and permanent jobs in the HLS District. Upon full build out, there will be a mixture of restaurant, retail, and service jobs.

4.6 Master Plan Approach

The current proposal is for the construction of the building that will house a drive through restaurant, however, this is the third phase of a Master Development Plan. The Master Development Plan is still evolving, but, by providing the interim Master Plan, driveways, walkways, utilities, and other requisite infrastructure elements were programmed in anticipation of the full development.

5.0 COMPLIANCE WITH SITE PLAN REVIEW STANDARDS

Sections 5470 of the New Bedford Zoning Ordinance prescribes specific Site Plan Review design requirements. This section presents how each of these design standards are being met:

5.1 Earthworks

The proposed grading for the site requires importing approximately 100 cubic yards of gravel required to be placed immediately beneath the slab and pavement and approximately 100 cubic yards of ordinary earth for beneath the gravel layer.

5.2 Pedestrian and Vehicular Safety

The site will be accessed by a proposed driveway and ample parking will be provided. Visitors will park near the building's main entrances, where a sidewalk will provide safe pedestrian access to the building.

5.3 Scenic Views

The proposed building will not obstruct any scenic views from publicly accessible locations.

5.4 Visual Intrusion

The parking, storage areas and outdoor service areas have been laid out so as not to be visually intrusive to any public way and residentially used or zoned areas. All trash storage areas will be screened by enclosures. Landscaping will be provided throughout the site.

5.5 Off-Site Glare

The proposed parking has been designed so as to prevent glare. The parking lot lighting will be dark sky compliant and will shine downward in order to prevent off-site glare.

5.6 Character, Material and Scale of Building

The proposed building is similar in character, material and scale to the other buildings at Riverside Landing that have already been approved by the Planning Board.

5.7 Water Contamination

All materials that have a potential for causing higher stormwater pollution loads will be stored indoors. The stormwater will be passed through a Stormceptor brand stormwater treatment system, thereby assuring proper water quality.

5.8 Zoning Compliance

Even though dimensional standards do not apply in the Hicks-Logan-Sawyer IPOD, except for the number of parking spaces, the site design complies with all the relevant zoning ordinance provisions in the mixed-use business district.

5.9 Public Ways

The proposed use will not damage Coggeshall Street, Veterans Memorial Drive or any other public way.

5.10 Internal Vehicle Circulation

The proposed driveways have been layed out in order to promote orderly and reasonable internal circulation within the site so as to protect public safety.

5.11 Shared Parking

The following is a tabulation of all of the parking spaces at Riverside Landing, including the Market Basket food store:

Use	Criteria	Parking Spaces Normally Required	Provided Spaces
97,300 SF - Food Store	1 sp/200 SF (first 20,000 SF) 1 sp/400 SF (over 20,000 SF)	294	441
Water Front Lot	Currently Vacant	0	146
✓ 10,200 SF - Urgent Care Clinic	1 sp/200 SF	51	63
✓ 2,265 SF - Restaurant	1 sp/100 SF plus 1 per employee	28	9
✓ 2,600 SF - Fast Food Restaurant	1 sp/100 SF + 1 sp/Employee	31	21
12,000 SF - Multi-Use Building	1 sp/200 SF	60	55
Totals:		464	735

The criteria within the IPOD district is that the number of parking spaces normally required outside the IPOD be treated as a maximum number with the goal being the multiple uses share parking. That is being accomplished in this case with all tenants on the multi-use parcel having shared access and parking agreements in their leases.

There is not an excess of parking spaces for the Market Basket food store. The lot is often filled to capacity. Once the waterfront lot is developed, many of the 146 parking spaces which are currently on that lot will be displaced by the proposed building. In addition, the proposed building will generate a requirement for more parking spaces.

The developers parcel, which include the urgent care facility, Taco Bell, and the multi-use building, and the proposed restaurant have the following parking schedule:

Use	Criteria	Parking Spaces Normally Required	Provided Spaces
✓ 10,200 SF - Urgent Care Clinic	1 sp / 200 SF	51	61
✓ 2,265 SF - Restaurant	1 sp / 100 SF plus 1 per employee	28	10
✓ 2,600 SF - Fast Food Restaurant (Taco Bell)	1 sp / 100 SF + 1 sp/Employee	31	21
✓ 12,000 SF - Multi-Use Building	1 sp / 200 SF	60	55
Totals:		170	147

6.0 CONCLUSION

The proposed facility will convert a vacant parcel of land to an effective restaurant, thereby bringing jobs and tax revenue to New Bedford while not imposing any significant demands on city services.

RE: Lot 2, Coggeshall Street
New Bedford, MA 02746

**MASSACHUSETTS QUITCLAIM DEED
BY LIMITED LIABILITY COMPANY**

COGGESHALL-NEW BEDFORD LLC, a Massachusetts Limited Liability Company, of Quincy, Massachusetts, and HIGHWAYVIEW, LLC (a/k/a HIGHWAYVIEW LLC), a Massachusetts Limited Liability Company, of Quincy, Massachusetts,

for consideration paid, and in full consideration of ONE and 00/100 (\$1.00) DOLLAR

grants to DW NEW BEDFORD, LLC, a Massachusetts Limited Liability Company, having a principal office of 1266 Furnace Brook Parkway, Suite 310, Quincy, Massachusetts 02169,

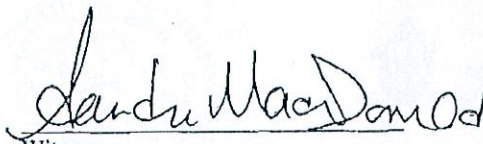
with Quitclaim Covenants

the land with any buildings thereon located in New Bedford, Massachusetts, bounded and described as follows:

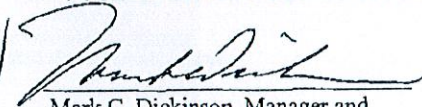
**SEE EXHIBIT "A" ATTACHED HERETO
AND
INCORPORATED HEREIN BY REFERENCE**


Subject to the fiscal year 2014 Real Estate Taxes which, the grantee hereby assumes and agrees to pay.

WITNESS my hand and seal this 17th day of July 2013.

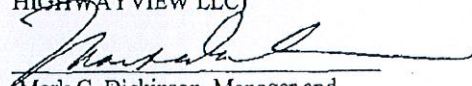

Witness

COGGESHALL-NEW BEDFORD LLC


Mark C. Dickinson, Manager and
Authorized Signatory


Witness

HIGHWAYVIEW, LLC(a/k/a
HIGHWAYVIEW LLC)


Mark C. Dickinson, Manager and
Authorized Signatory

ATTACHMENT 6

EXHIBIT "A"

"Lot 2" on that certain plan entitled "Approval Not Required Plan in New Bedford, Massachusetts" prepared for Highwayview, LLC, dated June 12, 2013, signed on June 28, 2013, and recorded in the Bristol County (S.D.) Registry of Deeds in Plan Book 170, Page 166.

For Title into Highwayview, LLC see deed dated February 9, 2009, and recorded in the Bristol County (S.D.) Registry of Deeds in Book 9259, Page 273.

For Title see deed dated December 20, 2012, and recorded in the Bristol County (S.D.) Registry of Deeds in Book 10621, Page 153.

170-66

REVISIONS

NO.	DATE	REVISION
1	07/15/2016	ISSUED FOR PERMIT

DATE: 07/15/2016
PREPARED BY: [Signature]
DRAWN BY: [Signature]

TABLE A

REVISION	DATE	DESCRIPTION
1	07/15/2016	ISSUED FOR PERMIT

PLAN REFERENCES

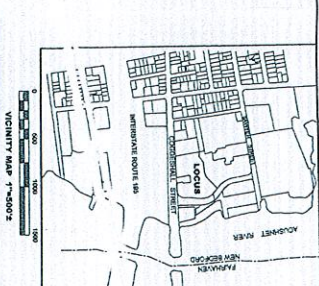
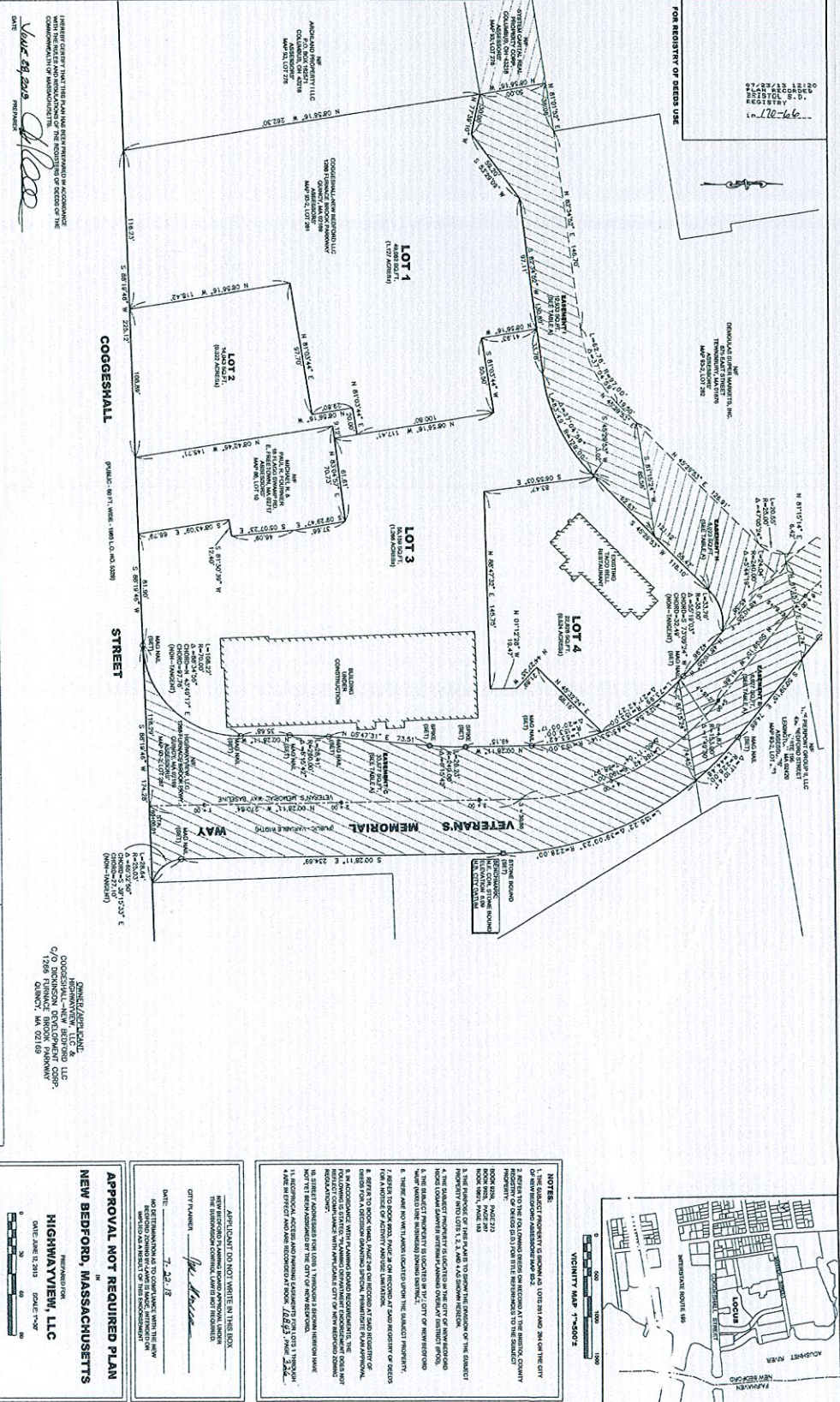
REVISION	DATE	DESCRIPTION
1	07/15/2016	ISSUED FOR PERMIT

LEGEND

SYMBOL	DESCRIPTION
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DATE: 07/15/2016
PREPARED BY: [Signature]
DRAWN BY: [Signature]

PRIME ENGINEERING
1000 WASHINGTON STREET
SUITE 200
BOSTON, MA 02108
TEL: 617-552-1234
FAX: 617-552-1235
WWW.PRIMEENGINEERING.COM



APPROVAL NOT REQUIRED PLAN
NEW BEDFORD, MASSACHUSETTS
HIGHWAYVIEW, LLC
DATE: 07/15/2016
SCALE: 1\"/>

APPROVAL NOT REQUIRED PLAN
NEW BEDFORD, MASSACHUSETTS
HIGHWAYVIEW, LLC
DATE: 07/15/2016
SCALE: 1\"/>

APPROVAL NOT REQUIRED PLAN
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DATE: 07/15/2016
SCALE: 1\"/>

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SCALE: 1\"/>

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SCALE: 1\"/>

APPROVAL NOT REQUIRED PLAN
NEW BEDFORD, MASSACHUSETTS
HIGHWAYVIEW, LLC
DATE: 07/15/2016
SCALE: 1\"/>

Site Photos
Riverside Landing – Coggeshall Street



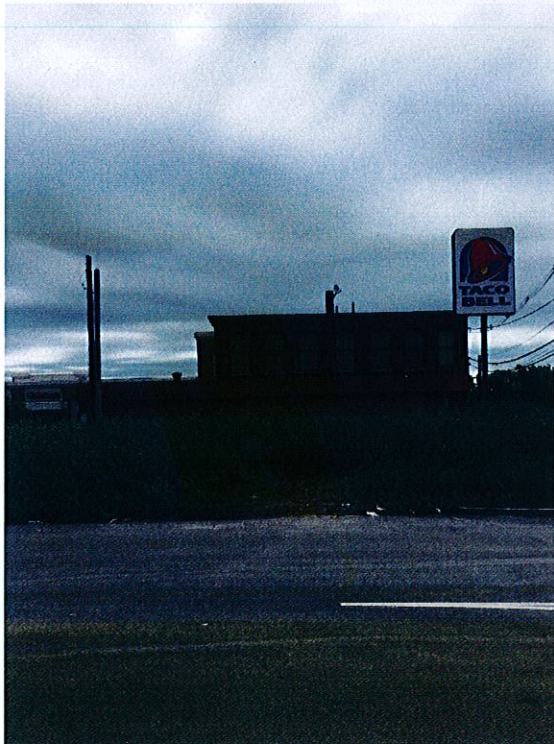
Locus looking northeast



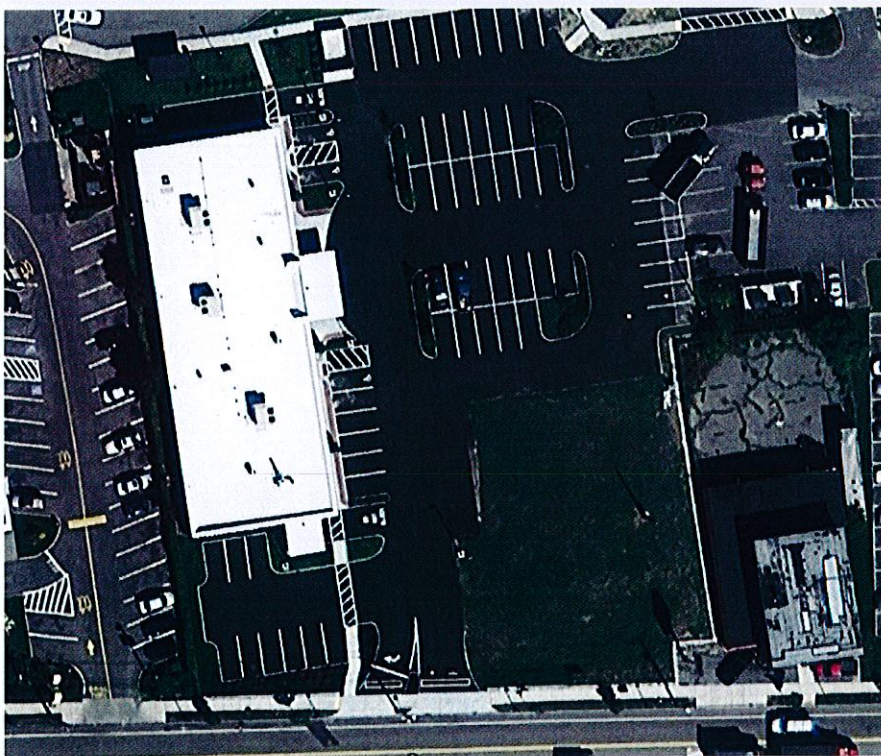
Locus looking southwest

Case 25-16
07/15/2016

Site Photos
Riverside Landing – Coggeshall Street



Locus looking east



Aerial photo of locus

MARKET BASKET SUPERMARKET

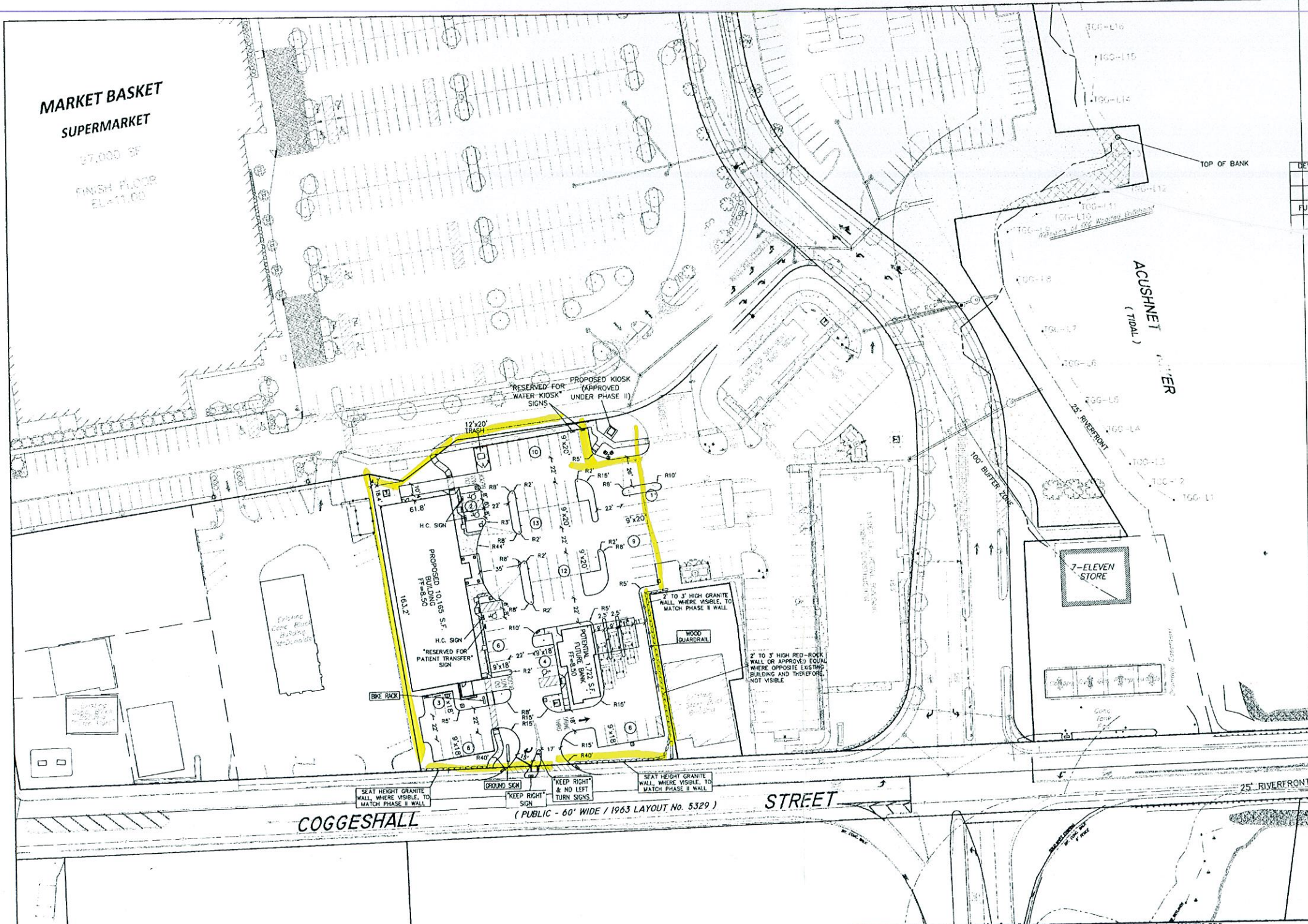
27,000 SF
FINISH FLOOR
EL+11.00

PARKING SUMMARY

DEVELOPMENT	GROSS AREA	PARKING SPACES REQ'D (MIN-POD)	PROPOSED	PROPOSED RATIO
KIOSK	30 S.F.	2	2	N/A
MEDICAL	10,165 S.F.	51	60	5.9/1,000 S.F.
FUTURE BANK	1,722 S.F.	9	10	5.8/1,000 S.F.
TOTAL	11,917 S.F.	62	72	6.0/1,000 S.F.

NOTES

1. ALL CURBING RADII SHALL BE 3' UNLESS OTHERWISE NOTED.
2. ALL CURBING TO BE 5"-WIDE VERTICAL GRANITE (SEE DETAIL) UNLESS OTHERWISE NOTED. AT THE DROP OFF AREA IN FRONT OF THE URGENT CARE BUILDING, WHERE THE PAVEMENT WILL BE FLUSH WITH THE SIDEWALK, THERE WILL BE NO CURBING. ALL OTHER EDGES OF PAVEMENT WILL BE BORDERED BY VERTICAL GRANITE CURBING.
3. WHEEL CHAIR RAMPS TO BE PROVIDED WHERE SIDEWALK TERMINATES AND AT ALL CROSS-WALKS. RAMPS TO HAVE DETECTABLE WARNING STRIP AND COMPLY WITH ADA STANDARDS.



APPROVED BY THE CITY OF NEW BEDFORD PLANNING BOARD

CITY OF NEW BEDFORD PLANNING BOARD

APPROVED _____ ENDORSED _____



DRAWING TITLE

LAYOUT PLAN

PROJECT

RIVERSIDE LANDING - PHASE III
NEW BEDFORD, MASSACHUSETTS

CLIENT

HIGHWAYVIEW LLC
NEW BEDFORD, MASSACHUSETTS

CIVIL ENGINEERING
LAND SURVEYING
ENVIRONMENTAL
ASSESSMENT

PRIME ENGINEERING
P.O. BOX 1288
350 BEDFORD ST.
LAKEVILLE, MA 02347
TEL: 508.947.0050
FAX: 508.947.2004

SCALE:

1" = 40'

DATE:

JUN 20, 2013

DRAWN BY:

EKW

DESIGNED BY:

EKW

CHECKED BY:

RJR

APPROVED BY:

RJR

SHEET NO.

3

PROJECT NO.

1304801

Case 25-16
07/15/2016

ATTACHMENT 9

RIVERSIDE LANDING - MULTI-USE DEVELOPMENT AT THE FORMER FAIRHAVEN MILLS SITE

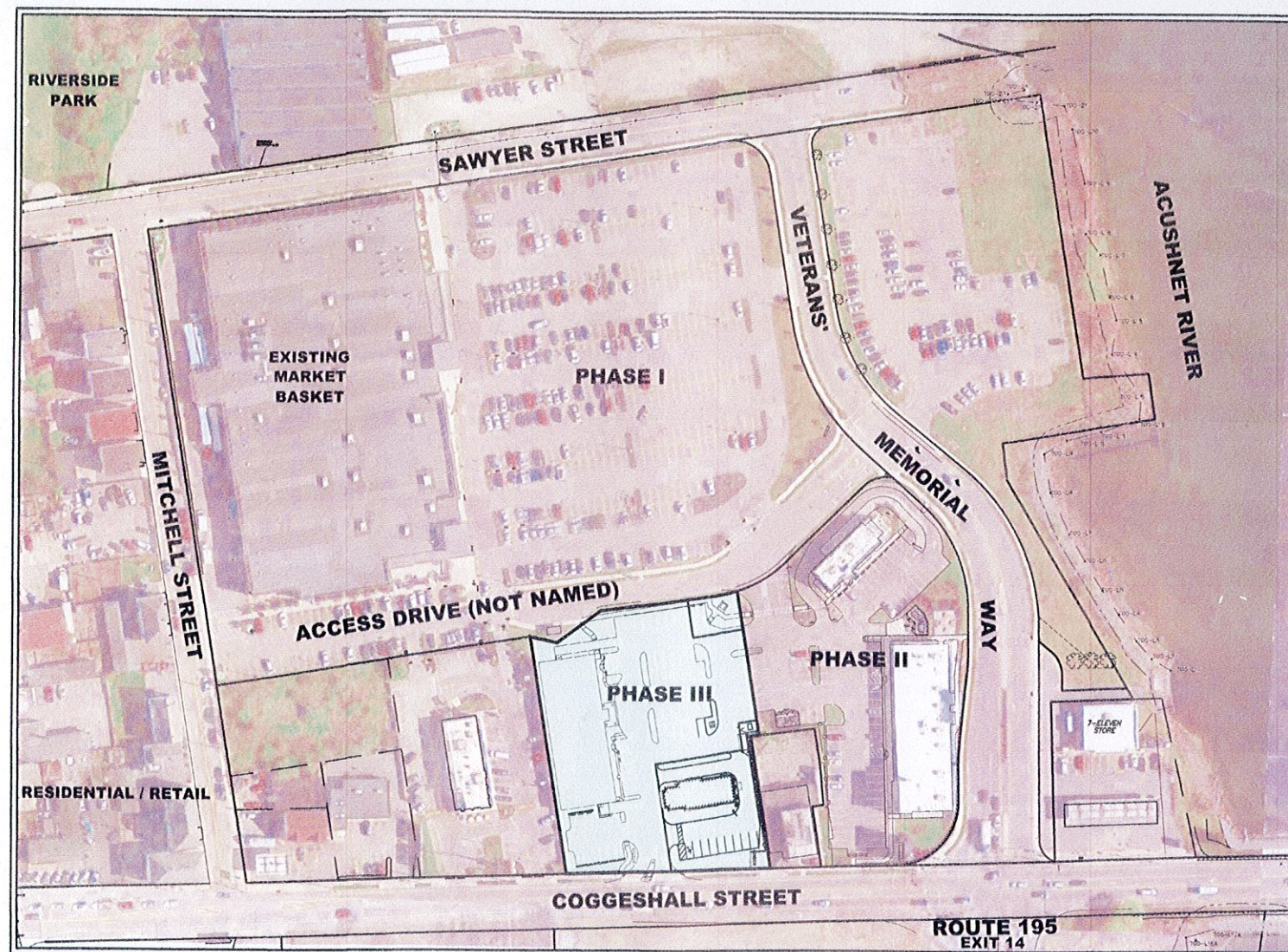
COGGESHALL STREET NEW BEDFORD, MASSACHUSETTS PHASE III MODIFICATION SITE PLANS ISSUED FOR SPECIAL PERMIT MODIFICATION

SCHEDULE OF DRAWINGS

SHEET NUMBER	PLAN TITLE
1	TITLE SHEET
2	EXISTING CONDITIONS PLAN
3	SITE LAYOUT
4	GRADING & DRAINAGE PLAN
5	UTILITIES PLAN
6	LIGHTING PLAN
7	LANDSCAPE PLAN
8	EROSION CONTROL PLAN
9	DETAILS 1
10	DETAILS 2
11	DETAILS 3

OWNER, DEVELOPER, & APPLICANT:

DW NEW BEDFORD LLC
1266 FURNACE BROOK PARKWAY
QUINCY, MA 02169



SITE LOCUS IS REFERENCED AS NEW BEDFORD ASSESSOR'S
MAP 93-2 LOTS 264 & 261. SEE BOOK 10843 / PAGE 287 RECORDED
IN THE BRISTOL COUNTY REGISTRY OF DEEDS, IN NEW BEDFORD.

THE SITE IS LOCATED IN THE HICKS LOGAN SAWYER INTERIM
PLANNING OVERLAY DISTRICT (IPOD).

NEIGHBORHOOD LOCUS PLAN

SCALE: 1"=80'

JUNE 13, 2016

AUGUST 4, 2016

DEVELOPMENT TEAM

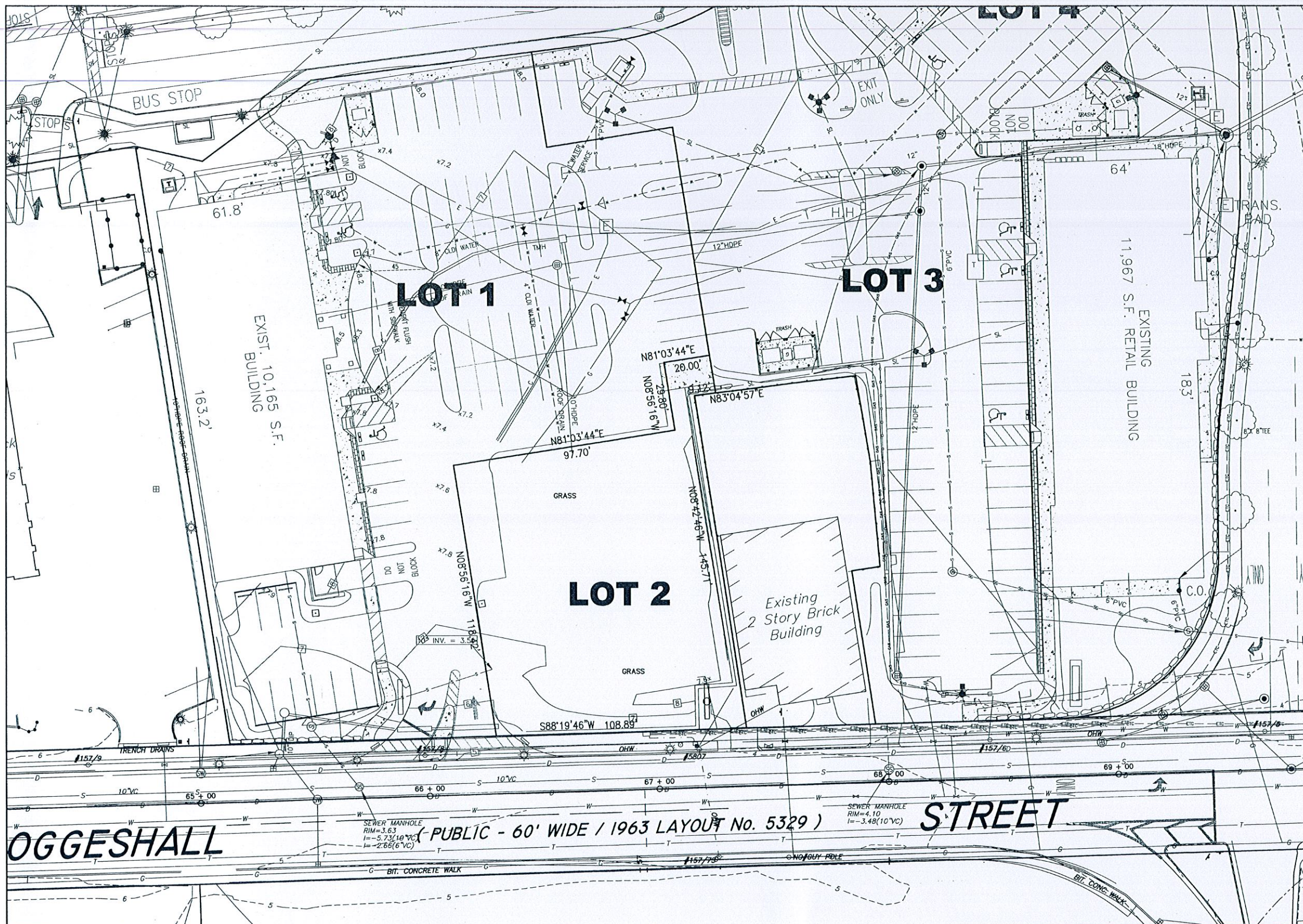
FUNCTION	TEAM MEMBER
DEVELOPER	COGGESHALL-NEW BEDFORD, LLC & HIGHWAYVIEW, LLC
SITE CONTRACTOR	D.W. WHITE
ARCHITECT	MICHAEL SUDANO ARCHITECT P.C.
LANDSCAPE ARCHITECT	RADNER DESIGN ASSOCIATES
SITE DESIGN ENGINEER	PRIME ENGINEERING, INC.

PREPARED BY: DEPARTMENT



CIVIL ENGINEERING—LAND SURVEYING—ENVIRONMENTAL ASSESSMENT
P.O. BOX 1088, 350 BEDFORD STREET, LAKEVILLE, MA 02347
TEL: 508.947.0050 FAX: 508.947.2004

SHEET 1

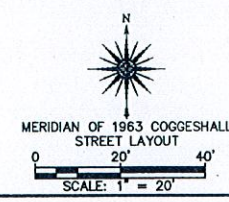


- NOTES:**
1. HORIZONTAL DATUM IS MASSACHUSETTS STATE PLANE-COORDINATION SYSTEM (NAD 83)
 2. VERTICAL DATUM IS CITY OF NEW BEDFORD DATUM.
 3. LOT 2 CONSISTS OF 14,043 S.F. AND HAS 108.9 FT. OF FRONTAGE ALONG COGGESHALL STREET.
 4. BENCH MARK IS SEWER MANHOLE IN COGGESHALL STREET, ELEVATION 3.63, CITY OF NEW BEDFORD DATUM.
 5. THERE ARE NO NHESP HABITATS ON OR NEAR THE SITE
 6. THERE ARE NO WETLANDS OR FLOODPLAIN ON THE SITE.

APPROVED BY THE CITY OF NEW BEDFORD PLANNING BOARD

CITY OF NEW BEDFORD PLANNING BOARD

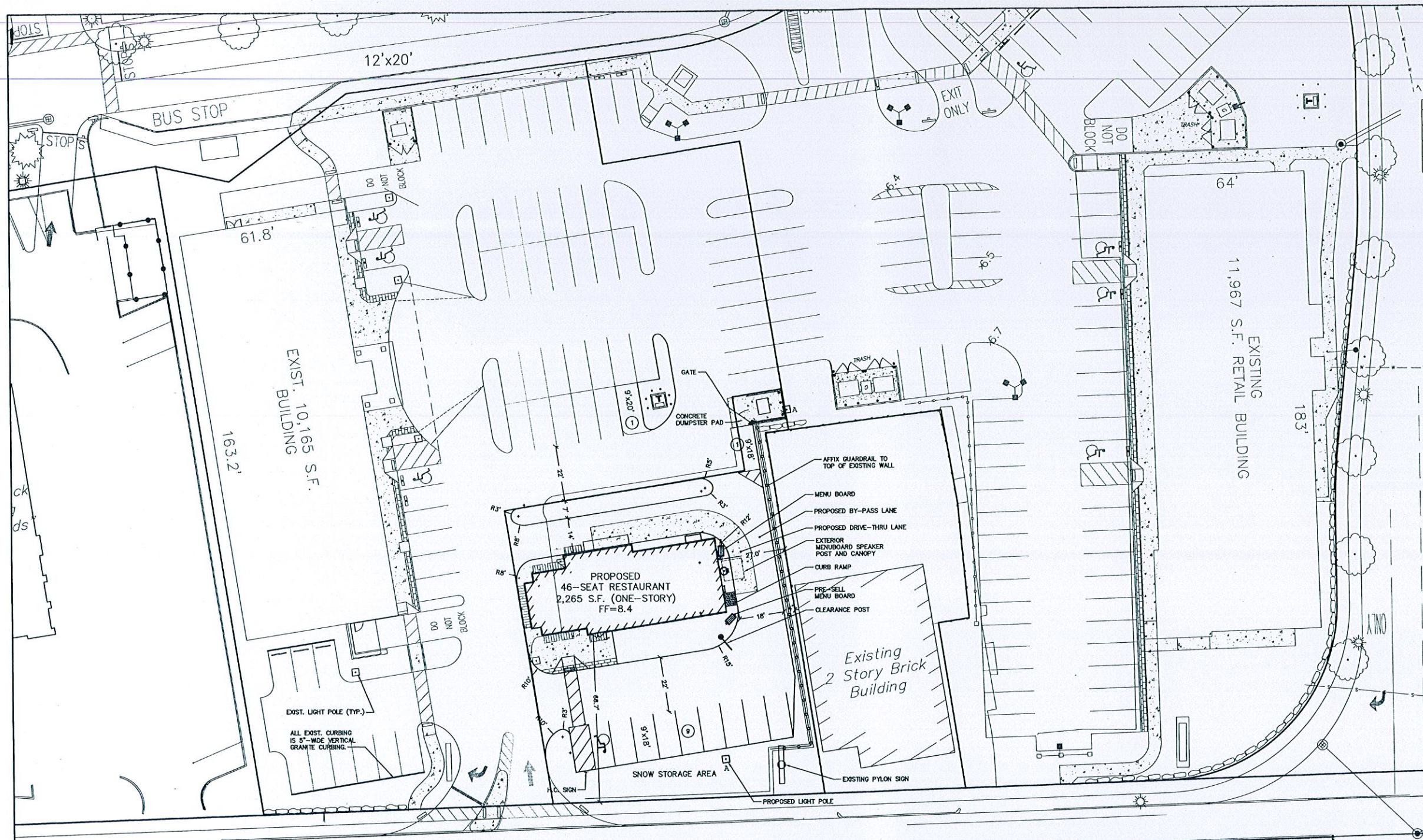
APPROVED _____ ENDORSED _____



STREET

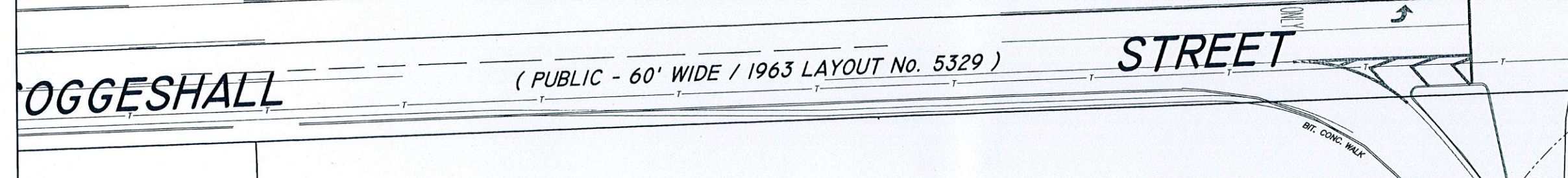
PLANNING
AUG 05 2015
DEPARTMENT

DRAWING TITLE EXISTING CONDITIONS PLAN				SCALE: 1" = 20'	
PROJECT RIVERSIDE LANDING - PHASE III MOD. NEW BEDFORD, MASSACHUSETTS				DATE: JUL 13, 2016	
CLIENT HIGHWAYVIEW LLC NEW BEDFORD, MASSACHUSETTS				DRAWN BY: EKW	
DESIGNED BY: EKW				CHECKED BY: RJR	
CIVIL ENGINEERING LAND SURVEYING ENVIRONMENTAL ASSESSMENT				APPROVED BY: RJR	
REV.	DATE	DESCRIPTION	BY	APP.	SHEET NO. 2
1	8/4/16	INITIAL PLANNING DEPT. REVIEW	EKW	RJR	PROJECT NO. 1304801



PARKING SUMMARY				
DEVELOPMENT	GROSS AREA	REQ'D (NON-IMP)	PROPOSED	PROPOSED RATIO
KIOSK	30 S.F.	2	2	N/A
MEDICAL	10,165 S.F.	51	59	5.8/1,000 S.F.
RESTAURANT	2,265 S.F.	20	10	4.4/1,000 S.F.
TOTAL	12,430 S.F.	73	71	5.7/1,000 S.F.

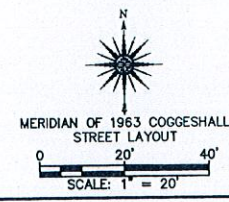
- NOTES:
1. ALL CURBING RADII SHALL BE 3' UNLESS OTHERWISE NOTED.
 2. ALL CURBING TO BE 5"-WIDE VERTICAL GRANITE (SEE DETAIL) UNLESS OTHERWISE NOTED.
 3. WHEEL CHAIR RAMPS TO BE PROVIDED WHERE SIDEWALK TERMINATES AND AT ALL CROSS-WALKS. RAMPS TO HAVE DETECTABLE WARNING STRIP AND COMPLY WITH ADA STANDARDS.
 4. ANY MINOR MODIFICATIONS (AS DETERMINED BY THE CITY ENGINEER AND/OR CITY PLANNER) TO THE INFORMATION SHOWN ON THE APPROVED SITE PLANS SHALL BE SUBMITTED TO THE CITY ENGINEER AND PLANNER AS A MINOR PLAN REVISION FOR APPROVAL PRIOR TO THE WORK BEING PERFORMED.
 5. ANY WORK AND MATERIAL WITHIN THE CITY RIGHT-OF-WAY SHALL CONFORM TO THE CITY OF NEW BEDFORD REQUIREMENTS.
 6. ALL HANDICAP PARKING, RAMPS, AND ACCESS SHALL CONFORM TO AAB & MAAB REQUIREMENTS.
 7. ALL EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO CONSTRUCTION. EROSION CONTROL SHALL CONFORM TO THE CITY OF NEW BEDFORD CONSERVATION COMMISSION REQUIREMENTS AS STATED IN THE ORDER OF CONDITIONS (REFER TO EROSION CONTROL PLAN).
 8. ALL PAVEMENT MARKINGS AND SIGNS SHALL CONFORM TO MUTCD REQUIREMENTS.



PLANNING
AUG 05 2016
DEPARTMENT

APPROVED BY THE CITY OF NEW BEDFORD PLANNING BOARD

CITY OF NEW BEDFORD PLANNING BOARD
APPROVED _____ ENDORSED _____

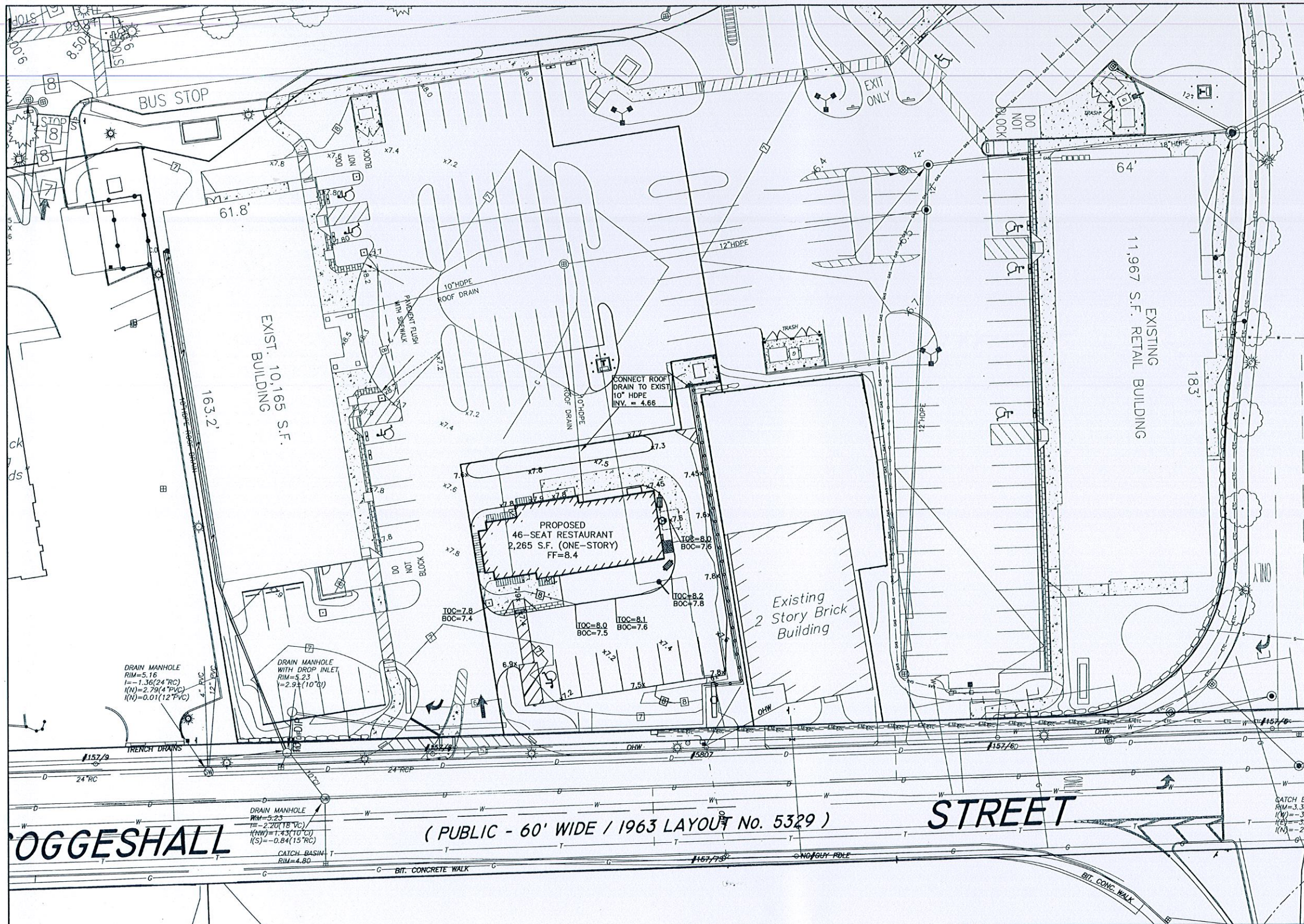


DRAWING TITLE LAYOUT PLAN		SCALE: 1" = 20'
PROJECT RIVERSIDE LANDING - PHASE III MOD. NEW BEDFORD, MASSACHUSETTS		DATE: JUN 29, 2016
CLIENT HIGHWAYVIEW LLC NEW BEDFORD, MASSACHUSETTS		DRAWN BY: EKW
DESIGNED BY: EKW		CHECKED BY: RJR
APPROVED BY: RJR		PROJECT NO. 1304801
SHEET NO. 3		

1 8/4/18 INITIAL PLANNING DEPT. REVIEW EKW RJR
REV. DATE DESCRIPTION BY APP.

CIVIL ENGINEERING
LAND SURVEYING
ENVIRONMENTAL ASSESSMENT

PRIME ENGINEERING
P.O. BOX 1088
350 BEDFORD ST.
LAKEVILLE, MA 02347
TEL: 508.947.0050
FAX: 508.947.2004



- NOTES:**
- 1) A U.S. EPA NPDES PERMIT MUST BE OBTAINED BY THE OWNER AND CONTRACTOR PRIOR TO START OF WORK.
 - 2) THE STORMWATER SYSTEM SHALL BE MAINTAINED IN ACCORDANCE WITH THE APPROVED LONG TERM MAINTENANCE PROGRAM, AT THE APPROVED SCHEDULE.
 - 3) THE CONTRACTOR SHALL NOTIFY THE ENGINEERING DIVISION 24 HOURS IN ADVANCE OF ANY SIDEWALK, ROADWAY, OR UTILITY WORK TO ENSURE THAT THE WORK IS IN COMPLIANCE WITH A.D.A. AND CITY OF NEW BEDFORD STANDARDS.
 - 4) PERMITS FOR WATER, SEWER, SIDEWALKS, AND DRIVEWAYS MUST BE OBTAINED FROM THE DEPARTMENT OF PUBLIC INFRASTRUCTURE, ENGINEERING DIVISION.
 - 5) A PRE-CONSTRUCTION MEETING MUST BE SCHEDULED WITH THE DEPARTMENT OF PUBLIC INFRASTRUCTURE AND THE CONSERVATION AGENT PRIOR TO START OF CONSTRUCTION.
 - 6) ALL DRAINAGE PIPING TO BE 12 INCH ADS N-12 HDPE UNLESS OTHERWISE NOTED. ALL PIPING WITHIN ACCEPTED ROADWAYS SHALL BE RCP.
 - 7) CONTRACTOR TO CONFIRM ELEVATIONS OF EXISTING UTILITIES WHERE PROPOSED UTILITIES TIE-IN.

OGGESHALL

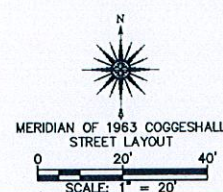
(PUBLIC - 60' WIDE / 1963 LAYOUT No. 5329)

STREET

APPROVED BY THE CITY OF NEW BEDFORD PLANNING BOARD

CITY OF NEW BEDFORD PLANNING BOARD

APPROVED _____ ENDORSED _____



LEGEND

- PROPOSED CONTOUR
- x7.8 - PROPOSED SPOT GRADE
- 5- - EXISTING CONTOUR
- 6.0 - EXISTING SPOT GRADE

REV.	DATE	DESCRIPTION	BY	APP.
1	8/4/16	INITIAL PLANNING DEPT. REVIEW	EKW	RJR

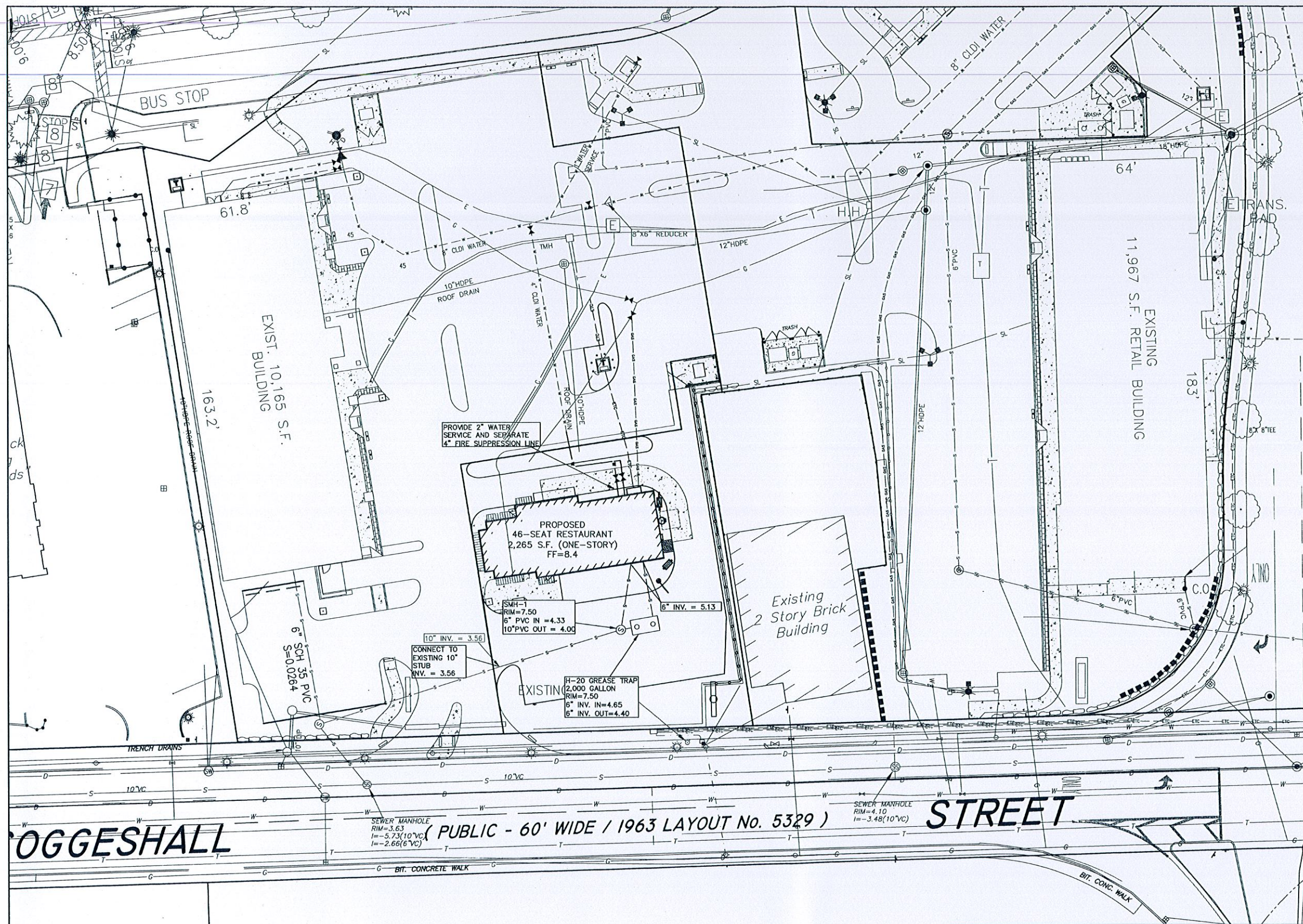
DRAWING TITLE	GRADING AND DRAINAGE PLAN
PROJECT	RIVERSIDE LANDING - PHASE III MOD. NEW BEDFORD, MASSACHUSETTS
CLIENT	HIGHWAYVIEW LLC NEW BEDFORD, MASSACHUSETTS
DESIGNED BY	EKW
CHECKED BY	RJR
APPROVED BY	RJR



P.O. BOX 1088
350 BEDFORD ST.
LAKEVILLE, MA 02347
TEL: 508.947.0050
FAX: 508.947.2004

SCALE	1" = 20'
DATE	JUL 13, 2016
DRAWN BY	EKW
DESIGNED BY	EKW
CHECKED BY	RJR
APPROVED BY	RJR
SHEET NO.	4
PROJECT NO.	1304801





ALL UTILITY WORK SHALL BE DONE IN CONFORMANCE WITH NEW BEDFORD DEPARTMENT OF PUBLIC INFRASTRUCTURE CONSTRUCTION STANDARDS AND SPECIFICATIONS DATED MARCH, 2012

NOTES:

- 1) THE CONTRACTOR SHALL NOTIFY THE ENGINEERING DIVISION 24 HOURS IN ADVANCE OF ANY SIDEWALK, ROADWAY, OR UTILITY WORK TO ENSURE THAT THE WORK IS IN COMPLIANCE WITH A.D.A. AND CITY OF NEW BEDFORD STANDARDS.
- 2) PERMITS FOR WATER, SEWER, SIDEWALKS, AND DRIVEWAYS MUST BE OBTAINED FROM THE DEPARTMENT OF PUBLIC INFRASTRUCTURE, ENGINEERING DIVISION.
- 3) CONTRACTOR TO CONFIRM ELEVATIONS OF EXISTING UTILITIES WHERE PROPOSED UTILITIES TIE-IN.
- 4) THE CONTRACTOR SHALL OBTAIN A STREET DISTURBANCE & OBSTRUCTION PERMIT PRIOR TO ANY CONSTRUCTION WITHIN THE RIGHT-OF-WAY
- 5) ALL WATER AND SEWER MATERIAL AND CONSTRUCTION SHALL CONFORM TO THE CITY OF NEW BEDFORD REQUIREMENTS
- 6) ALL WATER AND SEWER CONSTRUCTION SHALL BE INSPECTED BY THE CITY OF NEW BEDFORD BEFORE BEING BACKFILLED.
- 7) THE CITY SHALL BE NOTIFIED AT LEAST 24-HOURS PRIOR TO THE REQUIRED INSPECTIONS.

PLANNING
AUG 05 2016
DEPARTMENT

APPROVED BY THE CITY OF NEW BEDFORD PLANNING BOARD

CITY OF NEW BEDFORD PLANNING BOARD

APPROVED _____ ENDORSED _____

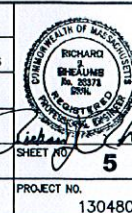


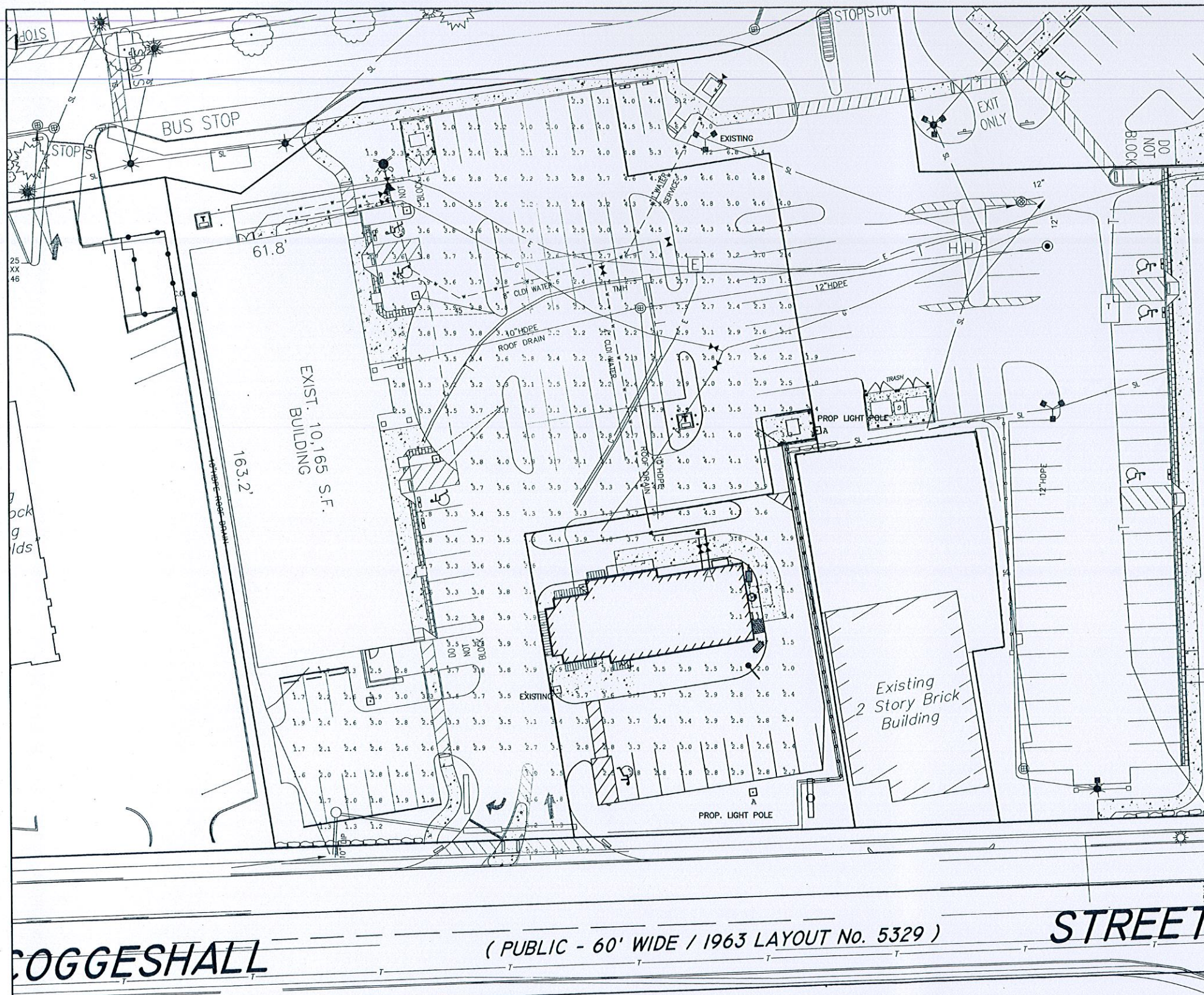
MERIDIAN OF 1963 COGGESHALL STREET LAYOUT



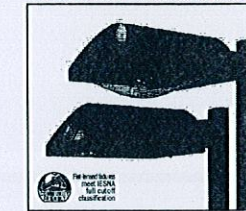
DRAWING TITLE				SCALE:	
UTILITIES PLAN				1" = 20'	
PROJECT				DATE:	
RIVERSIDE LANDING -- PHASE III MOD.				JUL 13, 2016	
CLIENT				DRAWN BY:	
HIGHWAYVIEW LLC				EKW	
				DESIGNED BY:	
NEW BEDFORD, MASSACHUSETTS				EKW	
				CHECKED BY:	
CIVIL ENGINEERING				RJR	
LAND SURVEYING				APPROVED BY:	
ENVIRONMENTAL ASSESSMENT				RJR	
				PROJECT NO.	
				1304801	

P.O. BOX 1088
350 BEDFORD ST.
LAKEVILLE, MA 02347
TEL: 508.947.0050
FAX: 508.947.2004

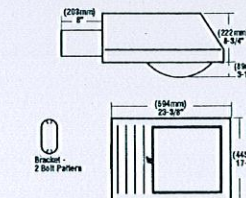




CHALLENGER® II MEDIUM (Various reflectors are protected by U.S. Patent No. 6,454,378.)



DIMENSIONS



Beam Angle	F	CT
10°	1.4	1.6
15°	2.8	3.2
20°	4.0	4.5
25°	5.1	5.7
30°	6.1	6.8

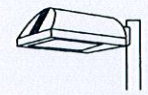
Catalog Number	Est. Weight (lbs.)	Length (inches)	Width (inches)	Height (inches)
CH2HM-10	19 / 42	606 / 27.5	556 / 22	432 / 17
CH2HM-15	19 / 42	606 / 27.5	556 / 22	432 / 17

- HOUSING** - Recessed, rectangular-shaped, aluminum housing with stainless steel or electro-zinc plated steel mounting hardware.
- DOOR FRAME** - Aluminum with two stainless steel captive fasteners for easy access into the fixture. A one-piece extruded EPDM gasket seals the door frame against the housing.
- LENS/GASKET** - Available with a contoured clear impact-resistant glass lens for vertical lamp optics. A tempered flat clear glass lens or contoured clear impact-resistant glass lens is offered for horizontal lamp optics. The lens is sealed to the door frame with EPDM gasketing.
- SOCKETS** - Porcelain mogul-base sockets. All sockets are factory prewired. An optional disconnect plug for the ballast is available. All sockets are pulse-rated.
- LIGHT SOURCES** - Pulse-Start Metal Halide, Pulse-Start Metal Halide Reduced Envelope, Ceramic Metal Halide or High Pressure Sodium. Clear lamp is supplied as standard.
- BALLASTS** - High power factor ballast, Pulse-Start Metal Halide, Metal Halide, and High Pressure Sodium fixtures feature a CWA type ballast. All ballasts are designed for -20° F operation.
- REFLECTORS/DISTRIBUTION PATTERNS** - Vertical oriented systems include: Type V (S), and Perimeter Forward Throw (FP). Horizontal oriented systems include: Type V (S), Forward Throw (FT), Type III (S), and Type II (S). Photometric data is tested in accordance with IESNA guidelines.
- BRACKETS** - Use with 5" traditional drilling pattern. An extruded radius 6" arm is shipped standard and compatible with all fixture mounting configurations. The fixture may also be mounted to round poles using the round pole plate adapter accessory (RPP2), which must be ordered separately.
- FINISHES** - Each fixture is finished with LSI's DuraGrip® polyester powder coat finishing process. The DuraGrip finish withstands extreme weather changes without cracking or peeling, and is guaranteed for five full years. Standard colors include bronze, black, platinum plus, white, satin verde green, metallic silver, and graphite.
- PHOTOMETRICS** - Please visit our web site at www.lighthouseinc.com for detailed photometric data.



NOTES:

1. LIGHTING TO MATCH LIGHTING INSTALLED IN PREVIOUS PHASES OF RIVERSIDE LANDING



CHALLENGER II MEDIUM
FLAT LENS

Symbol	Qty	Label	Description
CH	1	EXISTING	CH2HM-5-400-PSMER-F
CH	2	A	CH2HM-FT-400-PSMER-F 35' POLE

PLANNING
AUG 05 2016
DEPARTMENT

APPROVED BY THE CITY OF NEW BEDFORD PLANNING BOARD

CITY OF NEW BEDFORD PLANNING BOARD

APPROVED _____ ENDORSED _____

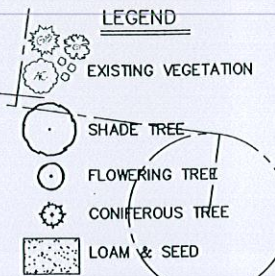
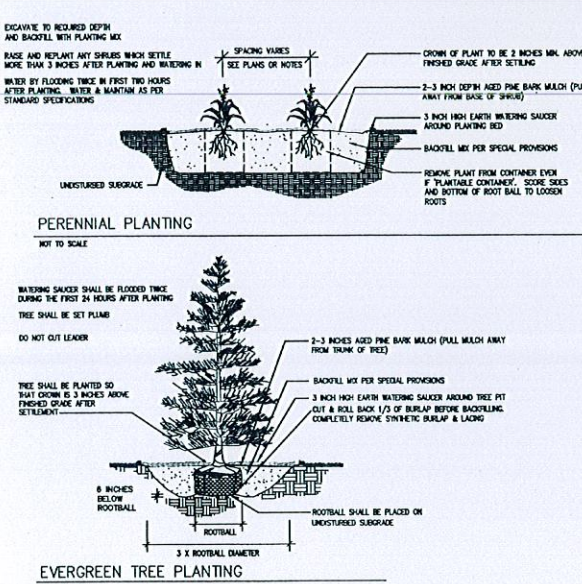
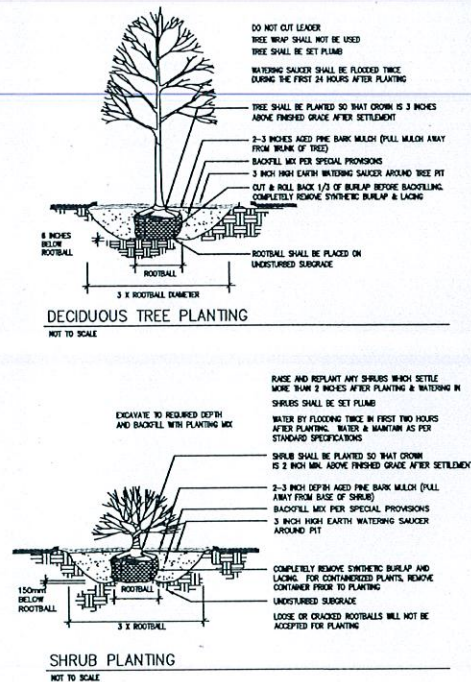


MERIDIAN OF 1963 COGGESHALL
STREET LAYOUT



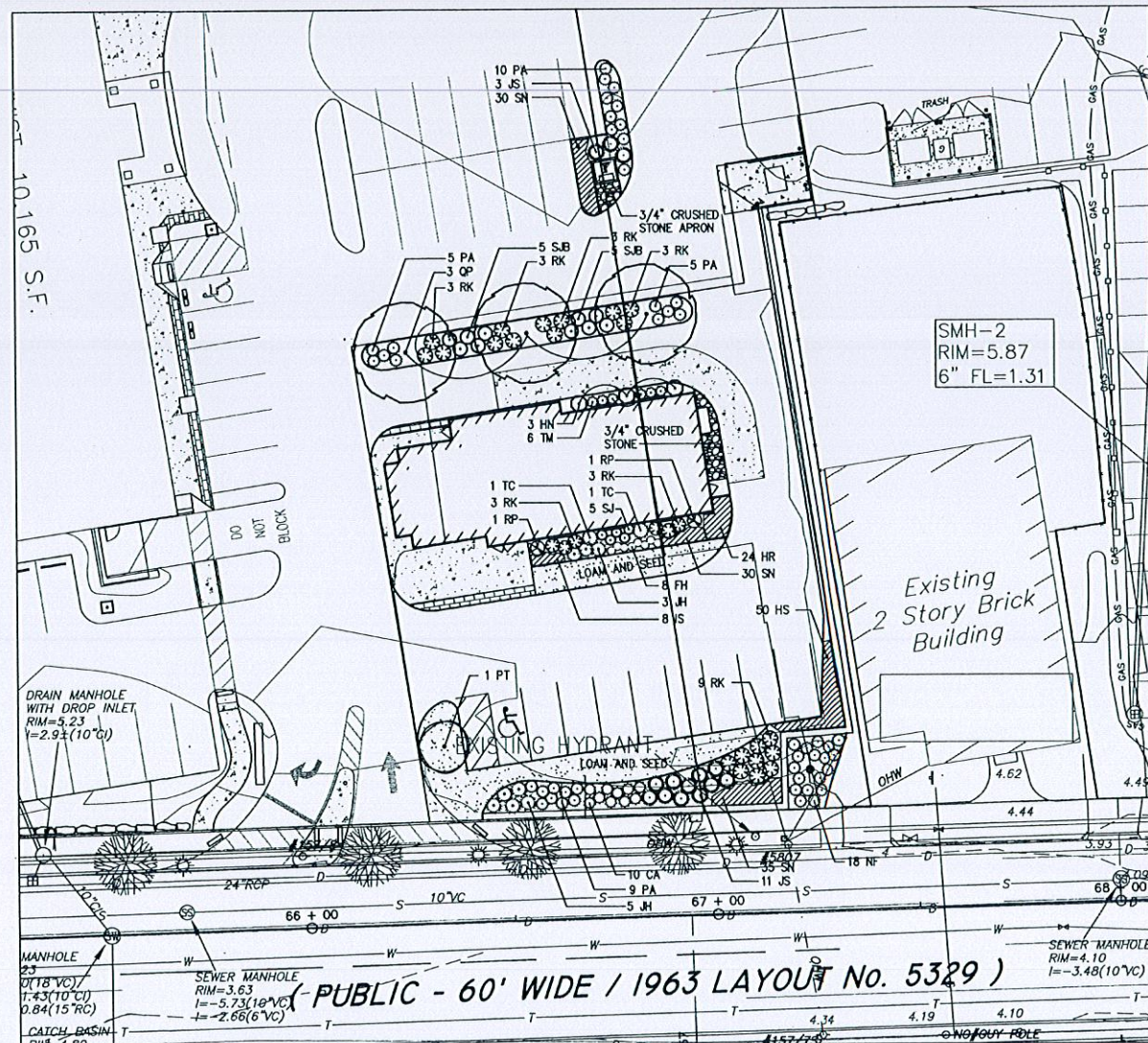
DRAWING TITLE LIGHTING PLAN		SCALE: 1" = 20'
PROJECT RIVERSIDE LANDING - PHASE III MOD. NEW BEDFORD, MASSACHUSETTS		DATE: JUL 13, 2016
CLIENT HIGHWAYVIEW LLC NEW BEDFORD, MASSACHUSETTS		DRAWN BY: EKW
DESIGNED BY: EKW		CHECKED BY: RJR
APPROVED BY: RJR		PROJECT NO. 1304801
1	8/4/16 INITIAL PLANNING DEPT. REVIEW	BY: EKW APP: RJR
REV.	DATE DESCRIPTION	BY APP.





NOTES

- FURNISH AND INSTALL PLANTS AS SHOWN. CONTRACTOR SHALL BE RESPONSIBLE FOR QUANTITY OF SHRUBS GRAPHICALLY SHOWN ON PLANS. IF THERE IS A DISCREPANCY BETWEEN PLANT LIST QUANTITIES AND GRAPHICS, THE GRAPHIC SHALL TAKE PRECEDENCE.
- PRIOR TO THE START OF EXCAVATION FOR THE PROJECT BOTH ON AND OF THE SITE, THE CONTRACTOR SHALL NOTIFY DIGSAFE AND BE PROVIDED A DIGSAFE NUMBER INDICATING THAT EXISTING UTILITIES HAVE BEEN LOCATED AND MARKED.
- CONTRACTOR SHALL BEGIN MAINTENANCE IMMEDIATELY UPON PLANTING AND WILL CONTINUE UNTIL FINAL ACCEPTANCE.
- CONTRACTOR SHALL VERIFY TREE REMOVALS WITH LANDSCAPE ARCHITECT PRIOR TO CONSTRUCTION START.
- CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE AWAY FROM BUILDING FOUNDATIONS, STRUCTURES, AND PLANTING BEDS.
- MAXIMUM SLOPE WITHIN DISTURBED AREAS SHALL NOT EXCEED 3:1, UNLESS OTHERWISE NOTED.
- PLANTS SHALL BEAR THE SAME RELATIONSHIP TO FINISH GRADE AS TO ORIGINAL GRADIES BEFORE DIGGING.
- PLANT MATERIALS SHALL CONFORM TO THE GUIDELINES ESTABLISHED BY "THE AMERICAN STANDARD FOR NURSERY STOCK", PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERMEN.
- PLANTS TO BE BALLED IN BURLAP OR CONTAINERIZED.
- MULCH FOR PLANTED AREAS TO BE AGED PINE BARK; PARTIALLY DECOMPOSED, DARK BROWN IN COLOR AND FREE OF WOOD CHIPS THICKER THAN 1/4 INCH.
- PLANTING SOIL MIX: LOAM THOROUGHLY INCORPORATED WITH ROTTED MANURE PROPORTIONED 5 C.Y. TO 1 C.Y. OR EQUIVALENT. PEAT MOSS WITH FERTILIZER ADDED PER MANUFACTURER'S RECOMMENDED RATES.
- THE LANDSCAPE CONTRACTOR SHALL GUARANTEE PLANT MATERIALS FOR ONE (1) FULL YEAR FROM DATE OF ACCEPTANCE.
- PLANT MATERIALS ARE SUBJECT TO THE APPROVAL OF THE LANDSCAPE ARCHITECT, AT THE NURSERY, AND AT THE SITE.
- LANDSCAPE ARCHITECT TO FLAG TREES TO BE TRANSPLANTED PRIOR TO CONSTRUCTION START.
- AREAS OF THE SITE WHICH HAVE BEEN DISTURBED AND NOT OTHERWISE DEVELOPED SHALL BE LOAMED WITH TOPSOIL TO A MINIMUM DEPTH OF 6", AND SEEDING WITH A MIX CONSISTING OF 40% PERENNIAL RYE GRASS; 30% CHEWINGS FESCUE; 30% KENTUCKY BLUEGRASS.
- SCREENED IMAGES SHOW EXISTING CONDITIONS. WHERE EXISTING CONDITIONS LIE UNDER OR ARE IMPINGED UPON BY PROPOSED BUILDINGS AND/OR SITE ELEMENTS, THE EXISTING CONDITION WILL BE REMOVED, ABANDONED AND/OR CAPPED OR DEMOLISHED AS REQUIRED.
- PROVIDE AUTOMATIC IRRIGATION SYSTEM FOR PLANTINGS AND LAWN AREAS UNLESS OTHERWISE NOTED.
- THERE SHALL BE NO SUBSTITUTION OF PLANT SPECIES WITHOUT AUTHORIZATION BY THE LANDSCAPE ARCHITECT.
- NO PLANTING SHALL BE INSTALLED BEFORE ACCEPTANCE OF ROUGH GRADING.
- PLANTS TO BE THOROUGHLY WATERED AFTER INSTALLATION, AT LEAST TWICE WITHIN THE FIRST 24 HOURS.



PLANT SCHEDULE

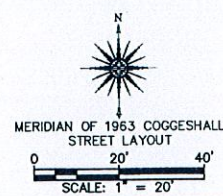
QTY.	KEY BOTANICAL NAME	COMMON NAME	SIZE/SPACING	COMMENTS
3	OP QUERCUS PALUSTRIS	PIN OAK	2-2.5' CAL	B&B, HIGH BRANCHED, FULL SYMMETRICAL
1	PT PRUNUS CERASIFERA THUNDERCLOUD	THUNDERCLOUD FLOWERING PLUM	8-10' HT.	B&B, SPECIMEN, PICTURESQUE
10	CA COTONEASTER APICULATUS	CRANBERRY COTONEASTER	18-24" SPR.	CONTAINER
3	HN HYDRANGEA 'NICKO BLUE'	BLUE HYDRANGEA	5 GAL.	CONTAINER
14	JS JUNIPERUS CHINENSIS 'SEA GREEN'	SEA GREEN JAMPER	24-30" HT.	CONTAINER
6	TM TAXUS MEDIA 'NICKO'S'	HICK'S YEW	24-30" HT.	CONTAINER
2	TC TAXUS CUSPIDATA 'CAPitata'	CAP YEW	3'-4' HT.	CONTAINER
27	RK ROSA 'KNOCKOUT'	KNOCKOUT ROSE	5 GAL.	CONTAINER
2	RP RHODODENDRON PUM 'COMPACTA'	COMPACT PUM RHODO	5 GAL.	CONTAINER
5	SJ SPIREA JAPONICA 'SHIROBANA'	SHIROBANA JAPANESE SPIREA	12-18" HT.	CONTAINER
10	S.B SPIREA JAPONICA 'BUNALDA 'ANTHONY WATERER'	ANTHONY WATERER SPIREA	18-24" HT.	CONTAINER
8	FH FESTUCA GLAUCA 'ELIJAH BLUE'	BLUE FESCUE	2 GAL. - 24" O.C.	CONTAINER
29	PA PENNSETUM ALPICOLOIDES 'WAMEN'	DWARF FOUNTAIN GRASS	2 GAL. - 36" O.C.	CONTAINER
24	HR HEMEROCALLIS 'TOSTY RETURNS'	RED DAYLILY	1 GAL. - 18" O.C.	CONTAINER
50	HS HEMEROCALLIS 'STELLA D'ORO'	YELLOW DAYLILY	1 GAL. - 18" O.C.	CONTAINER
18	NF NEPETA 'WALKER'S LOW'	WALKER'S LOW CATMINT	1 GAL. - 18" O.C.	CONTAINER
8	IS IRIS SIBERICA	SIBERIAN IRIS	1 GAL. - 18" O.C.	CONTAINER
95	SN SALVIA SILVESTRE 'MAY NIGHT'	MAY NIGHT SALVIA	1 GAL. - 18" O.C.	CONTAINER
8	JH JUNCUS HORIZONTALIS MILTON	BLUE RUG CREEPING JUNCUS	3 GAL. - 36" O.C.	CONTAINER

PLANNING
AUG 05 2016

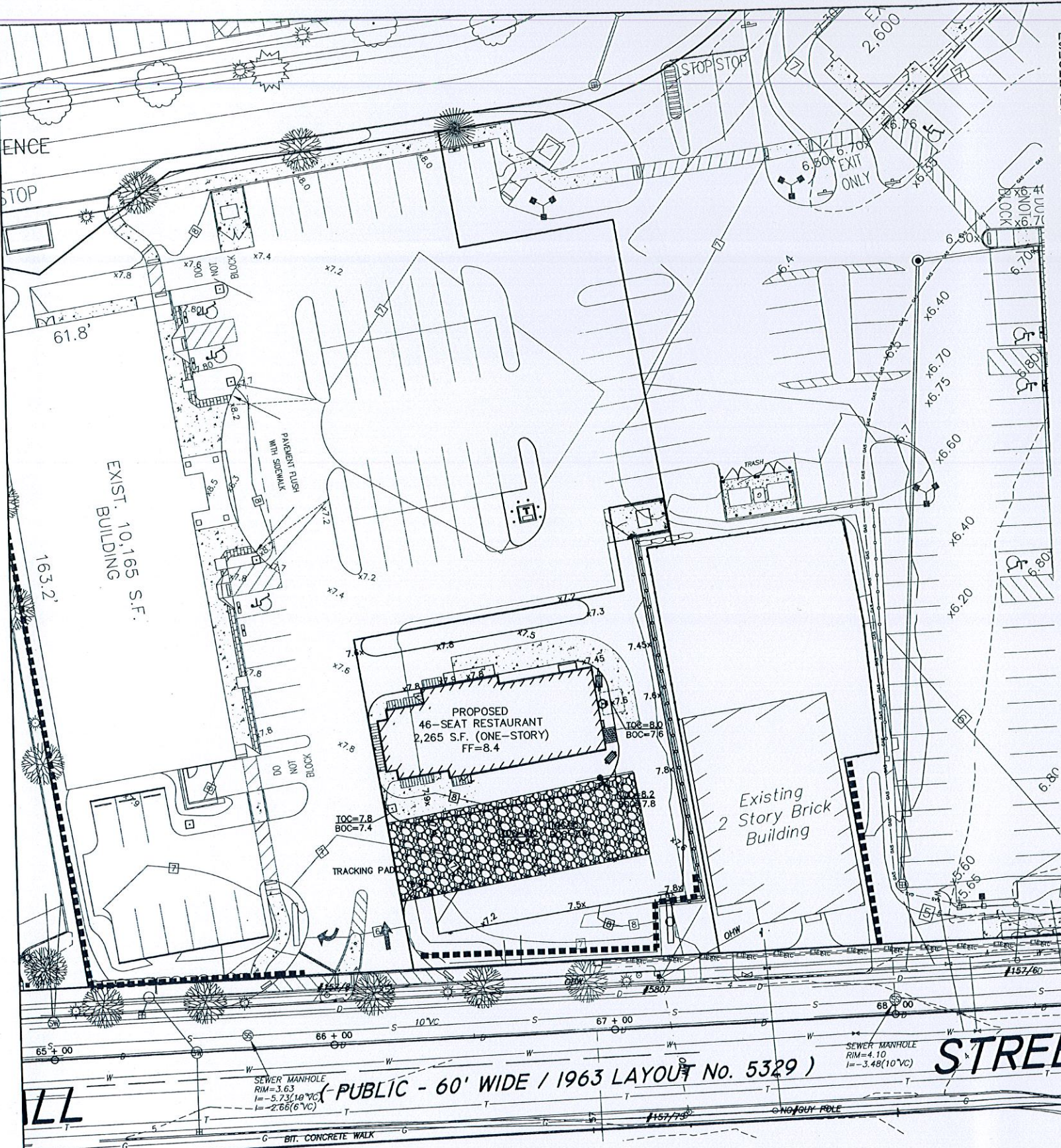
APPROVED BY THE CITY OF NEW BEDFORD PLANNING BOARD

CITY OF NEW BEDFORD PLANNING BOARD

APPROVED _____ ENDORSED _____



DRAWING TITLE LANDSCAPE PLAN		SCALE: 1" = 20'
PROJECT RIVERSIDE LANDING - PHASE IV NEW BEDFORD, MASSACHUSETTS		DATE: AUGUST 4, 2016
CLIENT HIGHWAYVIEW LLC NEW BEDFORD, MASSACHUSETTS		DRAWN BY: MPR
DESIGNED BY: MPR		CHECKED BY: RJR
APPROVED BY: RJR		SHEET NO. 7
PROJECT NO. 1304801		



EROSION & SEDIMENT CONTROL NOTES:

IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTROL EROSION AND PREVENT SEDIMENTATION WITHIN 25' OF RESOURCE AREAS OR OFFSITE PROPERTIES. IT IS INTENDED THAT THE IMPLEMENTATION OF THE FOLLOWING MEASURES WILL MEET THIS GOAL. WHEN IT IS CLEAR TO THE DESIGNER THAT EROSION AND SEDIMENTATION HAVE BEEN ADEQUATELY CONTROLLED WITHOUT THE IMPLEMENTATION OF EVERY MEASURE, ADDITIONAL MEASURES NEED NOT BE IMPLEMENTED. ALTERNATIVELY, IF ALL OF THE FOLLOWING MEASURES HAVE BEEN IMPLEMENTED AND THE CONTROL OF EROSION AND SEDIMENTATION IS INADEQUATE, THE CONTRACTOR MUST EMPLOY SUFFICIENT SUPPLEMENTAL MEASURES BEYOND THE SCOPE OF THIS PLAN.

1. EROSION AND SEDIMENT CONTROL MEASURES WILL BE INSTALLED PRIOR TO CONSTRUCTION. STABILIZATION OF ALL REGRADED AND SOIL STOCKPILE AREAS WILL BE INITIATED AND MAINTAINED DURING ALL PHASES OF CONSTRUCTION.
2. ALL EROSION AND SEDIMENT CONTROL MEASURES WILL BE CONSTRUCTED IN ACCORDANCE WITH LOCAL MUNICIPAL REGULATIONS. ALL EROSION CONTROL MEASURES ARE TO BE MAINTAINED AND UPGRADED AS REQUIRED TO ACHIEVE PROPER SEDIMENT CONTROL DURING CONSTRUCTION.
3. ADDITIONAL CONTROL MEASURES WILL BE INSTALLED DURING THE CONSTRUCTION PERIOD, IF DEEMED NECESSARY BY THE OWNER OR AGENTS OF THE OWNER.
4. CATCH BASINS WILL BE PROTECTED WITH STRAWBALE FILTERS THROUGHOUT THE CONSTRUCTION PERIOD UNTIL ALL DISTURBED AREAS ARE THOROUGHLY STABILIZED. SILT SOCKS SHOULD BE INSTALLED UNDER GRATE OPENING UNTIL PAVEMENT IS IN PLACE AND GROUND SURFACE IS STABILIZED.
5. SEEDING MIXTURE FOR FINISHED GRASSED AREAS WILL BE AS FOLLOWS:

KENTUCKY BLUE GRASS	45%
CREeping RED FESCUE	45%
PERENNIAL RYEGRASS	10%

SEED TO BE APPLIED AT A RATE OF 4 LBS./1000 SQ. FT.
PLANTING SEASONS SHALL BE APRIL 1 TO JUNE 1 AND AUGUST 1 TO OCTOBER 15.
AFTER OCTOBER 15, AREAS WILL BE STABILIZED WITH STRAWBALE CHECK, FILTER FABRIC, OR WOODCHIP MULCH, AS REQUIRED, TO CONTROL EROSION.

6. AREAS THAT ARE NOT THE LOCATION OF ACTIVE CONSTRUCTION WHICH ARE TO BE LEFT BARE FOR OVER ONE MONTH BEFORE FINISHED GRADING AND SEEDING IS ACHIEVED, SHALL BE MULCHED OR RECEIVE TEMPORARY STABILIZATION SUCH AS JUTE NETTING OR SHALL RECEIVE A TEMPORARY SEEDING OF PERENNIAL RYEGRASS APPLIED TO A RATE OF 2 LBS./1000 SQ. FT. LIME STONE (EQUVALENT TO BE 50 PERCENT CALCIUM PLUS MAGNESIUM OXIDE) SHALL BE APPLIED AS SEEDBED PREPARATION AT A RATE OF 90 LBS./1000 SQ. FT. PLANTING SEASONS SHALL BE APRIL 1 TO JUNE 1 AND AUGUST 1 TO OCTOBER 1. AREAS TO BE LEFT BARE BEFORE FINISH GRADING AND SEEDING OUTSIDE OF PLANTING SEASONS SHALL RECEIVE AN AIR-DRIED WOOD CHIP MULCH, FREE OF COARSE MATTER.

7. AT ALL PROPOSED FILL AREAS WHICH ARE NOT CURRENTLY SHOWN ON THESE PLANS, THE CONTRACTOR SHALL ESTABLISH AN EROSION CONTROL LINE (STRAWBALE CHECK OR FILTER FABRIC) ABOUT TEN (10') FEET FROM TOE TO SLOPE OF PROPOSED FILL AREAS PRIOR TO BEGINNING FILL INSTALLATION. STABILIZATION OF SLOPES IN FILL AREAS (USING MULCH OR GRASS) SHALL BE INITIATED WITHIN THIRTY (30) DAYS OF COMMENCEMENT OF FILL INSTALLATION.

8. STABILIZATION OF SLOPES IN CUT AREAS (USING MULCH OR GRASS) AND THE INSTALLATION OF CONTROL LINE (STRAWBALE CHECK OR FILTER FABRIC) AT THE TOE OF SLOPE SHALL BE INITIATED WITHIN THIRTY (30) DAYS OF COMPLETION.

9. SEDIMENT REMOVED FROM CONTROL STRUCTURES WILL BE DISPOSED IN A MANNER WHICH IS CONSISTENT WITH THE INTENT OF THE PLAN. ALL STRAWBALES OR SILT FENCE RETAINING SEDIMENT OVER 1/2 THEIR HEIGHT SHALL HAVE THE SEDIMENT REMOVED AND ALL DAMAGED EROSION CONTROLS SHALL BE REPAIRED OR REPLACED.

10. CONTRACTOR WILL BE ASSIGNED THE RESPONSIBILITY FOR IMPLEMENTING THIS EROSION AND SEDIMENT CONTROL PLAN. THIS RESPONSIBILITY INCLUDES THE INSTALLATION AND MAINTENANCE OF CONTROL MEASURES, INFORMING ALL PARTIES ENGAGED ON THE CONSTRUCTION SITE OF THE REQUIREMENTS AND OBJECTIVES OF THE PLAN, AND NOTIFYING THE PLANNING BOARD OF ANY TRANSFER OF THIS RESPONSIBILITY. THE OWNER SHALL BE RESPONSIBLE FOR CONVEYING A COPY OF THE EROSION AND SEDIMENT CONTROL PLAN IF THE TITLE TO THE LAND IS TRANSFERRED.

11. THE CONTRACTOR SHALL VERIFY IN THE FIELD THAT THE CONTROLS REQUIRED BY THIS PLAN ARE PROPERLY INSTALLED, SHALL MAKE INSPECTION OF SUCH FACILITIES NOT LESS FREQUENTLY THAN EVERY 14 DAYS OR AFTER A RAINFALL IN EXCESS OF 1/2 INCH, WHICHEVER OCCURS FIRST. THE INSPECTION REPORTS SHALL BE SUBMITTED TO THE PLANNING DEPARTMENT AND CONSERVATION COMMISSION OFFICE ON A MONTHLY BASIS.

12. STOCKPILES OF SOIL SHALL BE SURROUNDED BY A SEDIMENT BARRIER. SOIL STOCKPILES TO BE LEFT BARE FOR MORE THAN THIRTY (30) DAYS SHALL BE STABILIZED WITH TEMPORARY VEGETATION OR MULCH. IF SOIL STOCKPILES ARE TO REMAIN FOR MORE THAN SIXTY (60) DAYS, FILTER FABRIC SHALL BE USED IN PLACE OF STRAWBALES. SIDE SLOPES SHALL NOT EXCEED 2:1.

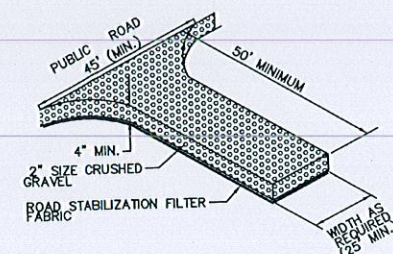
13. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONTROL DUST AND WIND EROSION THROUGHOUT THE LIFE OF HIS CONTRACT. DUST CONTROL SHALL INCLUDE, BUT IS NOT LIMITED TO SPRINKLING OF WATER ON EXPOSED SOILS AND HAUL ROADS. CONTRACTOR SHALL CONTROL DUST TO PREVENT A HAZARD TO TRAFFIC.

14. WHERE DEWATERING IS NECESSARY, THERE SHALL NOT BE A DISCHARGE DIRECTLY INTO WETLANDS OR WATERCOURSES. PROPER METHODS AND DEVICES SHALL BE UTILIZED TO THE EXTENT PERMITTED BY LAW, SUCH AS PUMPING WATER INTO A TEMPORARY SEDIMENTATION BOWL, PROVIDING SURGE PROTECTION AT THE INLET AND THE OUTLET OF PUMPS, OR FLOATING THE INTAKE OF THE PUMP, OR OTHER METHODS TO MINIMIZE AND RETAIN THE SUSPENDED SOLIDS. IF A PUMPING OPERATION IS CAUSING TURBIDITY PROBLEMS, SAID OPERATION SHALL CEASE UNTIL SUCH TIME AS FEASIBLE MEANS OF CONTROLLING TURBIDITY ARE DETERMINED AND IMPLEMENTED. SAID DISCHARGE POINTS SHALL BE LOCATED OVER 100 FEET FROM THE DELINEATED WETLANDS AS INDICATED ON THIS PLAN.

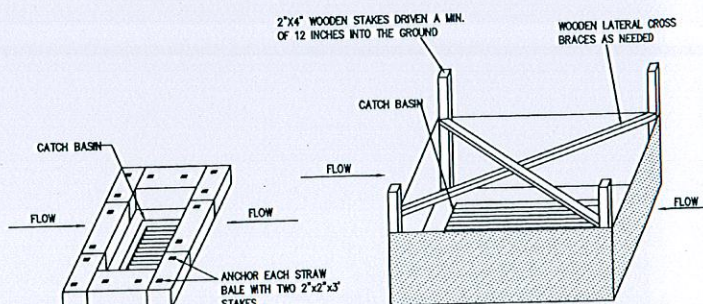
15. SILT SOCKS OR OTHER EQUIVALENT ARE TO BE PLACED IN THE FIRST DOWNSTREAM CATCH BASINS FROM THE SITE.

16. ALL BMP EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO ANY SITE WORK.

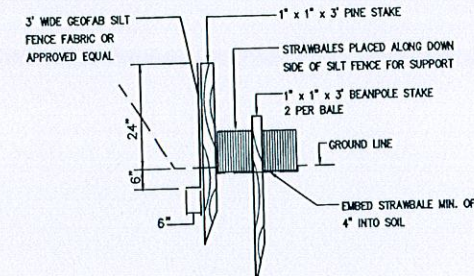
17. EROSION CONTROL BMPs SHALL CONFORM TO US EPA, NPDES, MA DEP AND MASSACHUSETTS EROSION AND SEDIMENT CONTROL GUIDELINES FOR URBAN AND SUBURBAN AREAS.



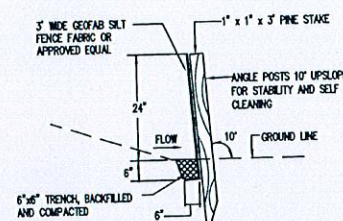
CONSTRUCTION ENTRANCE
NOT TO SCALE



SEDIMENT AND EROSION CONTROL AT CATCH BASINS
TO BE INSTALLED IF SPECIFIED BY FIELD ENGINEER
NOT TO SCALE



STRAWBALE AND SILT FENCE DETAIL
(TO BE PLACED AT THE TOE OF FILL SLOPES)
NOT TO SCALE



SILT FENCE DETAIL
NOT TO SCALE

APPROVED BY THE CITY OF NEW BEDFORD PLANNING BOARD

CITY OF NEW BEDFORD PLANNING BOARD

APPROVED _____ ENDORSED _____



0' 40' 80' 120'

DRAWING TITLE EROSION CONTROL PLAN		SCALE: 1" = 20'	
PROJECT RIVERSIDE LANDING - PHASE III MOD. NEW BEDFORD, MASSACHUSETTS		DATE: JUL 13, 2016	
CLIENT HIGHWAYVIEW LLC NEW BEDFORD, MASSACHUSETTS		DRAWN BY: EKW	
DESIGNED BY: EKW		CHECKED BY: RJR	
APPROVED BY: RJR		PROJECT NO. 1304801	
1 8/4/16 INITIAL PLANNING DEPT. REVIEW		BY EKW	APP. RJR
REV. DATE DESCRIPTION		BY	APP.

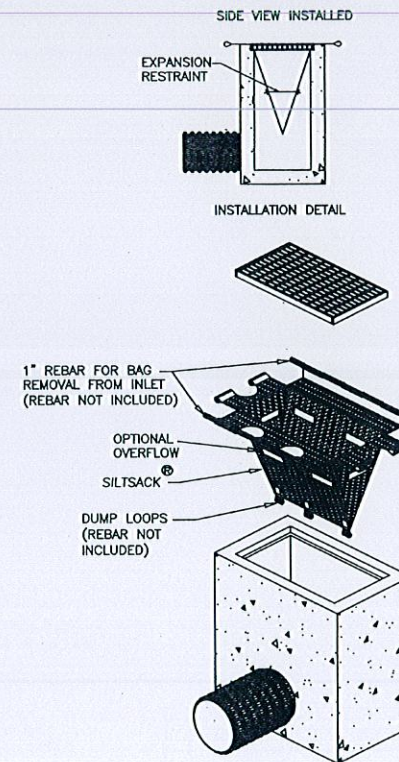
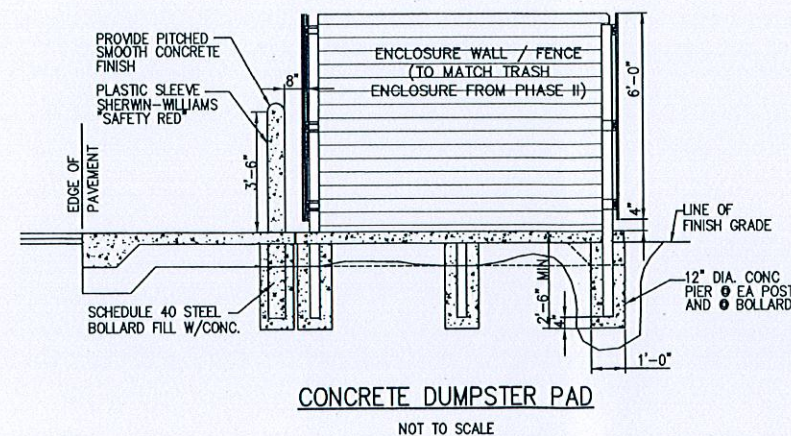
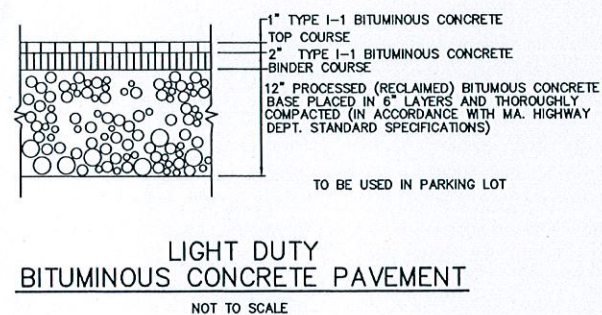
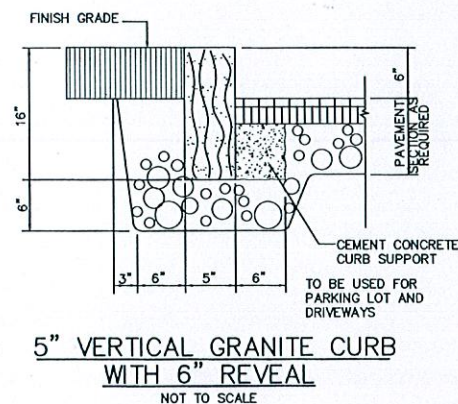
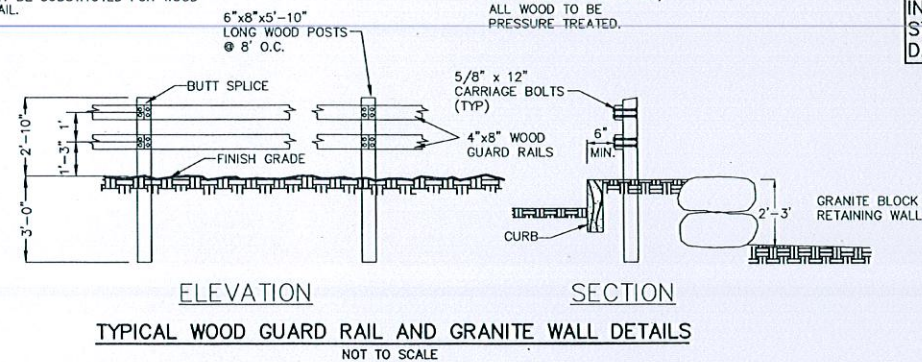
PRIME ENGINEERING
P.O. BOX 1288
350 BEDFORD ST.
LAKEVILLE, MA 02347
TEL: 508.947.0050
FAX: 508.947.2004

0:\PROJECTS\New Bedford\Supp\Shel - Former Fehrmann M 4\Site Design - Phase III\PROPERTIES BASELINE

NOTE:
A DRY STONE WALL MINIMUM 3 FT.
HIGH MAY BE SUBSTITUTED FOR WOOD
GUARDRAIL.

NOTE:
ALL CARRIAGE BOLTS, NUTS
& WASHERS TO BE GALV.,
ALL WOOD TO BE
PRESSURE TREATED.

ALL UTILITY WORK SHALL BE DONE
IN CONFORMANCE WITH NEW
BEDFORD DEPARTMENT OF PUBLIC
INFRASTRUCTURE CONSTRUCTION
STANDARDS AND SPECIFICATIONS
DATED MARCH, 2012



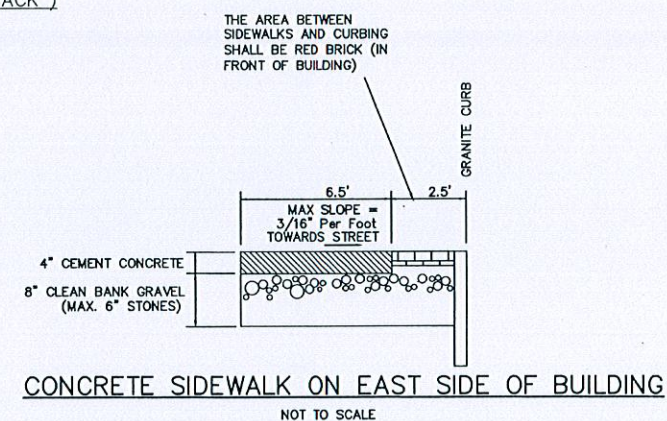
NOTE: THE SILTSACK® WILL BE MANUFACTURED FROM A
WOVEN POLYPROPYLENE FABRIC THAT MEETS OR EXCEEDS
THE FOLLOWING SPECIFICATIONS.

REGULAR FLOW SILTSACK®
(FOR AREAS OF LOW TO MODERATE PRECIPITATION AND RUN-OFF)

PROPERTIES	TEST METHOD	UNITS
GRAB TENSILE	ASTM D-4632	300 LBS
STRENGTH GRAB	ASTM D-4632	20%
TENSILE ELONGATION	ASTM D-4833	120 LBS
PUNCTURE MULLEN	ASTM D-3786	800 PSI
BURST TRAPEZOID	ASTM D-4533	120 LBS
TEAR UV RESISTANCE	ASTM D-4355	80%
APPARENT OPENING	ASTM D-4751	40 US SIEVE
SIZE FLOW RATE	ASTM D-4491	40 GAL/MIN/SQ FT 0.55
PERMITTIVITY	ASTM D-4491	SEC -1

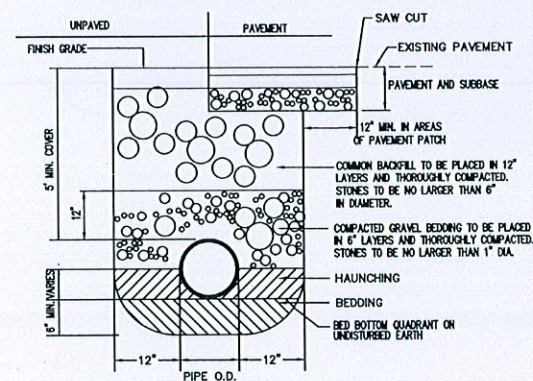
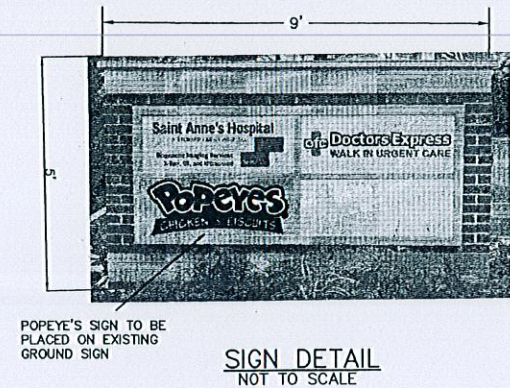
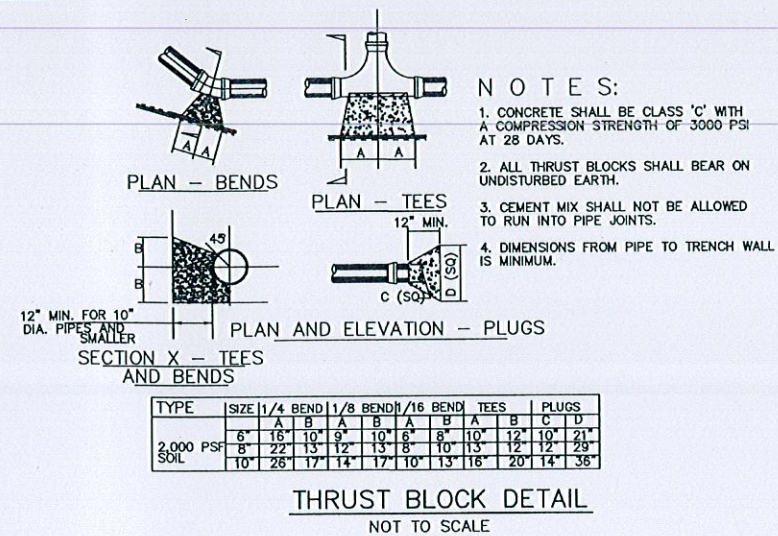
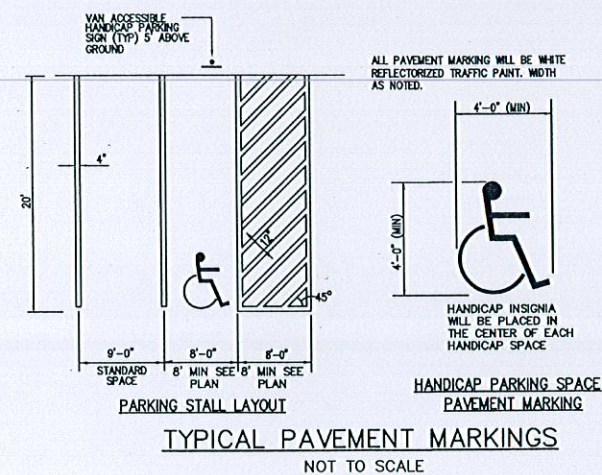
HI-FLOW SILTSACK®
(FOR AREAS OF MODERATE TO HEAVY PRECIPITATION AND RUN-OFF)

PROPERTIES	TEST METHOD	UNITS
GRAB TENSILE	ASTM D-4632	265 LBS
STRENGTH GRAB	ASTM D-4632	20%
TENSILE ELONGATION	ASTM D-4833	135 LBS
PUNCTURE MULLEN	ASTM D-3786	420 PSI
BURST TRAPEZOID	ASTM D-4533	45 LBS
TEAR UV RESISTANCE	ASTM D-4355	90%
APPARENT OPENING	ASTM D-4751	20 US SIEVE
SIZE FLOW RATE	ASTM D-4491	200 GAL/MIN/SQ FT 1.5
PERMITTIVITY	ASTM D-4491	SEC -1

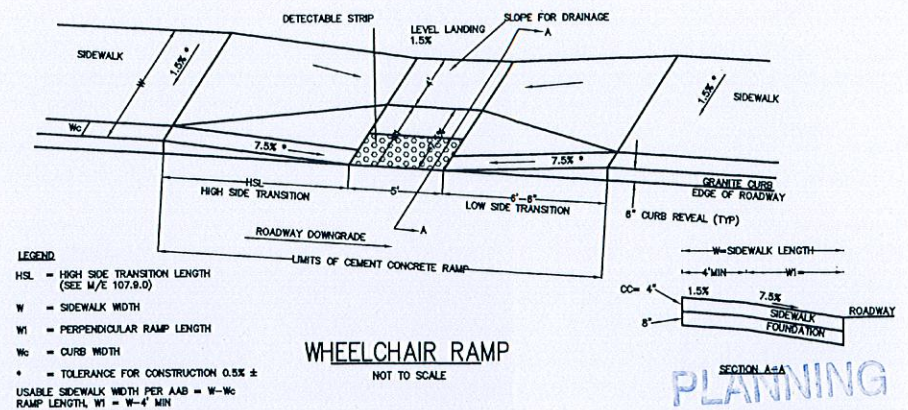
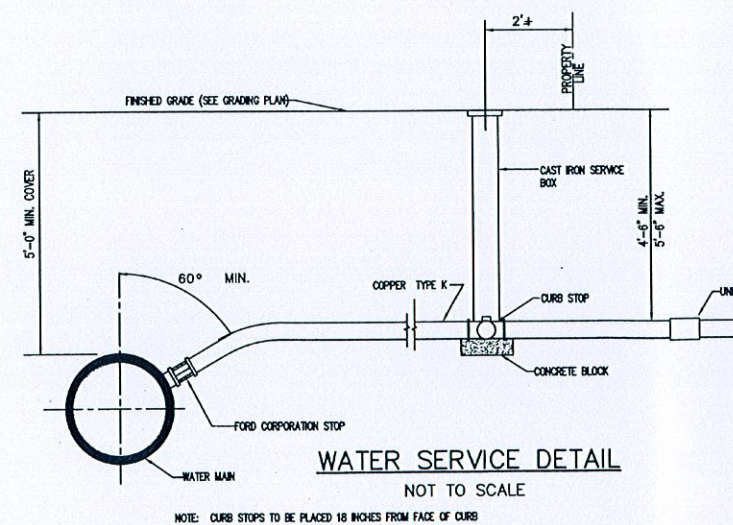
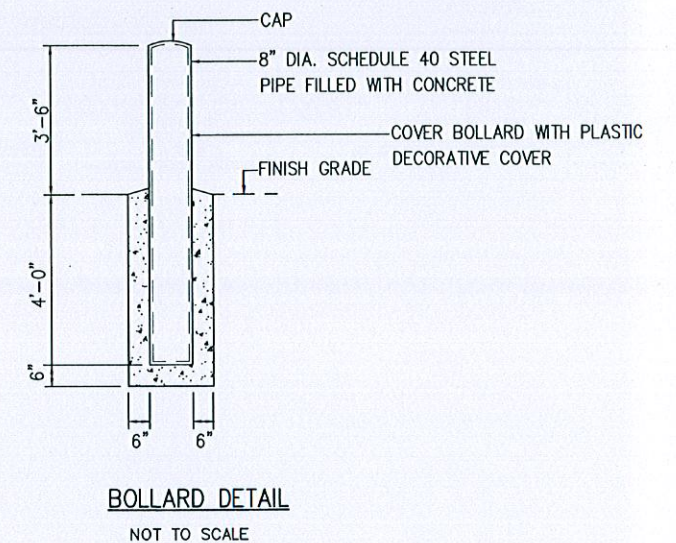
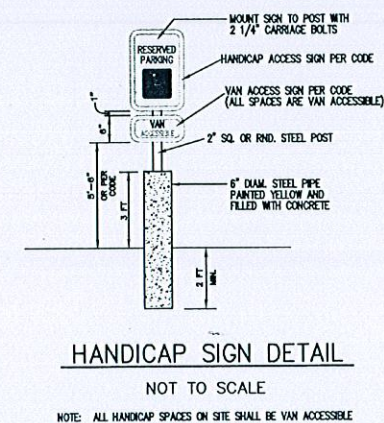




PLANNING
AUG 05 2013
DEPARTMENT

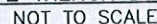
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REV.	DATE	DESCRIPTION	BY	APP.	
DRAWING TITLE					
DETAILS - 1					
PROJECT					
RIVERSIDE LANDING - PHASE III					
NEW BEDFORD, MASSACHUSETTS					
CLIENT					
HIGHWAYMEW LLC					
NEW BEDFORD, MA					
CIVIL ENGINEERING					
LAND SURVEYING					
ENVIRONMENTAL					
ASSESSMENT					
P.O. BOX 1088					
350 BEDFORD ST.					
LAKEVILLE, MA 02347					
TEL: 508.947.0050					
FAX: 508.947.2004					
SCALE:					
NOT TO SCALE					
DATE:					
JUN 20, 2013					
DRAWN BY:					
EKW					
DESIGNED BY:					
RJR					
CHECKED BY:					
RJR					
APPROVED BY:					
RJR					
PROJECT NO.					
1304801					



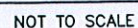
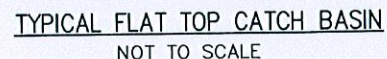
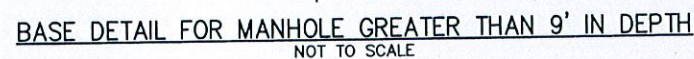
ALL UTILITY WORK SHALL BE DONE
IN CONFORMANCE WITH NEW
BEDFORD DEPARTMENT OF PUBLIC
INFRASTRUCTURE CONSTRUCTION
STANDARDS AND SPECIFICATIONS
DATED MARCH, 2012






				DRAWING TITLE		DETAILS - 2		SCALE: NOT TO SCALE			
				PROJECT		RIVERSIDE LANDING - PHASE II NEW BEDFORD, MASSACHUSETTS		DATE: JUN 20, 2013			
				CLIENT		HIGHWAYVIEW LLC NEW BEDFORD, MA		DRAWN BY: EKW			
								DESIGNED BY: RJR			
						* CIVIL ENGINEERING LAND SURVEYING ENVIRONMENTAL ASSESSMENT		 PRIME ENGINEERING INC.		P.O. BOX 1088 350 BEDFORD ST. LAKEVILLE, MA 02347 TEL: 508.947.0050 FAX: 508.947.2004	
1	8/4/16	INITIAL PLANNING DEPT. REVIEW	EKW	RJR	CHECKED BY: SHEET NO. 10						
REV.	DATE	DESCRIPTION	BY	APP.	APPROVED BY: PROJECT NO. 1304801						

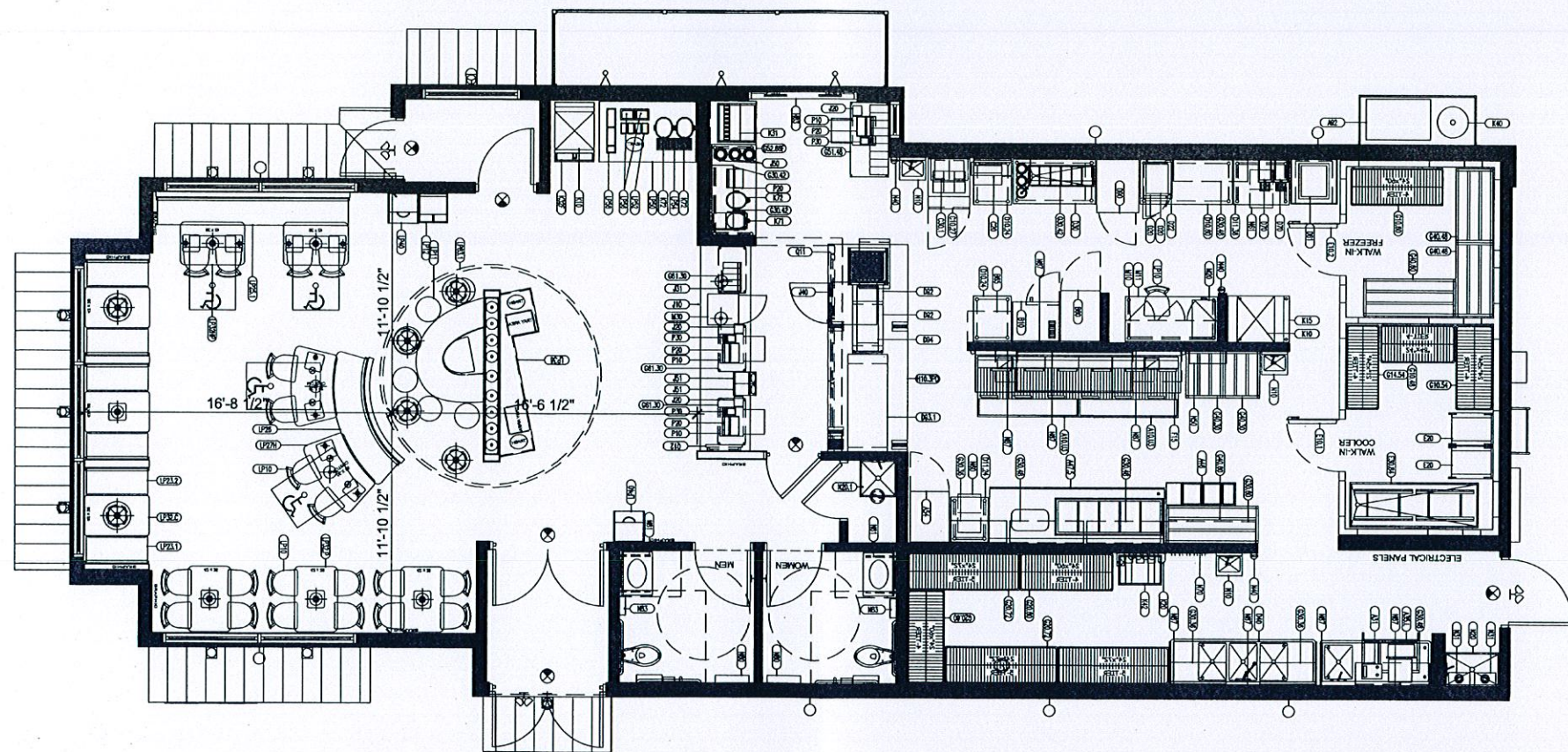


1. THE STORM WATER MANAGEMENT SYSTEM SHALL BE INSPECTED
2. AFTER TWO YEARS OF FULL OPERATION BY A REGISTERED PROFESSIONAL CIVIL ENGINEER TO CONFIRM ITS ADEQUACY. THE INSPECTION SHALL INCLUDE EXAMINATION OF ALL COMPONENTS OF THE SYSTEM INCLUDING FOREBAYS, WATER QUALITY BASINS, CATCH BASINS, STORM WATER OUTLETS AND STORMCEPTOR UNITS.



DEPARTMENT

				DRAWING TITLE				DETAILS - 3				SCALE:			
				PROJECT				RIVERSIDE LANDING - PHASE III NEW BEDFORD, MASSACHUSETTS				NOT TO SCALE			
				CLIENT				HIGHWAYVIEW LLC NEW BEDFORD, MA				DATE:		JUN 20, 2013	
				CIVIL ENGINEERING LAND SURVEYING  PRIME ENGINEERING				DRAWN BY:				EKW			
								DESIGNED BY:				RJR			
								CHECKED BY:				RJR		SHEET NO.	
								APPROVED BY:				RJR		PROJECT NO.	
														130480	
1 8/4/16 INITIAL PLANNING DEPT. REVIEW				EKW RJR				P.O. BOX 1088 350 BEDFORD ST. LAKESVILLE, MA 02457 TEL: 508.947.0050 FAX: 508.947.2004							
2 2/27/17 DESCRIPTION				BY APP. ENVIRONMENTAL ASSESSMENT											



1 FLOOR PLAN
K-1 1/4"=1'-0"



PLANNING
AUG 05 2016
DEPARTMENT

ISSUED FOR:	
DATE	DESCRIPTION
08-03-16	FOR PLANNING REVIEW

REVISIONS		
NO.	DATE	DESCRIPTION
1	08-03-16	REVISED PER TOWN COMMENTS



POPEYE'S LOUISIANA KITCHEN
400 Perimeter Center Terrace Suite 1000
Atlanta, GA 30346



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DRAWING
FLOOR PLAN

PROJECT:
Riverside Landing
Popeye's
Coggeshall Street
New Bedford, MA

DATE	07.28.2016
PROJECT NO.	160526
DRAWN BY	CPC
CHECK BY	PM
DWG NO.	

K-1

SCALE	SHT. NO.
AS NOTED	1 OF 3

SPECIFICATIONS:

DIVISION 7: THERMAL AND MOISTURE PROTECTION

GENERAL PROVISION
1. SCOPE: FURNISH AND INSTALL GRAVEL STOPS, FLASHING, PARAPET CAP, DOWNSPOUTS, AND GUTTERS.

A. ROOFING MEMBRANE FLASHING IS INCLUDED IN SECTION 7B: MEMBRANE ROOFING.

MATERIALS
1. MATERIALS SHEET METAL: .032 ALUMINUM.
2. NAIL FASTENERS: 1 3/4" X 11 GAUGE GALVANIZED, STAINLESS STEEL, OR ALUMINUM ROOFING NAILS MAY BE USED FOR FASTENERS INTO WOOD WHEN CONCEALED ONLY.

3. WASHERS: NEOPRENE
4. SCREW FASTENERS: CORROSION-RESISTANT, SELF-TAPPING, HEX HEAD SCREW, 1/4" MINIMUM DIAMETER WITH SUFFICIENT LENGTH TO PENETRATE 1" MINIMUM INTO WOOD OR 1/2" MINIMUM INTO STEEL. PROVIDE NEOPRENE SEALING WASHER FOR EXPOSED FASTENING.

PERFORMANCE
1. INSTALLATION: EXPOSED FLASHINGS SHALL BE PAINTED TO MATCH ADJACENT MATERIALS. VERIFY WITH POPEYE'S REPRESENTATIVE.

SECTION 7C: STANDING SEAM CANOPY

PART 1 - GENERAL
1.0 SUBMITTALS
A. SUBMIT FOR APPROVAL SAMPLES, SHOP DRAWINGS, PRODUCT DATA.

QUALITY ASSURANCE
A. COMPLY WITH GOVERNING CODES AND REGULATIONS. PROVIDE PRODUCTS OF ACCEPTABLE MANUFACTURERS WHICH HAVE BEEN IN SATISFACTORY USE IN SIMILAR SERVICE FOR THREE YEARS. USE EXPERIENCED INSTALLERS. DELIVER, HANDLE, STORE MATERIALS IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS.

METAL ROOF SYSTEM MANUFACTURER, UPON FINAL ACCEPTANCE FOR PROJECT, FURNISH A WARRANTY COVERING BARE METAL AGAINST RUPTURE, STRUCTURAL FAILURE AND PERFORATION DUE TO NORMAL ATMOSPHERIC CORROSION EXPOSURE FOR A PERIOD OF 20 YEARS.

PART 2 - PRODUCTS (UC-4 SERIES, AS MANUFACTURED AND SPECIFIED BY UNA-CLAD, METAL ROOF SYSTEMS.)

2.0 MATERIALS

A. METAL ROOF SYSTEM PROFILE:
1. UC-4 TWO CLIP, 1 1/2" HIGH BATTENS X 12" RIB TO RIB. (SMALL BATTEN-58)
2. CONCEALED FASTENER

B. GAUGE:
1. .028 GAUGE - STEEL

C. TEXTURE:
1. SMOOTH

D. FINISH:
1. PREMIUM FLUOROCARBON COATING PRODUCED WITH KYNAR 500 OR HYLAR 5000 RESIN (20 YEAR WARRANTY.)

E. MANUFACTURER:
1. UNA-CLAD OR EQUAL

PART 3 - EXECUTION
3.0 INSTALLATION
A. COMPLY WITH EQUALS SHEET METAL MANUAL RECOMMENDATIONS. COMPLY WITH ACCESSORY MANUFACTURER'S INSTRUCTIONS AND RECOMMENDATIONS. COORDINATE INSTALLATION WITH ROOFING SYSTEM TO ENSURE WEATHERTIGHT PERFORMANCE.

B. ANCHOR SECURELY TO STRUCTURE TO WITHSTAND INWARD AND OUTWARD LOADS.
C. ISOLATE DISSIMILAR METALS TO PREVENT GALVANIC CORROSION.

DIVISION 9: FINISHES

SECTION 9G: EIFS

PART 1 GENERAL
1.01 DESCRIPTION
A. DESIGN REQUIREMENTS: THE STRUCTURAL WALL SYSTEM TO WHICH THE EIFS IS ATTACHED SHALL MEET L240 MAXIMUM ALLOWABLE DEFLECTION CRITERIA AND APPLICABLE BUILDING CODE REQUIREMENTS.

1.02 SUBMITTALS
A. SUBMIT SAMPLES FOR APPROVAL AS DIRECTED BY OWNER.

1.03 DELIVERY, STORAGE AND HANDLING
A. ALL EIFS MATERIALS SHALL BE DELIVERED IN THEIR ORIGINAL SEALED CONTAINERS BEARING MANUFACTURER'S NAME AND IDENTIFICATION OF PRODUCT WITH WRITTEN APPLICATION INSTRUCTIONS AND APPROPRIATE HEALTH, HAZARD, AND SAFETY DATA.

B. ALL EIFS READY-MIXED MATERIALS SHALL BE PROTECTED FROM EXTREME HEAT, SUN AND FROST. FACTORY PROPORTIONED BAGGED MATERIALS SHALL BE STORED OFF THE GROUND AND PROTECTED FROM MOISTURE.

1.04 JOB CONDITIONS
A. ALL EIFS MATERIALS SHALL NEVER BE APPLIED IF AMBIENT AND SURFACE TEMPERATURES CANNOT BE KEPT ABOVE 40° F DURING APPLICATION AND DRYING PERIOD. FOR INSTALLATION IN TEMPERATURES LESS THAN 40° F SUPPLEMENTARY HEAT SHALL BE PROVIDED. THE INSTALLED EIFS MATERIALS SHALL BE PROTECTED FROM EXPOSURE TO RAIN AND FREEZING UNTIL DRY.

1.11 WARRANTY
A. PROVIDE MANUFACTURER'S STANDARD LABOR AND MATERIAL WARRANTY.

PART 2 PRODUCTS

2.01 MANUFACTURERS
A. STO CORP.
B. DRYVIT SYSTEMS, INC.

2.02 ADHESIVES
A. DISPERSION ADHESIVE - NONCEMENTITIOUS, ACRYLIC BASED ADHESIVE.

2.03 INSULATION BOARD
A. NOMINAL 1.0 R VALUE PER INCH (16 lb/cu yd) EXPANDED POLYSTYRENE (EPS) INSULATION BOARD IN COMPLIANCE WITH ASTM C 578 TYPE I REQUIREMENTS, AND ENR GUIDELINE SPECIFICATION FOR EXPANDED POLYSTYRENE (EPS) INSULATION BOARD.

2.04 BASECOAT
A. ONE-COMPONENT POLYMER MODIFIED CEMENTITIOUS BASE COAT WITH FIBER REINFORCEMENT AND LESS THAN 33% PORTLAND CEMENT CONTENT BY WEIGHT.

2.05 REINFORCING MESHES
A. STANDARD MESH
1. STO MESH - NOMINAL 4.5 oz/sq yd. (193 g/m²), SYMMETRICAL, INTERLACED OPEN-WEAVE GLASS FIBER FABRIC MADE WITH MINIMUM 25 PERCENT BY WEIGHT ALKALINE RESISTANT COATING FOR COMPATIBILITY WITH STO MATERIALS.

B. HIGH IMPACT MESH
1. STO INTERMEDIATE MESH (MESH C) - NOMINAL 11.0 oz/sq yd. HIGH IMPACT, INTERWOVEN, OPEN WEAVE GLASS FIBER FABRIC WITH ALKALINE RESISTANT COATING FOR COMPATIBILITY WITH STO MATERIALS.

2.06 PRIMER
A. STO PRIMER
ACRYLIC BASED PRIMER (FOR STO ACRYLIC BASED FINISHES)

2.07 FINISH COAT
A. STO ACRYLIC BASED TEXTURED WALL COATING. SEE E.I.F.S. FORMULAS FOR FINISH COLOR.

2.08 JOB MIXED INGREDIENTS
A. PORTLAND CEMENT: ASTM C 150, TYPE I.
B. WATER: CLEAN AND POTABLE.

PART 3 EXECUTION

A. UNDER NO CIRCUMSTANCES SHALL ANY OF THE PRODUCTS BE ALTERED BY ADDING ANY ADDITIVES, EXCEPT FOR SMALL AMOUNTS OF CLEAN WATER AS DIRECTED ON LABEL. ANTIFREEZE, ACCELERATORS, RAPID BINDERS, ETC., ARE FORBIDDEN.

B. THE SURFACE TO RECEIVE THE EIFS SHALL BE STRUCTURALLY SOUND, CLEAN, DRY AND FREE OF WARPAGE, RESIDUAL MOISTURE OR DAMAGE FROM MOISTURE. SURFACES SHALL BE UNIFORM, WITH NO IRREGULARITIES GREATER THAN 1/8" IN 4'-0". SURFACES SHALL BE INSPECTED FOR COMPLIANCE WITH THE FOLLOWING REQUIREMENTS PRIOR TO INSTALLATION OF THE EIFS:

1. PLYWOOD SHEATHING SHALL MEET A.P.A. (AMERICAN PLYWOOD ASSOCIATION) REQUIREMENTS FOR EXTERIOR OR EXPOSURE 1 CLASSIFICATION. APA DESIGN AND CONSTRUCTION GUIDELINES SHALL BE FOLLOWED FOR STORAGE, HANDLING AND INSTALLATION. MANUFACTURER'S PUBLISHED RECOMMENDATIONS SHALL BE FOLLOWED FOR ALL SHEATHING NOT IN COMPLIANCE SHALL BE REPLACED TO CONFORM WITH SPECIFICATION REQUIREMENTS PRIOR TO INSTALLATION OF THE EIFS.

2. CONCRETE, MASONRY OR PLASTER SURFACES SHALL BE PROPERLY CURED AND FREE OF DIRT, DUST, OIL, GREASE, MILDEW, FUNGUS, LATENCY, PAINT, EFFLORESCENCE AND ANY OTHER CONTAMINANT. ANY SURFACES NOT IN COMPLIANCE SHALL BE CORRECTED PER MANUFACTURER'S RECOMMENDATIONS PRIOR TO INSTALLATION OF THE EIFS.

C. AFTER SATISFACTORY INSPECTION OF SURFACES AND CORRECTION OF ANY DEVIATIONS FROM SPECIFICATION REQUIREMENTS, THE EIFS INSTALLATION MAY BEGIN PER MANUFACTURER'S INSTRUCTIONS.

D. THE STARTER STRIP OF MESH SHALL BE WIDE ENOUGH TO ADHERE 4" OF MESH ONTO THE WALL. BE ABLE TO WRAP AROUND THE BOARD EDGE AND COVER APPROXIMATELY 4" ON THE OUTSIDE SURFACE OF THE BOARD. THIS "BACKWRAP" PROCEDURE SHALL BE FOLLOWED AT ALL EXPOSED BOARD EDGES IN ACCORDANCE WITH DETAILS (EXAMPLE: WINDOW AND DOOR HEADS AND JAMBS).

E. APPLY THE ADHESIVE TO THE BACK OF THE INSULATION BOARD. STAGGER VERTICAL JOINTS AND INTERLOCK JOINTS AT ALL INSIDE AND OUTSIDE CORNERS. APPLY FIRM PRESSURE OVER ENTIRE SURFACE OF THE BOARDS TO INSURE UNIFORM CONTACT. BOARDS SHALL BRIDGE SHEATHING JOINTS BY A MINIMUM OF 8". ALL BOARD JOINTS SHALL BE BUTTED TIGHTLY TOGETHER TO ELIMINATE ANY THERMAL BREAKS IN THE EIFS. CARE MUST BE TAKEN TO PREVENT ANY ADHESIVE FROM GETTING BETWEEN THE JOINTS OF THE BOARDS. ALL OPEN JOINTS IN THE INSULATION BOARD LAYER SHALL BE FILLED WITH SLIVERS OF INSULATION OR AN APPROVED SPRAY FOAM.

F. NAILS, SCREWS, OR ANY OTHER TYPE OF NONTHERMAL MECHANICAL FASTENER SHALL NOT BE USED.

G. EXPANSION JOINTS ARE REQUIRED IN THE EIFS WHERE THEY EXIST IN THE SUBSTRATE, WHERE THE EIFS ADJOINS DISSIMILAR CONSTRUCTION, AND AT FLOOR LINES IN MULTILEVEL WOOD FRAME CONSTRUCTION. THE EIFS SHALL TERMINATE AT THE EXPANSION JOINT TO PROVIDE APPROPRIATE JOINT SIZE (SEE DETAILS) AND ALL BOARD EDGES SHALL BE COATED WITH APPROPRIATE GROUND COAT AND MESH IN ACCORDANCE WITH STANDARD "BACKWRAPPING" PROCEDURE. APPROPRIATE SEALANT/PRIMER AND BACKER SHALL BE INSTALLED AFTER GROUND COAT IS FULLY DRY TO PREVENT ANY WATER FROM GETTING INTO OR BEHIND THE SYSTEM.

H. USE OF PLASTIC OR METAL CORNER BEADS, STOPBEADS, ETC., IS FORBIDDEN.

I. APPLY APPROPRIATE GROUND COAT OVER THE INSULATION BOARD WITH PROPER SPRAY EQUIPMENT OR A STAINLESS STEEL TROWEL TO A UNIFORM THICKNESS OF APPROXIMATELY 1/16". WORK HORIZONTALLY OR VERTICALLY IN STRIPS OF 40", AND IMMEDIATELY EMBED STANDARD REINFORCING MESH INTO THE WET GROUND COAT. THE MESH SHALL BE DOUBLE WRAPPED AT ALL CORNERS AND OVERLAPPED NOT LESS THAN 2-1/2" AT MESH JOINTS. AVOID WRINKLES IN THE MESH. THE FINISH THICKNESS OF THE GROUND COAT SHALL BE SUCH THAT THE MESH IS FULLY EMBEDDED. ALLOW GROUND COAT TO THOROUGHLY DRY BEFORE APPLYING PRIMER OR FINISH.

J. DUPLICATE INSTALLATION PROCESS NOTED IN 3.01 M USING STANDARD MESH CREATING SECOND MESH LAYER AND ADDITIONAL IMPACT RESISTANCE. ALLOW TO DRY BEFORE APPLICATION OF EITHER STO PRIMER (OPTIONAL) OR STO FINISH.

K. IF A PRIMER IS USED, APPLY WITH BRUSH, ROLLER OR PROPER SPRAY EQUIPMENT OVER CLEAN, DRY GROUND COAT AND ALLOW TO DRY THOROUGHLY BEFORE APPLYING FINISH. P. APPLY FINISH DIRECTLY OVER THE GROUND COAT (OR PRIMED GROUND COAT) ONLY AFTER THE GROUND COAT/PRIMER HAS THOROUGHLY DRIED. THE FINISH SHALL BE APPLIED BY SPRAYING, ROLLING OR TROWELING WITH A STAINLESS STEEL TROWEL, DEPENDING ON FINISH SPECIFIED. GENERAL RULES FOR APPLICATION OF FINISHES ARE AS FOLLOWS:

1. USE A CLEAN, RUST-FREE, HIGH-SPEED MIXER TO THOROUGHLY STIR THE FINISH TO A UNIFORM CONSISTENCY (SMALL AMOUNTS OF CLEAN WATER MAY BE ADDED TO AID WORKABILITY).

2. AVOID APPLICATION IN DIRECT SUNLIGHT.

3. APPLY FINISH IN A CONTINUOUS APPLICATION, ALWAYS WORKING TO A WET EDGE.

4. WEATHER CONDITIONS AFFECT APPLICATION AND DRYING TIME. HOT OR DRY CONDITIONS LIMIT WORKING TIME AND ACCELERATE DRYING AND MAY REQUIRE ADJUSTMENTS IN THE SCHEDULING OF WORK TO ACHIEVE DESIRED RESULTS. COOL OR DAMP CONDITIONS EXTEND WORKING TIME AND RETARD DRYING AND MAY REQUIRE ADDED MEASURES OF PROTECTION AGAINST WIND, DUST, DIRT, RAIN AND FREEZING.

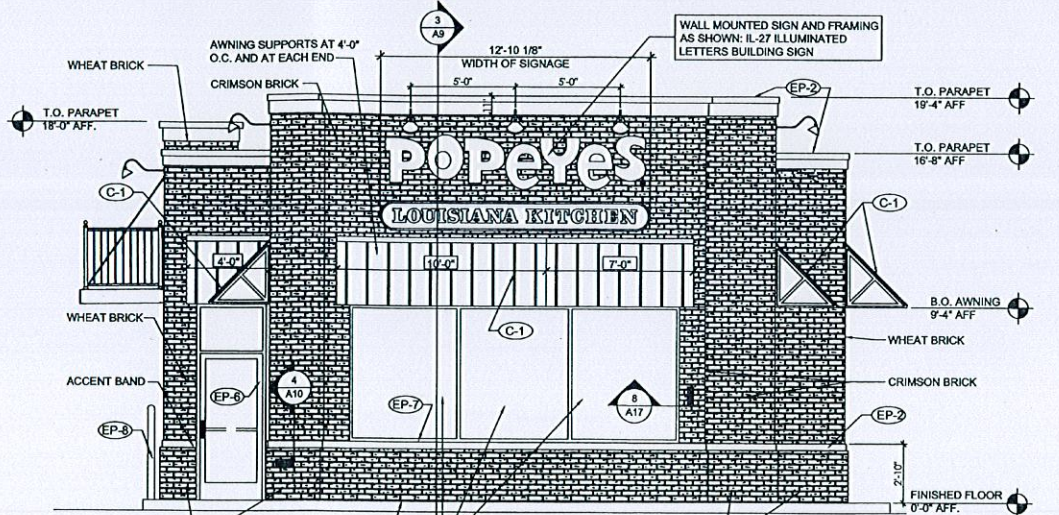
5. AESTHETIC "U"-GROOVES MAY BE DESIGNED INTO THE SYSTEM. (A MINIMUM OF 3/4" INSULATION BOARD MUST BE LEFT AFTER ANY GROOVES ARE CUT).

6. "R" (RILLED TEXTURE) FINISHES MUST BE FLOATED WITH A PLASTIC TROWEL TO ACHIEVE THEIR RILLED TEXTURE.

7. AVOID INSTALLING SEPARATE BATCHES OF FINISH SIDE-BY-SIDE.

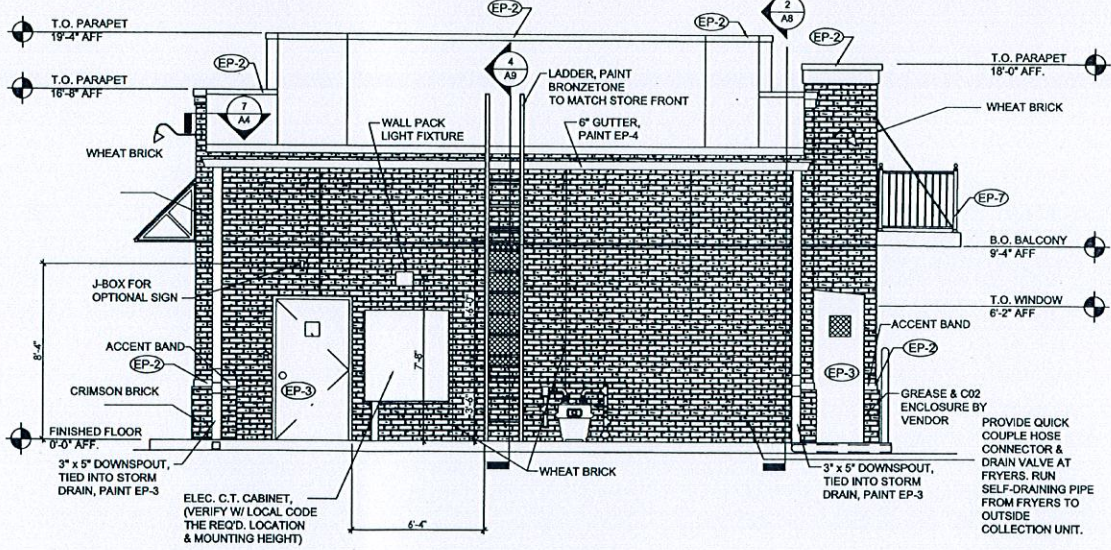
8. APPLY FINISH COLOR TO EIFS MIX AND APPLY TO WALL COLOR TO MATCH EXTERIOR FINISH SCHEDULE COLORS.

L. STO EXTERIOR INSULATION AND FINISH TEXTURE SYSTEM: APPLY HIGH IMPACT SYSTEM ADJACENT TO DOORS FOR ADDITIONAL IMPACT RESISTANCE. USING STO INTERMEDIATE MESH. USE THE STANDARD SYSTEM SPECIFICATIONS AT ALL OTHER LOCATIONS.



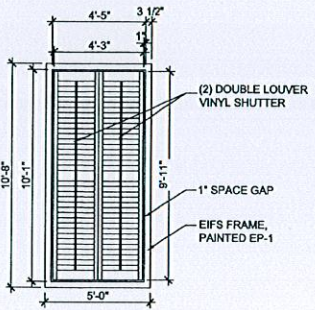
1 FRONT ELEVATION

1/4"=1'-0"



2 REAR ELEVATION

1/4"=1'-0"

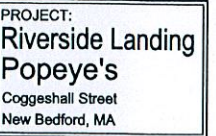
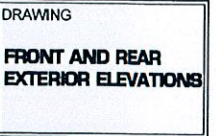
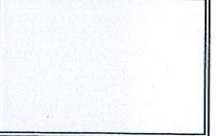
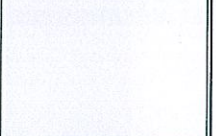
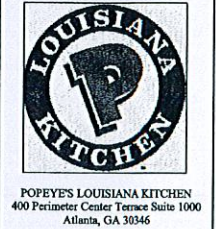


3 SHUTTER DETAIL

1/4"=1'-0"

ISSUED FOR:	
DATE	DESCRIPTION
08-03-16	FOR PLANNING REVIEW

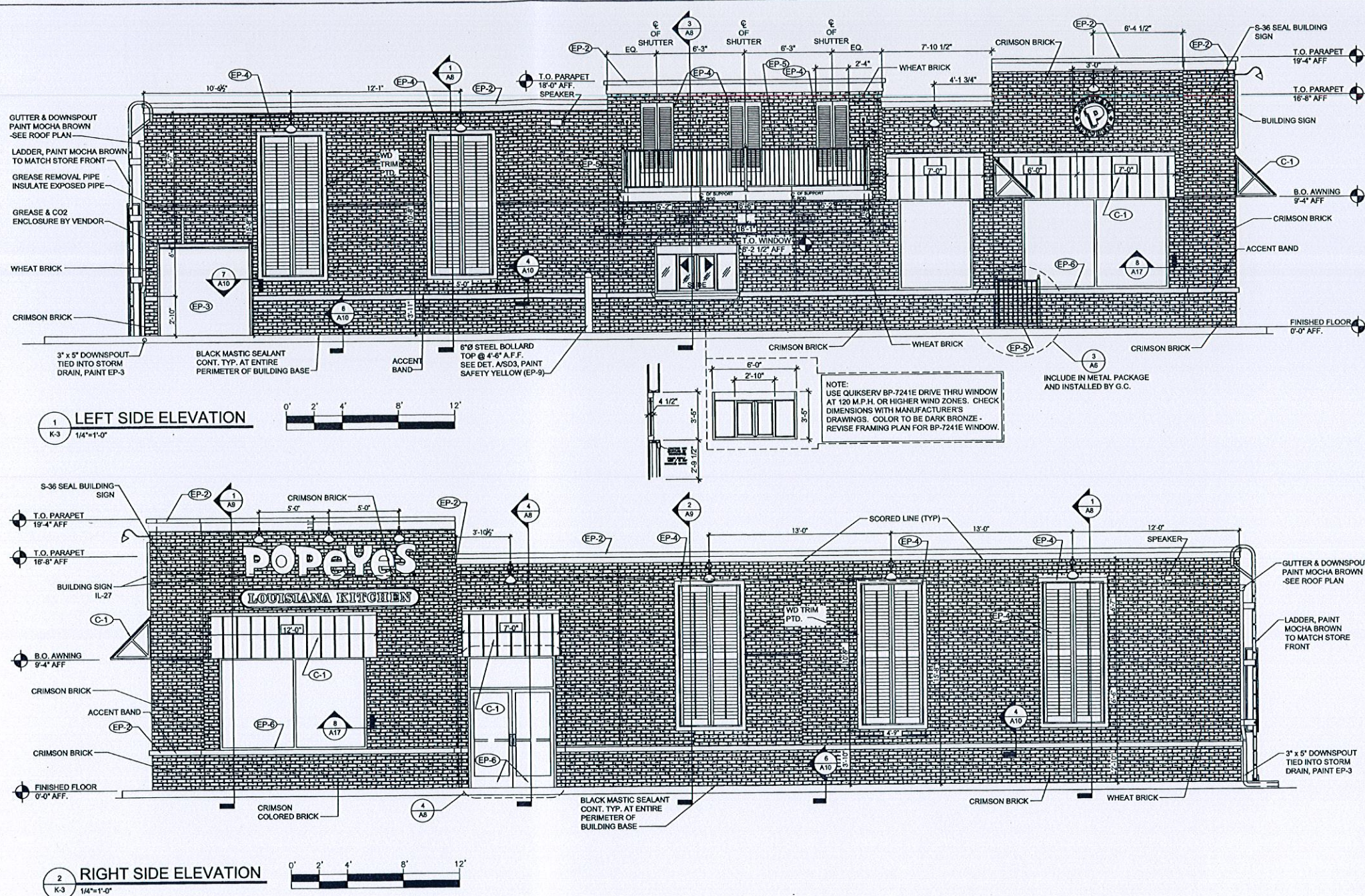
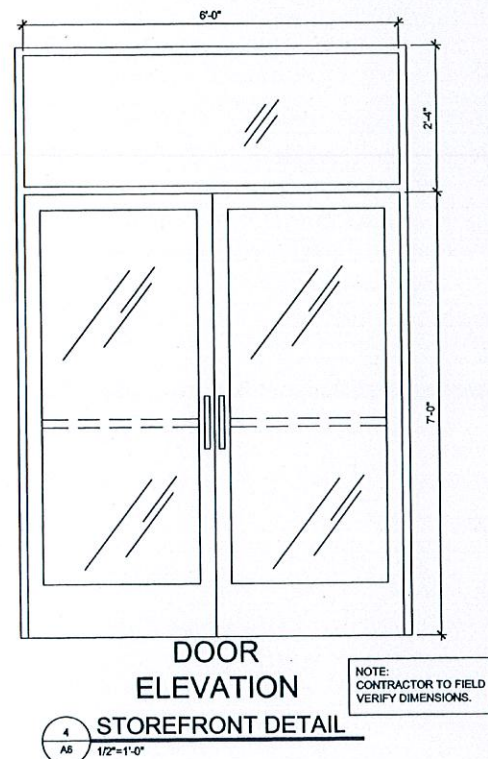
REVISIONS		
NO.	DATE	DESCRIPTION
1	08-03-16	REVISED PER TOWN COMMENTS



DATE	07.28.2016
PROJECT NO.	160626
DRAWN BY	CPC
CHECK BY	PM
DWG NO.	

SCALE	AS NOTED	SHT. NO.	2 OF 3
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PLANNING
AUG 05 2016
DEPARTMENT

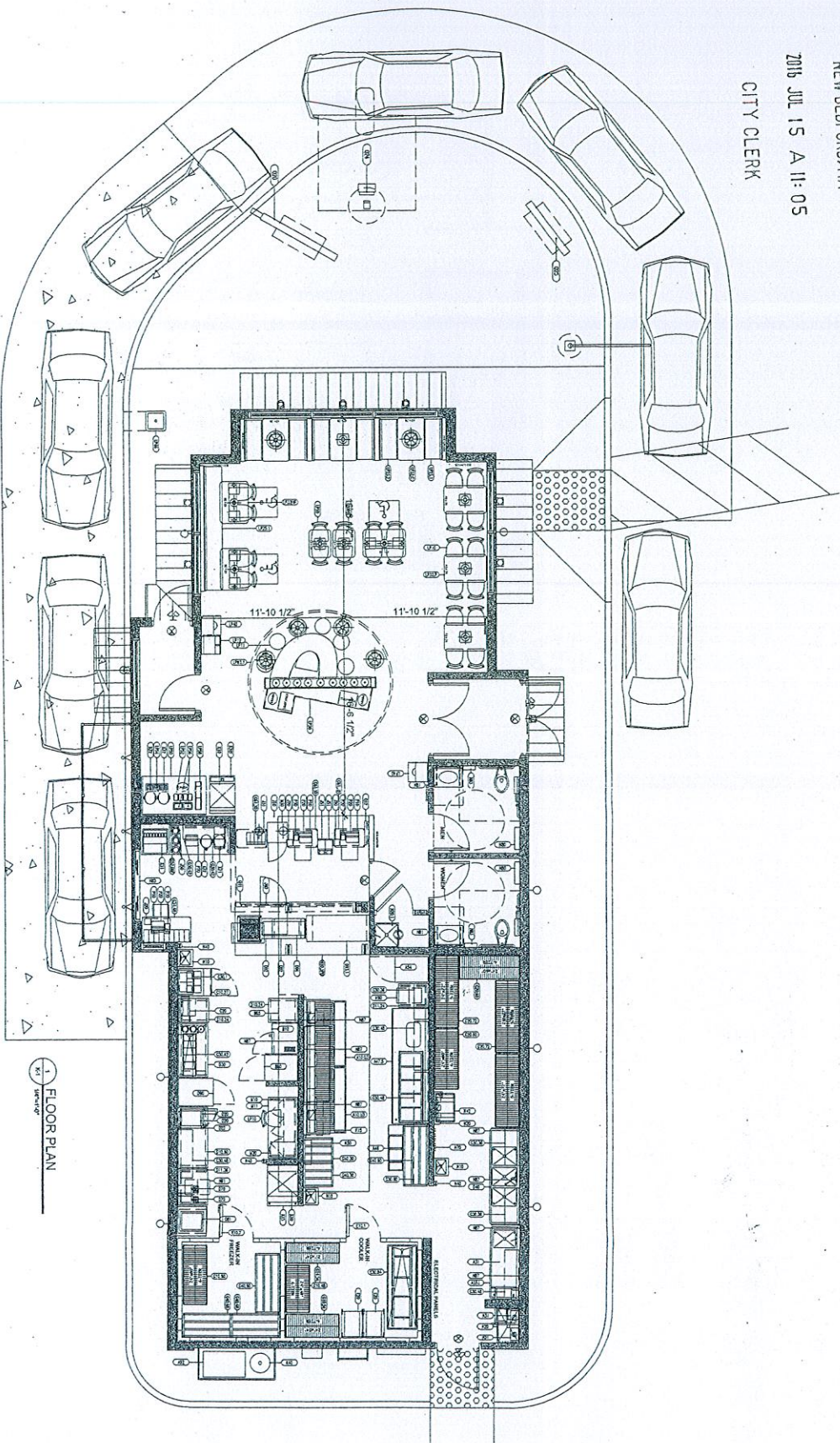


3. **GUARANTEES AND WARRANTIES:** AT THE COMPLETION OF THE GLAZING PROVE THE INSTALLATION WATERTIGHT BY SPRAYING ALL JOINTS WITH A GARDEN HOSE WITH NOZZLE SET FOR MAXIMUM PRESSURE. PROVIDE ADDITIONAL CAULKING OR SEALANT, OR RESET GLASS AS NECESSARY TO EFFECT A WATERTIGHT JOB.

K-3	
SCALE AS NOTED	SHT. NO. 3 OF 3

PLANNING
AUG 05 2018
DEPARTMENT

CITY CLERK



1
K-1
146'-1'-0"

FLOOR PLAN

Case 25-16
07/15/2016

[illegible]



Department of Public Infrastructure

Euzebio Arruda
Commissioner


CITY OF NEW BEDFORD

Jonathan F. Mitchell, Mayor

Water
Wastewater
Highways
Engineering
Cemeteries
Park Maintenance
Forestry
Energy

MEMORANDUM

TO: City of New Bedford Planning Board

FROM: Euzebio Arruda, Commissioner, D.P.I. 

DATE: August 1, 2016

SUBJECT: Site Plan - Riverside Landing
Phase III Modification
Coggeshall Street
Lot 93 Lots 264 & 261

The Department of Public Infrastructure has reviewed the proposed site plan referenced above and recommends approval with the following conditions:

1. All utilities to be installed in accordance with City of New Bedford standards.
2. Developer or engineer must contact the Department of Public Infrastructure about water services to the proposed building.
3. Owner to donate 10 trees to the City of New Bedford to be planted in the vicinity of the proposed project.
4. Owner/Developer must contact the Department of Public Infrastructure Engineering Division to assign a new address for the proposed building.
5. The Department of Public Infrastructure requires a final set of approved plans to be submitted, reflecting all revisions made, prior to the start of construction.
6. Developer and site contractor must schedule a pre-construction meeting with the Department of Public Infrastructure prior to the start of the construction.
7. Upon completion, engineer must submit "As-Built Drawings" in CADD format prior to the Certificate of Occupancy being issued.

CC: Department of Inspectional Services
Environmental Stewardship
Prime Engineering, Inc
DW New Bedford LLC

PLANNING
AUG 05 2016
DEPARTMENT



City of New Bedford
REQUEST FOR WAIVER

CASE #:25-16

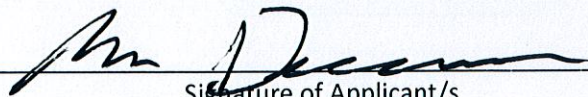
APPLICATION FOR WAIVER FROM SITE PLAN REVIEW REQUIREMENTS

In certain instances, after consulting with the City Planner, the applicant may submit, in writing, a request for waiver for any of the submittal or technical requirements of Section 5430 and 5440 where the project involves relatively simple development plans. The Planning Board will take a separate vote on written waiver requests by the applicant. Each request for waiver must be submitted individually to be considered by the Board. Please provide one (1) original and fifteen (15) copies of the request.

Any granted waivers must be disclosed on the final submitted and approved site plan.

SUBJECT PROPERTY			
ASSESSOR'S MAP PLOT#	93	LOT(S)#	261 & 292
REGISTRY OF DEEDS BOOK:	10843	PAGE #	287
PROPERTY ADDRESS: Former Fairhaven Mills Site, Coggeshall Street			
ZONING DISTRICT: HLS IPOD			
OWNER INFORMATION			
NAME: D.W. New Bedford LLC			
MAILING ADDRESS: 1266 Furnace Brook Parkway, Quincy, MA 02169			
APPLICANT/CONTACT PERSON INFORMATION			
NAME (IF DIFFERENT):			
APPLICANT'S RELATIONSHIP TO THE PROPERTY: Check one:	OWNER <input checked="" type="checkbox"/>	CONTRACT VENDEE <input type="checkbox"/>	OTHER Describe <input type="checkbox"/> _____
MAILING ADDRESS (IF DIFFERENT):			
TELEPHONE #	(617) 770-1955		
EMAIL ADDRESS:	mark@dickinsondev.com		

By signing below, I/we acknowledge that all information presented herein is true to the best of my/our knowledge. I/we further understand that any false information intentionally provided or omitted is grounds for the revocation of the approval(s). If petition is granted, I/we understand the approvals are specific to the plans submitted, unless the Board states otherwise and that if granted, that the waiver(s) must be noted on the approved Site Plan and acted upon within one year.


Signature of Applicant/s

AUG. 4, 2016
Date

PLANNING

AUG 05 2016

DEPARTMENT

If the applicant differs from the owner, this section must be completed/signed by the property owner/s:

I hereby authorize the applicant represented above and throughout this application to apply and to represent my/our interests on my/our behalf for the relief requested herein for the premises I/we own noted as "property address" above and presented throughout this application. Furthermore, by signing this application I/we acknowledge having read and understood this application and the accompanying instructions and information. If petition is granted, I/we understand the approvals are specific to the plans submitted, unless the Board states otherwise and that if granted, that the waiver(s) must be noted on the approved Site Plan and acted upon within one year.

Signature of Owner/s

Date

DESCRIPTION		Ordinance Section	CLEARLY Describe why this request is being made.
	1	***Example*** 5451. b. Topography and Drainage Plan	***Example*** There currently exists a structure and pavement on the site. No excavation of the site is being proposed, therefore topography and drainage will not be altered.
	2	Size of parking spaces - 18 feet long, instead of 20 feet long.	Allows additional pervious landscaping (see attached Narrative).
	3	Submission of new Stormwater Report.	Previously submitted Stormwater Report has already been approved (see attached Narrative).
	4		

Additional pages describing the waiver request may be attached to this form following the same chart format, if necessary.



Please check here if additional pages are attached.



Number of Waiver requests submitted for consideration: 2

PLANNING

AUG 05 2016

DEPARTMENT

- **Basis for Waiver Request for Reduction in Size of Parking Spaces**

It is proposed to have the southernmost row of parking spaces to be 9 feet wide by 18 feet long, rather than the 9 feet wide by 20 feet long as typically required. This will allow an additional 180 square feet of landscaping along Coggeshall Street and will still accommodate a 20 foot long vehicle, since there can be 2 feet of vehicle overhang over the landscaped area.

- **Basis for Waiver Request for Not Submitting a Second Drainage Report**

A Drainage Report was already submitted and approved, when the Special Permit and Site Plan Review approval, was issued in July 2013. Over eighty percent of the approved site work was initiated and completed, thereby validating the Special Permit, which is still active. All of the approved stormwater system has already been installed and is fully functional. Since there will be no significant change in the approved runoff characteristics, it is requested to not be required to submit a second report. When the Special Permit was approved, almost the entire site was covered by impervious concrete slab with no stormwater controls. Since then, the slab was removed and the approved stormwater system, with a water quality treatment system, has been constructed. In addition, a significant amount of pervious landscaping has been added.

PLANNING
AUG 05 2013
DEPARTMENT